



# Kennedy Bay Coastal Village

## Local Development Plan

Revision Letter	Date	Reason for Issue	INITIAL
A	13-03-2023	WIP Draft for Review	GY
B	21-04-2023	Draft for Client Review	GY
C	28-07-2023	Draft for LG Pre-Lodgement Review	GY
D	18-10-2023	Final Draft for Advertising	GY
E	06-12-2023	Final for Lodgement	GY
F	08-03-2024	Adopted	GY
G	04-07-2024	Minor Amendment	GY

**Project No:** 44381

**Project Name:** Kennedy Bay Coastal Village

**Prepared for:**



**Prepared by:**



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### Endorsement

This LDP has been approved under delegated authority by the City pursuant to Clause 52(1)(a) of the deemed provisions

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Manager Statutory Planning

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# TABLE OF CONTENTS

<b>01 INTRODUCTION</b>	<b>4</b>	<b>02 DEVELOPMENT CONTROLS</b>	<b>8</b>	<b>03 PROMINENT LOCATIONS</b>	<b>16</b>
1.1 PURPOSE OF THE LDP	4	2.1 LOT ONE	9	3.1 MAIN STREET INTERFACE	16
1.2 OPERATION	4	2.2 LOT TWO AND THREE	10	3.2 FORESHORE INTERFACE	17
1.3 RELATIONSHIP TO THE R-CODES	5	2.3 LOT FOUR	11	3.3 POS INTERFACE	17
1.4 VISION	6	2.4 LOT FIVE	12		
		2.5 LOT SIX	13	<b>APPENDICES</b>	
		2.6 LOT TEN	14	KENNEDY BAY LANDSCAPE MASTERPLAN	
		2.7 LOT THREE THOUSAND AND NINETEEN	15	KENNEDY BAY COASTAL VILLAGE TOWN CENTRE PARKING STRATEGY	
				LOTS SUBJECT TO RECIPROCAL PARKING ARRANGEMENTS	

# 01 INTRODUCTION

## 1.1 PURPOSE OF THE LDP

The purpose of the Kennedy Bay Coastal Village Local Development Plan (herein referred to as the LDP) is to provide site specific development provisions for land identified in Kennedy Bay Local Centre located within the core of the Kennedy Bay Structure Plan (Structure Plan), refer Map 1.

**Figure 1: Kennedy Bay Coastal Village LDP Area**



## 1.2 OPERATION

This LDP has been prepared in accordance with Part 6 of the Deemed Provisions of the Planning and Development (Local Planning Scheme) Regulations 2015 (P&D Regulations). This LDP comes into effect on the day it is approved by the City of Rockingham/Western Australian Planning Commission. As per the Deemed Provisions of the P&D Regulations, from the date of endorsement, this LDP is to have effect for a period of 10 years, unless otherwise determined by the City of Rockingham.

This LDP is to be read in conjunction with the:

- + Structure Plan;
- + City of Rockingham Town Planning Scheme No.2;
- + State Planning Policy 7.3 - Residential Design Codes, Volume 1 - Part C: Medium Density (the Medium Density Code); and
- + State Planning Policy 7.3 - Residential Design Codes, Volume 2 - Apartments (the R-Codes Vol.2).

The development requirements apply to residential, mixed-use and non-residential development.

### EXERCISE OF JUDGMENT

Where an application does not meet the development controls of the LDP, the decision-maker may exercise judgement and undertake a merit-based assessment to determine the proposal. The development should be consistent with the Vision and Objectives of the Structure Plan and LDP.

## 1.3 RELATIONSHIP TO THE R-CODES

### RESIDENTIAL DEVELOPMENT

The State's Design WA policy suite offers contemporary provisions to guide development at Kennedy Bay through the R-Codes Volume 1 and 2. In accordance with the zoning of the Structure Plan, the R-Codes Volumes apply in accordance with **Table 1**.

**Table 1: Application of the R-Codes - Residential**

Zoning	R-Codes Volume 1	R-Codes Volume 2
Commercial (R-AC0)	Single Houses and Grouped Dwellings	Multiple Dwellings (including dwelling components of mixed use developments)
Residential (R80 - R100)		

In addition to the requirements of the R-Codes, a range of additional general and site-specific development provisions are noted within Section 2 of this document.

- + Amendments to the R-Codes Volume 1 and 2 are outlined in **Table 2**.
- + All other elements of the R-Codes apply without amendment

For development with multiple dwelling residential uses including residential mixed-use, the Intent, Element Objectives, Acceptable Outcomes and Design Guidance of the R-Codes Volume 2 will apply across this LDP with the amendments outlined in **Table 2**.

**Table 2: Relationship to the R-Codes - Residential**

R-Codes Volume	Design Element	Clause modified
R-Codes Volume 2	2.2 Building height	A2.2.1
	2.3 Street Setbacks	A2.3.1
	2.4 Side and Rear Setbacks	A2.4.1
	3.6 Public domain interface	Additional acceptable outcomes
	3.7 Pedestrian access and entries	Additional acceptable outcomes
	3.8 Vehicle access	Additional acceptable outcomes
	4.10 Façade Design	Additional acceptable outcomes
	4.11 Roof Design	Additional acceptable outcomes
R-Codes Volume 1 2021 / 2024 (Part C)	3.2 Building Height	2021 - 5.1.6 (C6) 2024 - C3.2.1
	3.3 Street Setbacks	2021 - 5.1.2(C2.1, C2.2), 5.2.1(C1.1) 2024 - C3.3.1, C3.3.4, C3.3.6
	3.4 Lot Boundary Setbacks	2021 - 5.1.3(C3.1, C3.2) 2024 - C3.4.1, C3.4.4
	3.5 Site works and retaining walls	Additional deemed-to-comply
	3.6 Streetscape	2021 - 5.2.2(C2), 5.2.4(C4.1) 2024 - C3.6.5, C3.6.7
	3.7 Access	2021 - 5.2.5(C5.1) 2024 - C3.7.1
	3.9 Solar access for adjoining sites	2021/2024 - All clauses
	3.10 Visual privacy	2021/2024 All clauses

### NON-RESIDENTIAL DEVELOPMENT

While the R-Codes Volume 2 is focused on apartment development, it has useful content which has been built upon to acknowledge commercial and retail development that is likely in the Local Centre. The Intent and Objectives identified in **Table 3** may be of use when applying discretion for non-residential development.

