

DETAILED SITE PLANS  
R-CODE VARIATIONS

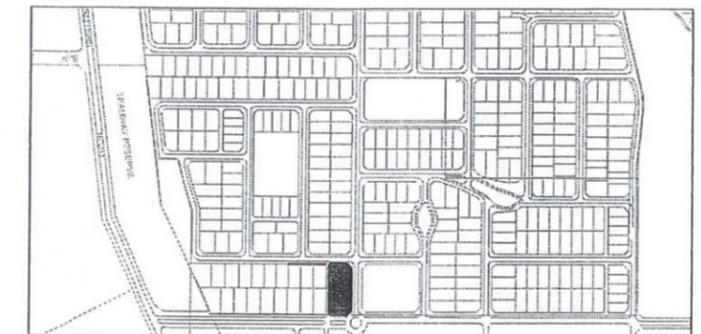
The Town Planning Scheme and The Residential Design Codes are varied in the following manner:

- All dwellings and ancillary development must be located within the building envelopes as depicted on the DAP.
- The requirements of the Residential Design Codes are varied as shown on the DAP.
- The requirements of the Residential Design Codes and Town Planning Scheme shall be satisfied in all other matters.
- Consultation with adjoining or other landowners to achieve a variation to the Residential Design Codes, as provided for by the DAP, is not required.
- Density coding is R30.
- An alternative garage location to that shown on the plan may be approved by the Manager, Statutory Planning, subject to the design meeting solar orientation principles and statutory requirements.
- It is mandatory for all lots to locate carports and garages at the rear with access via the laneway.
- Rear garage setbacks shall be a minimum of 1.0m with no protrusion of both eaves and gutters into the setback.
- A five (5%) variation to minimum open space requirements (in addition to that described in the Open Space definition of the Codes) shall be permitted. Open space shall be calculated in accordance with the provisions of the 2002 Residential Design Codes.
- The front setback of all dwellings shall be determined in accordance with the following:
  - The minimum primary street setback shall be 2.5m;
  - The maximum setback of a wall addressing the primary street shall be 4.5m;
- All dwellings must orientate towards the street which it abuts.
- A nil side setback is permissible on the lots where shown on the DAP. Side setbacks may be reduced to nil for 2/3 of the length of the boundary provided that the walls do not exceed 3.0m average and 3.5m maximum height.
- A 2m solar setback applies, as shown on the DAP, generally along the boundary which adjoins a permitted nil setback boundary (on an abutting lot). Council will only consider applications for structural development in the solar setback where the principles and benefits of solar access to the dwelling are being maintained.
- A 1.8 metre second storey setback applies, as shown on the DAP, along the boundary where a nil ground storey setback is permitted, unless the R-Codes specify a greater setback.
- Fences shall not be located within 1.0 metre of the intersection of a crossover and shall be truncated at 45 degrees so that no portion of the fence is within the truncation in order to enhance safety and sightlines.
- Setback variations to corner lots may be approved at the discretion of the Manager Building Services at the City of Rockingham where the configuration of these lots may limit compliance with setback requirements.

This Detailed Area Plan has been approved by the Manager, Statutory Planning.

*[Signature]*  
Manager Statutory Planning

17/4/2007  
Date



LOCATION PLAN

- LEGEND
- BUILDING ENVELOPE
  - DESIGNATED GARAGE LOCATION
  - PREFERRED GARAGE LOCATION
  - NO VEHICLE ACCESS

client:  
CEDAR WOODS

scale: 1:1000 @ A3

drawn: LH

checked: LH

approved: ER

date: 20.02.07

datum: PCG 94

day

town planning design

dra no: CWP PEE DAP 05 rev D

**LOCAL DEVELOPMENT PLAN APPROVAL EXPIRED**

This Local Development Plan expired on 19 October 2025, pursuant to Clause 57 of the Planning and Development (Local Planning Schemes) Regulations 2015, being 10 years after the Regulations came into effect.



DETAILED AREA PLAN

GRANDIS DRIVE "THE RIVERGUMS"  
BALDIVIS - CITY OF ROCKINGHAM