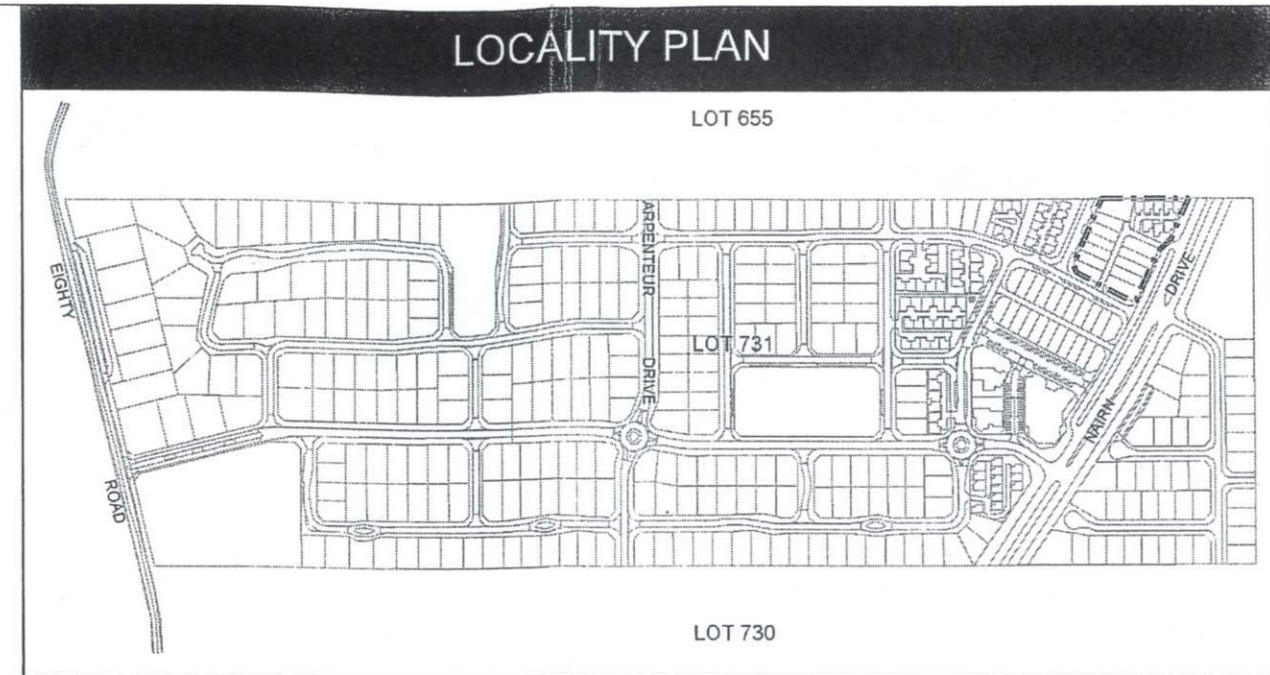


**LEGEND**

DESIGNATED GARAGE LOCATION      cly COURTYARD LOCATION  
 BUILDING ENVELOPES      NO VEHICLE ACCESS  
 ORIENTATION OF BUILDINGS  
 ALL LAND WITHIN THE DAP IS CODED R40

# DETAILED AREA PLAN

## 'THE RIDGE' BALDIVIS (1)



### DETAILED AREA PLAN R-CODE VARIATIONS

The Town Planning Scheme and The Residential Design Codes are varied in the following manner:

- All dwellings and ancillary development must be located within the building envelopes as depicted on the DAP.
- The requirements of the Residential Design Codes are varied as shown on the DAP.
- The requirements of the Residential Design Codes and Town Planning Scheme shall be satisfied in all other matters.
- Consultation with adjoining or other landowners to achieve a variation to the Residential Design Codes, as provided for by the DAP, is not required.
- Density Coding is as depicted on the Detailed Area Plan.
- An alternative garage location to that shown on the plan may be approved by the Manager, Statutory Planning, subject to the design meeting solar orientation principles, streetscape objectives and statutory requirements.
- A five percent (5%) variation to minimum open space requirements (in addition to that described in the Open Space definition of the Codes) shall be permitted for single storey dwellings. Open space shall be calculated in accordance with the provisions of the 2002 Residential Design Codes.
- A nil setback is permissible on the lots where shown on the DAP (generally the southern boundaries). Side setbacks for lots coded R30 or higher may be reduced to nil for 2/3 the length of the boundary, provided that walls do not exceed 3.0 m average and 3.5 m maximum height.
- A 2 m solar setback applies, as shown on the DAP, generally along the boundary which adjoins a permitted nil setback boundary (on an abutting lot). Council will only consider applications for structural development in the solar setback where the principles and benefits of solar access to the dwelling are being maintained.
- The front setback of dwellings shall be determined in accordance with the following:-
  - The minimum primary street setback shall be 2.0 metres.
  - The maximum setback of a wall addressing the primary street shall be 4.0 metres.
 Variations to these provisions may be approved by the Manager, Statutory Planning for corner lots, subject to the design meeting solar orientation and streetscape objectives.
- It is mandatory for all allotments with laneway access to locate carports and garages at the rear with access via the laneway.
- Fences shall not be located within 0.5 m of the intersection of a crossover and shall be truncated at 45 degrees so that no portion of the fence is within the truncation in order to enhance safety and sight lines.
- Minimum rear setback shall be 1.0 m for house and garage with no protrusion of eaves into setbacks.

This Detailed Area Plan has been adopted by Council and signed by the Manager Planning Services.

Manager Planning Services      28/11/05 Date

DATE : 24 NOVEMBER 2005  
 PLAN NO. 95/059/033B  
 SCALE : 1:750 @ A3



49207

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