- + For development without residential uses the Intent, Element Objectives, Acceptable Outcomes and Design Guidance of the R-Codes Volume 2 do not apply unless referenced in **Table 3**.
- + Where the provisions of the R-Codes are applied to development without residential uses, references to "residents" should be construed to mean references to "building occupants".

**Table 3: Relationship to the R-Codes - Non-Residential**

R-Codes Vol. 2 Design Element	Development WITHOUT residential uses
3.3 Tree canopy and deep soil areas	The Intent and Objectives described for this element in R-Codes Vol. 2 applies.
3.6 Public Domain Interface	The Intent and Objectives described for this element in the R-Codes Vol.2 applies.
3.7 Pedestrian Access and Entries	The Intent and Objectives described for these elements in the R-Codes Vol.2 applies.
3.8 Vehicle Access	The Intent and Objectives described for these elements in the R-Codes Vol.2 applies.
3.9 Car and bicycle parking	
4.1 Solar daylight and access	The Intent described for this element in the R-Codes Vol.2 applies.
4.10 Façade Design	The Intent and Objectives described for these elements in the R-Codes Vol.2 applies.
4.11 Roof Design	
4.18 Utilities	The Intent and Objectives described for these elements in the R-Codes Vol.2 applies.
NOTES: + Note 1: Definition of Deep Soil Area as in the R-Codes Vol.2 + Note 2: Definition of Landscape/landscaping/landscaped as in the R-Codes Vol.2 + Note 3: Definition of tree sizes as described in Table 3.3b and Figure 3.3b in the R-Codes Vol.2	

## 1.4 VISION

The Kennedy Bay Coastal Village will be a relaxed coastal village attracting new residents and visitors to its protected north-facing beach, a unique place along Perth's coastline. It will grow as the heart of Kennedy Bay – connecting an enhanced Kennedy Bay Golf Course with the beach and tying together existing western and new eastern residential areas with a mix of uses.

- + Re-imagining an under used pristine, waterfront destination
- + Key focus is to unite nature, culture, community and business
- + Distinct connection between the Indian Ocean, residential homes, world-class golf course
- + Selection of cafes and restaurants, entertainment and retail options
- + Leisure community and tourism activities to create a vibrant waterfront

A range of living opportunities and activities will be available in the Coastal Village, linking urban experiences with the coastal environment – accessible streets and spaces that layer a mix of residential, retail, food and beverage, and short stay opportunities for everyone's enjoyment.

The design rationale for the Kennedy Bay Coastal Village is outline in more detail in the Structure Plan.





## KENNEDY BAY FORESHORE

- + The **Kennedy Bay Landscape Masterplan** is provided in Appendix A and illustrates the proposed key features that will contribute to the amenity and functionality of the Coastal Village. It is envisaged that the Foreshore will provide a variety formal and informal public spaces, with the majority of active uses centred around the Foreshore Plaza, the jetty connection adjoining the Coastal Village.
- + Foreshore Plaza to provide public pedestrian access and a strong connection through to the beach and jetty. This space is designed to be flexible and multi purpose, allowing for select vehicle loop access at certain times, the ability for ground floor businesses to open up with alfresco, and programming of temporary events such as exhibitions and markets.

## MAIN STREET

- + The local centre is designed around an important north-south main street that connects existing roads with new residential development beyond and celebrates views to and from the Golf Course club house.
- + It also serves a unique intersection for two green links, that bring the dunal landscape into the centre, softening the approach for residential development and offering connections through the site from existing residential areas west.
- + Thoughtful orientation of the main street helps it become more than a destination. As the arrival point to Kennedy Bay, it has been designed as the route most local pedestrians and vehicles take to reach the activities in the area and their homes, creating a hub of community activity.

## LANDSCAPE AND PUBLIC REALM

- + Provide wider dual use footpaths, pedestrian crossings, street furniture, avenue shade trees and formal planting beds that create a safe functional street environment
- + Incorporate artwork, signage and interpretive elements to assist with legibility of themes and aesthetics
- + High end material finishes and landscape treatments reflective of the more urban streetscape environment throughout the Coastal Village area
- + CPTED principles to be an integral component of landscape and public realm design to create a secure and inviting environment



## CLIMATE

- + The local centre has been designed with climatic conditions in mind. Main street footpaths and open spaces will be protected from south-westerly breezes and receive natural light on both sides. This allows visitors pleasant environments to stay outside and helps activate future food and beverage outlets with outdoor dining.

## MOVEMENT AND STRUCTURE

- + Urban blocks are influenced by the main street, creating a permeable grid that is simple for users to navigate.
- + Streets designed to ensure a low speed, pedestrian focused environment. To promote the pedestrian experience, it is envisaged that the 'main street' will be constructed of a different surface treatment to differentiate the main street from the remainder of the movement network.
- + Pedestrian and cyclist connection throughout the site with a focus on permeability and legibility through to the foreshore, linking up to the regional pedestrian and cycle networks that run east-west along the coast.

## PARKING

- + Parking in the Coastal Village has been informed by the **Kennedy Bay Coastal Village Town Centre Parking Strategy** provided in the Appendices.
- + Parking is distributed throughout the precinct to meet resident and visitor needs, without impacting the amenity of the foreshore and town centre. An off-street car park is proposed in the foreshore reserve and additional parking can be utilised at the Kennedy Bay Links Golf Course.
- + Car parking has been carefully considered so as not to detract from the intended character of streetscapes.
- + Visible on-site parking spaces and garages are minimised through laneways that direct active uses to the main street and offering high quality interfaces between residential front doors, porches and the street – enhancing resident's amenity and public safety.
- + On-street parking provision is convenient and practical for visitors and helps to reinforce an urban street relationship.
- + Parking occupancy should be monitored to ensure actual demand generated within the centre can be accommodated within the precinct (through both on and off-street parking). If a lower level of parking demand is evident, some space currently dedicated to parking could be re-allocated to other land uses that enable placemaking such as landscaping or provision of public art.
- + Reciprocal parking easements may be used to manage parking for non-residential development across the precinct, as shown in the **Lots subject to Reciprocal Parking Arrangements** provided in the Appendices.

# 02 DEVELOPMENT CONTROLS

The following general and lot specific provisions are to be read together with the Structure Plan and include additional detail on primary controls, building siting and access.

GENERAL DEVELOPMENT PROVISIONS	
<b>General</b> Applies to all Development	<ul style="list-style-type: none"> <li>+ Buildings respond to the prevailing south-westerly wind conditions experienced in this coastal location, optimising year-round comfort and minimising unfavourable conditions and wind tunnel effects.</li> <li>+ Building design responds to Built-Form information provided in the Prominent Locations in Section 3 of this document.</li> <li>+ Development to provide a schedule of materials reflecting the coastal theme of the proposed development, in line with the vision outlined in this LDP, as well as responding to the Local Centre Design Rationale provided in the Kennedy Bay Structure Plan.</li> </ul>
<b>Landscaping</b> Applies to Non-Residential Development	<ul style="list-style-type: none"> <li>+ Tree canopy, deep soil areas and landscaping to be provided for non-residential development in accordance with the lot specific development controls of Section 2.</li> <li>+ All surface car parking is to have trees at a minimum of 1 tree per 4 car bays with a minimum deep soil area of 1.2m x 1.2m.</li> <li>+ A Landscaping Plan is required as part of the Development Application demonstrating the above requirements are met.</li> </ul>
<b>Streetscape</b> Applies to all Development	<ul style="list-style-type: none"> <li>+ Entries to retail and commercial shop fronts should support the fine grain of shopfronts, providing multiple doorways along the street, rather than a single congregated entry point.</li> <li>+ The entry doors to shops may be recessed up to 1.2m from the shopfront to provide a more articulated and fine grain appearance.</li> <li>+ Building elements such as roofs, balconies, windows, doorways and cladding materials should be arranged to contribute to the vision and public domain interface described in the Prominent Locations in Section 3 of this document.</li> <li>+ Window patterns and composition should reinforce the larger building facade emphasis, whether vertical or horizontal, and should encourage variation along the street.</li> <li>+ Design response should emphasise prominent entry points into buildings from the street.</li> </ul>
<b>Solar Access and Natural ventilation</b> Applies to Non-Residential Development	<ul style="list-style-type: none"> <li>+ For commercial and retail development, design of buildings should encourage use of natural daylight, solar access and ventilation through the use of windows, skylights, and light wells where appropriate.</li> <li>+ For commercial and retail development, buildings should be orientated to optimise the amount of occupied space receiving northern daylight.</li> <li>+ For commercial and retail development design should maximise the amount of space for occupants that can be naturally ventilated.</li> </ul>

**Figure 2: Indicative Coastal Village Built Form**



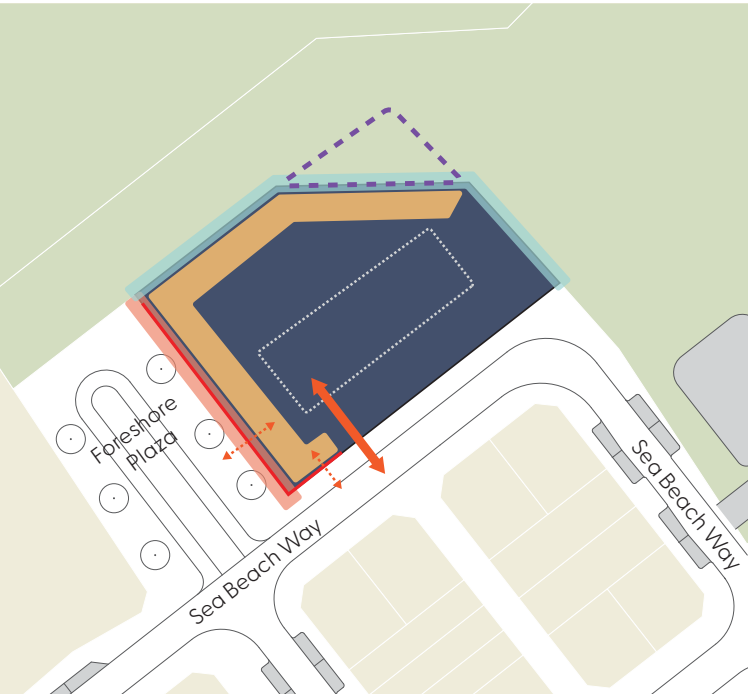


2.1 LOT ONE

**Advice Note for Lot 1** – This lot is situated in a mixed-use precinct and is currently affected (or may in the future be affected) by noise generated from adjacent preferred/permissible land uses. Residents should expect ambient noise levels to be relatively higher both inside and outside residences, due to the potential proximity of entertainment venues.

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Commercial R-AC0
Preferred land use	Commercial, cafe, restaurant, tavern, brewery, tourism, residential
Building height	<b>Minimum Height:</b> 7.5m minimum wall height (min 2 storey equivalent) <b>Maximum Height:</b> 3 storeys
Setbacks (Minimums)	<b>Foreshore plaza</b> Nil ground floor setback Nil upper floor setback <b>All other boundaries</b> 2.0m ground floor setback Nil upper floor setback
Pedestrian access	Location of pedestrian access provides safe, legible and convenient access for residents, and strengthens the connection to the Foreshore Plaza, main street and beach.
Vehicle access and servicing	Vehicle access to be provided from Street 2 only. Secondary vehicle access may be considered off Street 2 if required for servicing requirements. Width of vehicle access to be kept to a minimum and integrated into the design of the building, reducing the impact on the streetscape.
Parking	Centralising and concealing the impact of servicing and parking onto street edges, particularly away from the Foreshore Plaza. Car parking in this approximate location would allow for the siting of meaningful commercial, retail or residential building depths that address the adjoining streets Where parking can't be provided on-site, options for reciprocal parking and/or allocating parking throughout the precinct may be considered (refer <b>Parking Strategy</b> ) across all non-residential lots. A reciprocal parking arrangement shall apply between Lot 1 and Lot 4. Reciprocal parking will be enforced by way of condition of development approval requiring public access easements on relevant Certificate of Titles.
Landscaping	<b>Non-Residential Development</b> Non-residential development to provide a minimum 10% landscaping per site with a minimum dimension of 1.5m. <b>Residential Development</b> Refer to the R-Codes.

Site Plan



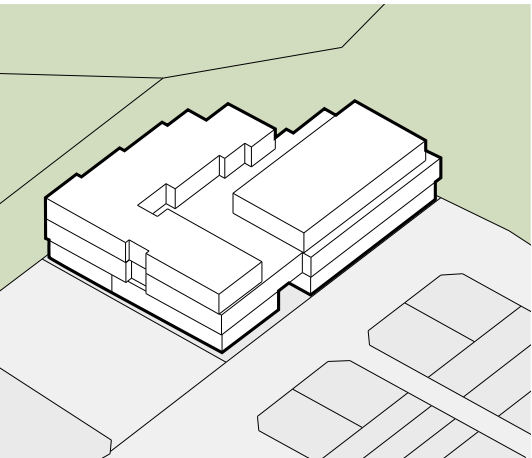
<b>Streetscape</b>	Development to front and provide an urban edge to Street 2, sleeving internal car parking, OR Provide a high quality landscaping interface to act as visual buffer between the carpark and adjoining development to the south-east.
<b>Prominent Locations</b> <i>In addition to the requirements of Section 3, development is to address the following:</i>	<b>Foreshore Interface</b> Development should maximise northern aspect and views to the coast, without dominating the visual amenity of the foreshore area. Development should avoid blank walls/façades, and large extents of non-operable glazing Preferred commercial uses on ground floor with residential on upper floors. Alfresco and uses that open out onto the foreshore area and foreshore plaza are encouraged, including a licensed alfresco area that leverages the relationship between the public and private realm (refer to the Kennedy Bay Landscape Masterplan). <b>Main Street Interface</b> Building facades should provide activation at the street level through a mixture of glazing, signage, lighting and external building fabric. Development should avoid blank walls/façades, and large extents of non-operable glazing Preferred commercial uses on ground floor with residential on upper floors. Alfresco and uses that open out onto the street encouraged.

Site Location



- Preferred ground floor commercial
- Main Street Interface
- Foreshore Interface
- Indicative alfresco area
- Preferred pedestrian access
- Preferred vehicle access
- Vehicle access restrictions
- Preferred location for parking/service areas
- Indicative On-Street Car Parking

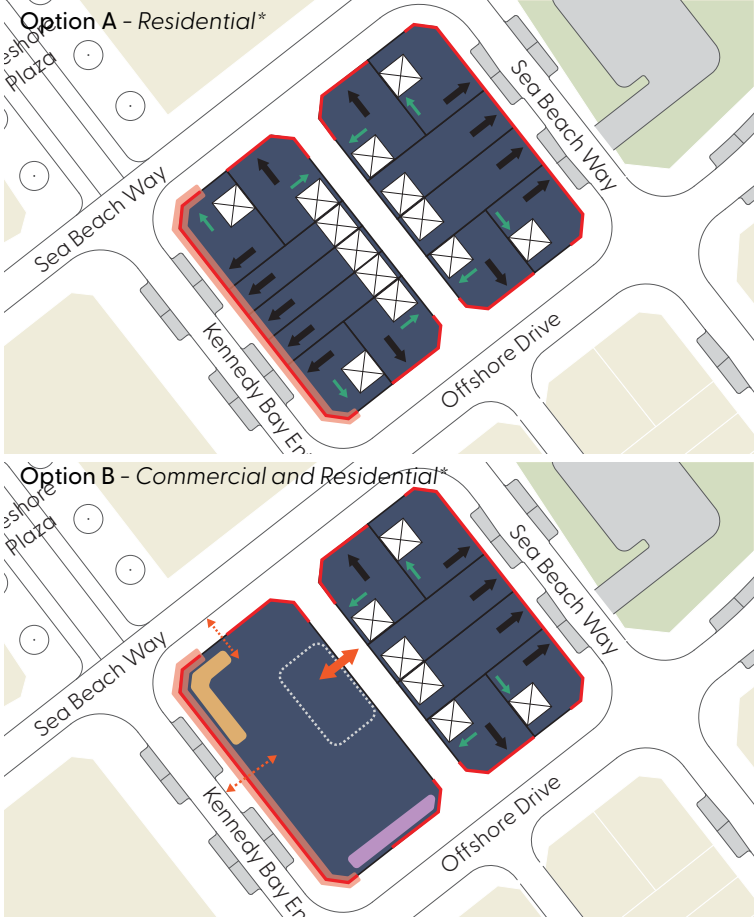
Indicative Built Form



2.2 LOT TWO AND THREE

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Commercial R-AC0
Preferred land use	Residential, mixed-use Opportunity for commercial and home businesses on ground floor fronting the street.
Building height	<b>Minimum Height:</b> 7.5m minimum wall height (min 2 storey equivalent) <b>Maximum Height:</b> 3 storeys
Street setbacks <small>(Residential)</small>	<b>Primary and secondary streets</b> 3.0m ground floor setback 2.0m upper floor setback Garages to be set back a minimum 3.5m. No additional setback to corner truncation. <b>Laneways</b> 1.0m ground floor setback Nil upper floor setback for a maximum of 2/3 of the site width.
Street setbacks <small>(Commercial)</small>	1.0m setback to all streets, including the laneway.
Lot boundary setbacks	7m maximum boundary wall height with no maximum length to both side boundaries.
Streetscape	The finished level of all of the lots with interface to public realm (excluding the laneway) must be elevated a minimum of 350mm and a maximum 500mm above the adjoining public realm. No visible retaining permitted in street setback area. Permeable fencing permitted to a maximum 1.2m height.
Vehicle access	Garages and preferred vehicle access as per the locations designated on the LDP. Minor variations to the locations may be permitted.
Parking <small>(Commercial)</small>	Where parking can't be provided on-site, options for reciprocal parking and/or allocating parking throughout the precinct may be considered (refer <b>Parking Strategy</b> ) across all non-residential lots. Reciprocal parking will be enforced by way of condition of development approval requiring public access easements on relevant Certificate of Titles.
Waste	Refuse collection is to occur from the rear laneway for those lots which abut laneway reserves.
Overshadowing	No maximum overshadowing.
Privacy	No privacy provisions apply.

Site Plan



\* Site plan options are indicative only and subject to subdivision application and approval by the WAPC.

**Prominent Locations**

*In addition to the requirements of Section 3, development is to address the following:*

**Main Street Interface**

Commercial, mixed-use encouraged on ground floor with residential on upper floors.

Building facades should provide activation at the street level through a mixture of glazing, signage, lighting and external building fabric. Development should avoid blank walls/façades, and large extents of non-operable glazing.

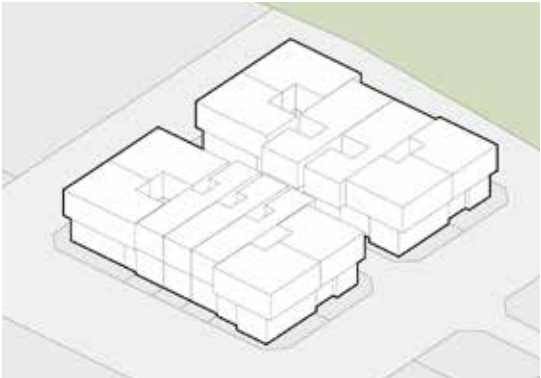
Where residential is provided on ground floor, dwellings should provide an urban edge and ensure a high level of interaction with the street.

Site Location



- Preferred ground floor commercial
- Preferred residential edge
- Main Street Interface
- Primary Dwelling/Development Orientation
- Secondary Dwelling/Development Orientation
- Preferred pedestrian access
- Preferred vehicle access
- Vehicle access restrictions
- Preferred Garage Location
- Preferred location for parking/service areas
- Indicative On-Street Car Parking

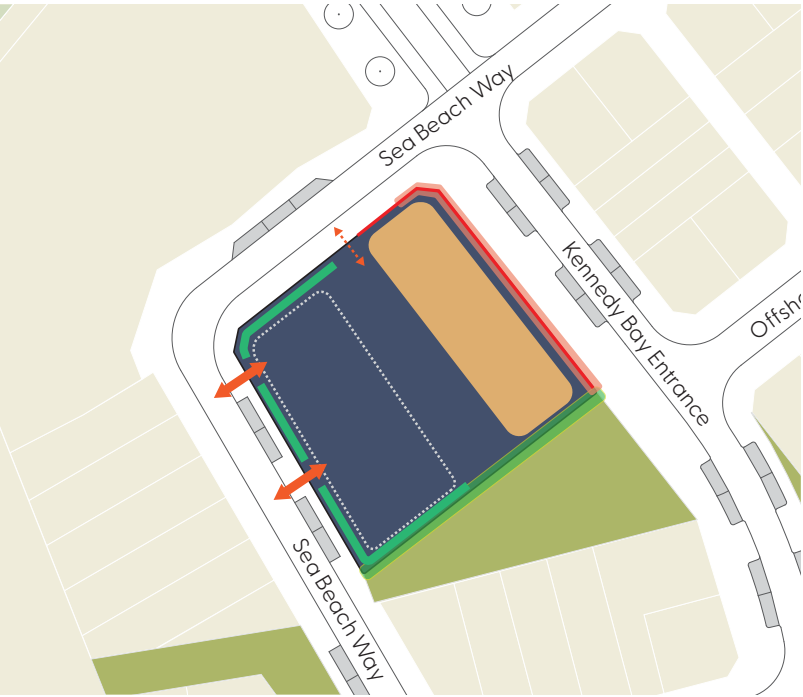
Indicative Built Form



2.3 LOT FOUR

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Commercial R-AC0
Preferred land use	<b>Ground Floor:</b> Commercial, cafe, restaurant, residential <b>Upper floors:</b> Residential, commercial, short-stay residential
Building height	<b>Minimum Height:</b> 7.5m minimum wall height (min 2 storey equivalent) <b>Maximum Height:</b> 4 storeys
Setbacks	Nil setback to all boundaries
Pedestrian access	Location of pedestrian access provides safe, legible and convenient access for residents, and strengthens the connection to the main street and POS
Vehicle access and servicing	No vehicle access is permitted from Kennedy Bay Entrance. Width of vehicle access to be kept to a minimum and integrated into the design of the building, reducing the impact on the streetscape.
Parking	Parking to be provided on the south western side of the site and to be concealed from the Main Street and POS areas. Car parking in this approximate location would allow for the siting of meaningful commercial, retail or residential building depths that address the main street. Where parking can't be provided on-site, options for reciprocal parking and/or allocating parking throughout the precinct may be considered (refer <b>Parking Strategy</b> ) across all non-residential lots. A reciprocal parking arrangement shall apply between Lot 1 and Lot 4. Reciprocal parking will be enforced by way of condition of development approval requiring public access easements on relevant Certificate of Titles.
Landscaping	<b>Non-Residential Development</b> Development to provide a minimum 10% deep soil area per site with a minimum dimension of 15m. The deep soil area is to be landscaped with a mix of large, medium and small tree sizes, for a minimum of 40% of the required deep soil area. <b>Residential Development</b> Refer to the R-Codes.

Site Plan



Streetscape

Provide a high quality landscaping interface to act as visual buffer between the carpark and adjoining development to the south-east.  
OR  
Development to front and provide an urban edge to Sea Beach Way, sleeving internal car parking.

Prominent Locations

*In addition to the requirements of Section 3, development is to address the following:*

Main Street Interface

Building facades should provide activation at the street level through a mixture of glazing, signage, lighting and external building fabric.  
Development should avoid blank walls/façades, and large extents of non-operable glazing.  
Preferred commercial uses on ground floor with residential on upper floors. Alfresco and uses that open out onto the street encouraged.

POS Interface

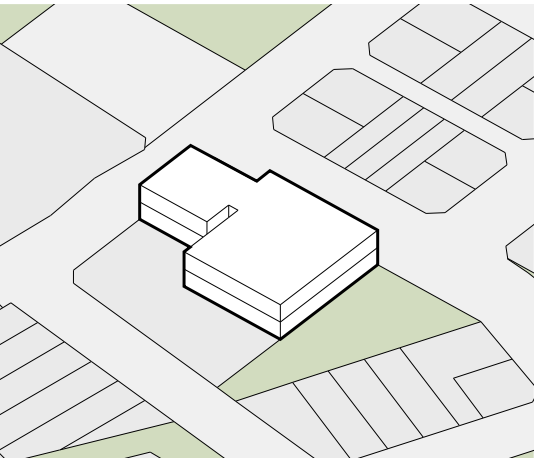
Development to provide passive surveillance of the public open space.  
Alfresco and uses that open out onto the public open space are encouraged.  
Development should demonstrate how CPTED design principles have been addressed through the design.

Site Location



- Preferred ground floor commercial
- Main Street Interface
- POS Interface
- Landscape edge
- Preferred pedestrian access
- Preferred vehicle access
- Vehicle access restrictions
- Preferred location for parking/service areas
- Indicative On-Street Car Parking

Indicative Built Form



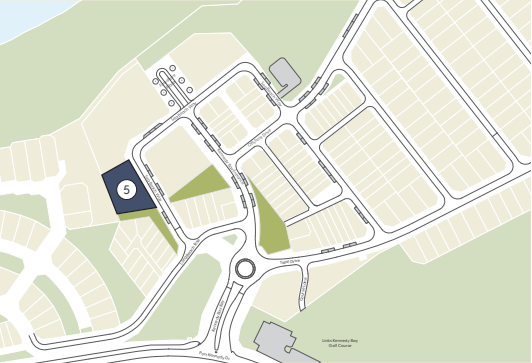
2.4 LOT FIVE

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Residential R80-R100
Preferred land use	Residential
Building height	<b>Minimum Height:</b> 7.5m minimum wall height (min 2 storey equivalent) <b>Maximum Height:</b> 3 storeys
Street setbacks	<b>Primary Street</b> 4.0m ground floor setback 3.0m upper floor setback Garages to be set back a minimum 4.5m.
Lot boundary setbacks	<b>Rear setback</b> 3.0m setback <b>POS setback</b> 1.5m <b>Boundary walls</b> 7m maximum boundary wall height to no maximum length to both side boundaries.
Streetscape	The finished level of all of the lots with interface to public realm must be elevated a minimum of 500mm and a maximum 600mm above the adjoining public realm. No visible retaining permitted in street setback. Development to provide passive surveillance of Sea Beach Way, with habitable rooms and balconies that front the street. Garages are permitted to a maximum of 70% of the frontage, provided: <ul style="list-style-type: none"><li>+ the garage has been well integrated into the design of the front facade;</li><li>+ the upper floor projects a minimum of 1m forward of the garage; and</li><li>+ a minimum one small tree and adequate soft landscaping within the street setback area is still able to be achieved.</li></ul>
Vehicle access	Garages and driveway widths are to be kept to a functional minimum, prioritising pedestrian access and landscaping within the street setback.
Waste	Refuse/storage areas are to be fully integrated with the design of the building. Refuse storage areas shall be screened from view from the public realm.
Overshadowing	No maximum overshadowing.
Privacy	No privacy provisions apply.

Site Plan



Site Location



- POS Interface
- Primary Dwelling/Development Orientation
- Secondary Dwelling/Development Orientation
- Indicative On-Street Car Parking

**Prominent Locations**

*In addition to the requirements of Section 3, development is to address the following:*

**POS Interface**

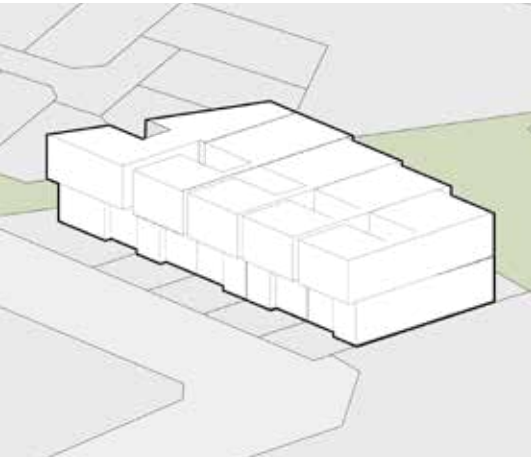
Visually permeable fencing of a consistent design is to be provided along the boundary of lots adjoining public open space.

Dwellings are to be designed to have a secondary orientation to the southern public open space, and incorporate at least one habitable room window to overlook the public open space.

Development should demonstrate how CPTED design principles have been addressed through the design.

Public realm to provide a 1.5m wide footpath between the lot and the public open space.

Indicative Built Form





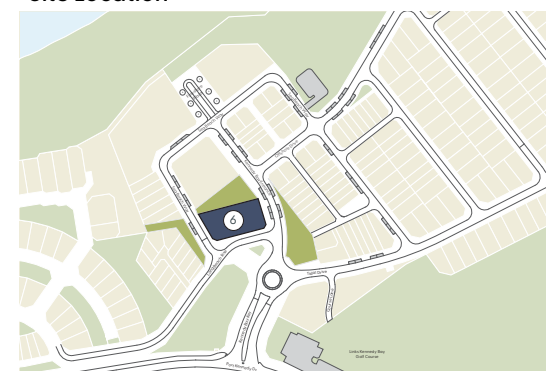
## 2.5 LOT SIX

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Commercial R-AC0; and Residential R80-R100
Preferred land use	Residential, mixed-use Opportunity for commercial and home businesses on ground floor fronting the main street and POS.
Private open space	Private Open Space is orientated toward the POS, optimising northern solar access into the dwelling and providing outlook and passive surveillance. All other Private Open Space requirements as per the R-Codes.
Building height	<b>Minimum Height:</b> 7.5m minimum wall height (min 2 storey equivalent) <b>Maximum Height:</b> 3 storeys
Street setbacks	2.0m ground floor setback (including garages) 1m upper floor setback
Lot boundary setbacks	<b>Boundary walls</b> 7m maximum boundary wall height to no maximum length to both side boundaries. <b>POS setback</b> 2.5m setback
Streetscape	The finished level of all of the lots with interface to public realm must be elevated a minimum of 500mm and a maximum 600mm above the adjoining public realm. No visible retaining permitted in street setback. Garages are permitted to a maximum of 70% of the frontage, provided: <ul style="list-style-type: none"> <li>+ the garage has been well integrated into the design of the front facade;</li> <li>+ the upper floor projects a minimum of 1m forward of the garage; and</li> <li>+ a minimum one small tree and adequate soft landscaping within the street setback area is still able to be achieved.</li> </ul>
Vehicle access	Garages are to be located as per the locations designated on the LDP. Minor variations to the locations may be permitted.
Waste	Refuse/storage areas are to be fully integrated with the design of the building. Refuse storage areas shall be screened from view from the public realm.
Overshadowing	No maximum overshadowing.
Privacy	No privacy provisions apply.

Site Plan

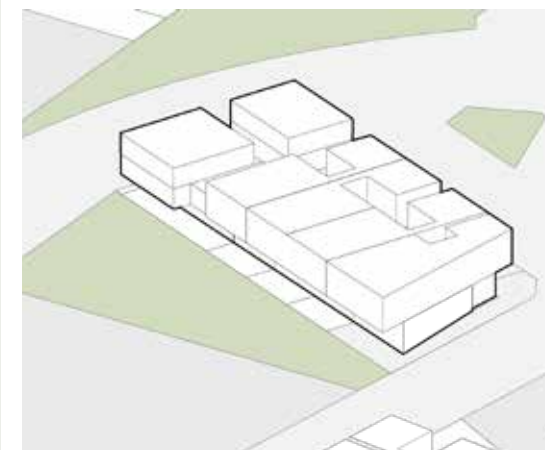


Site Location



- Commercial R-AC0
- Residential R80-R100
- Main Street Interface
- POS Interface
- Primary Dwelling/Development Orientation
- Secondary Dwelling/Development Orientation
- Vehicle access restrictions
- Indicative On-Street Car Parking

Indicative Built Form



### Main Street Interface

Building facades should provide activation at the street level through a mixture of glazing, signage, lighting and external building fabric. Development should avoid blank walls/façades, and large extents of non-operable glazing.

Preferred commercial uses on ground floor with residential on upper floors.

### POS Interface

Development to provide passive surveillance of public realm, with active uses (for commercial), primary living spaces and balconies that overlook the public open space.

Visually permeable fencing of a consistent design is to be provided along the boundary of lots adjoining public open space.

Development should demonstrate how CPTED design principles have been addressed through the design.

Public realm to provide a 1.5m wide footpath between the lot and the public open space.

### Prominent Locations

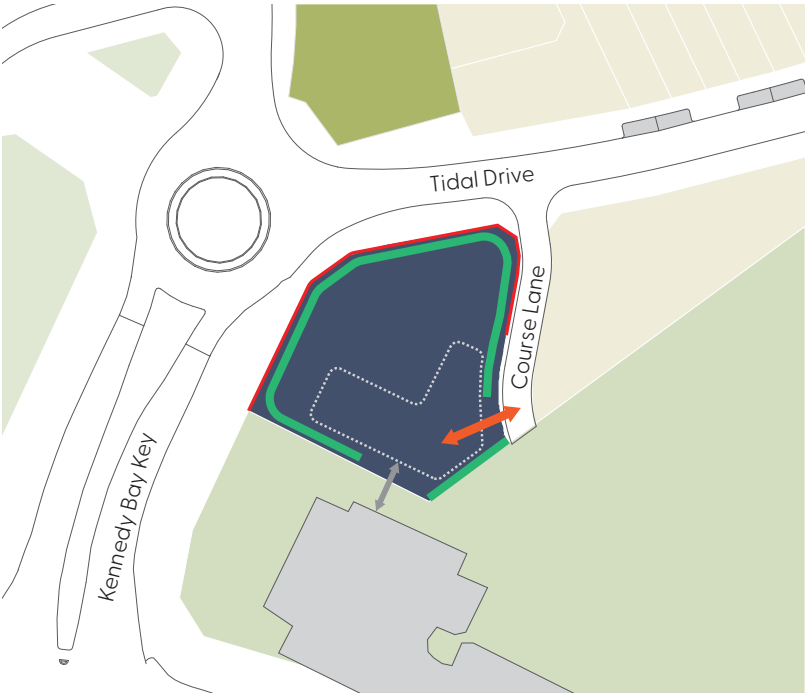
*In addition to the requirements of Section 3, development is to address the following:*



2.6 LOT TEN

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Commercial R-AC0
Preferred land use	Child care, gym, health studio, recreation – private
Building height	<b>Minimum Height:</b> 6m at top of roof height to provide a consistent height datum with nearby Kennedy Links Clubhouse. <b>Maximum Height:</b> 3 storeys
Setbacks	3m minimum setback to all lot boundaries
Vehicle access and servicing	Vehicle access to be provided off Lane 1 only, minimising the impact traffic during peak drop off and pick up times. No access is permitted from Kennedy Bay Key
Parking	Parking to be provided on the southern side of the site. Opportunity to link with existing parking to the south, increasing parking availability. Where parking can't be provided on-site, options for reciprocal parking and/or allocating parking throughout the precinct may be considered (refer <b>Parking Strategy</b> ) across all non-residential lots. Reciprocal parking will be enforced by way of condition of development approval requiring public access easements on relevant Certificate of Titles.
Landscaping	<b>Non-Residential Development</b> Development to provide a minimum 10% deep soil area per site with a minimum dimension of 1.5m. The deep soil area is to be landscaped with a mix of large, medium and small tree sizes, for a minimum of 40% of the required deep soil area. <b>Residential Development</b> Refer to the R-Codes.
Streetscape	<b>Landscape Edge</b> Development to provide high quality landscaping within the setback areas, to provide an acoustic and visual buffer between development and the surrounding streets <b>Street Address</b> Development to provide a built form response that addresses both streets (Kennedy Bay Key and Tidal Drive), providing visual connection and articulation, and avoids the use of blank walls facing the street.

Site Plan

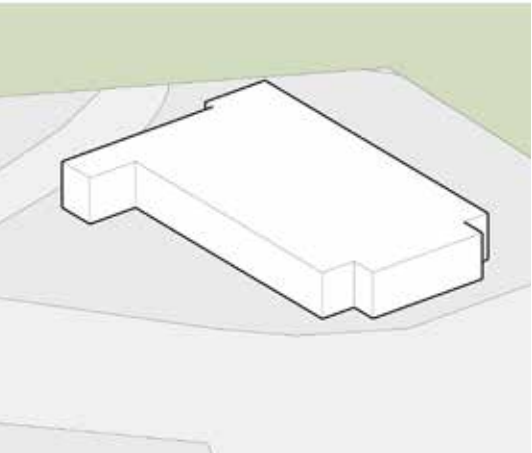


Site Location



- Landscape edge
- ↔ Preferred vehicle access
- Vehicle access restrictions
- ↔ Potential carpark access
- - - Preferred location for parking/service areas
- ▭ Indicative On-Street Car Parking

Indicative Built Form

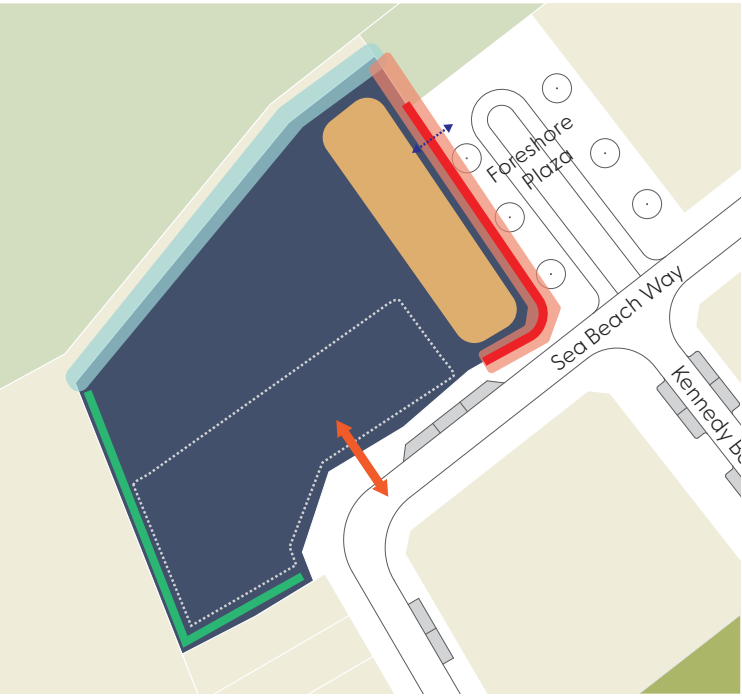


2.7 LOT THREE THOUSAND AND NINETEEN

**Advice Note for Lot 3019** – This lot is situated in a mixed-use precinct and is currently affected (or may in the future be affected) by noise generated from adjacent preferred/permissible land uses. Residents should expect ambient noise levels to be relatively higher both inside and outside residences, due to the potential proximity of entertainment venues.

DEVELOPMENT CONTROLS	
Structure Plan Zoning	Commercial R-AC0
Preferred land use	Commercial, cafe, restaurant, short-stay residential, tourism
Building height	<b>Minimum Height:</b> 7.5m minimum wall height (min 2 storey equivalent) <b>Maximum Height:</b> 3 storeys
Setbacks	3m setback to southern boundary. Nil setback to all other boundaries.
Pedestrian access	Location of pedestrian access provides safe, legible and convenient access for residents, and strengthens the connection to the Foreshore Plaza, main street and beach.
Vehicle access and servicing	Vehicle access to be provided from Sea Beach Way only. Width of vehicle access to be kept to a minimum and integrated into the design of the building, reducing the impact on the streetscape.
Parking	Centralising and concealing the impact of servicing and parking onto street edges, particularly away from the Foreshore Plaza. Car parking in this approximate location would allow for the siting of meaningful commercial, retail or residential building depths that address the adjoining streets. Where parking can't be provided on-site, options for reciprocal parking and/or allocating parking throughout the precinct may be considered (refer <b>Parking Strategy</b> ) across all non-residential lots. Reciprocal parking will be enforced by way of condition of development approval requiring public access easements on relevant Certificate of Titles.
Landscaping	<b>Non-Residential Development</b> Non-residential development to provide a minimum 10% landscaping per site with a minimum dimension of 1.5m. <b>Residential Development</b> Refer to the R-Codes.
Streetscape	<b>Landscape Edge</b> A high quality landscaping interface is to be provided as an acoustic and visual buffer between the carpark and adjoining residential development. <b>Street 2</b> Development to front and provide an urban edge to Sea Beach Way, sleeving internal car parking, OR Provide a high quality landscaping interface to act as visual buffer between the carpark and adjoining development to the south-east.

Site Plan



**Prominent Locations**

*In addition to the requirements of Section 3, development is to address the following:*

**Main Street Interface**

Building facades should provide activation at the street level through a mixture of glazing, signage, lighting and external building fabric. Development should avoid blank walls/façades, and large extents of non-operable glazing. Preferred commercial uses on ground floor with residential on upper floors. Alfresco and uses that open out onto the street encouraged.

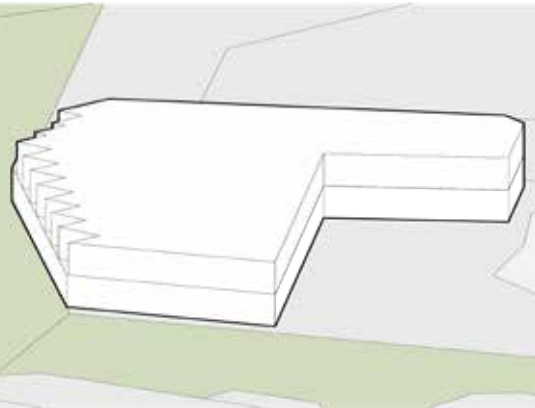
**Foreshore Interface**

Development should maximise northern aspect and views to the coast, without dominating the visual amenity of the foreshore area. Development should avoid blank walls/façades, and large extents of non-operable glazing. Provide an articulated facade that minimises long extents of wall or balconies and provides visual interest and relief. Preferred commercial uses on ground floor with residential on upper floors. Alfresco and uses that open out onto the foreshore area and foreshore plaza are encouraged, including a licensed alfresco area that leverages the relationship between the public and private realm (refer to the Kennedy Bay Landscape Masterplan).

Site Location

- Preferred ground floor commercial
- Main Street Interface
- Foreshore Interface
- Landscape Edge
- Preferred pedestrian access
- Preferred vehicle access
- Vehicle access restrictions
- Preferred location for parking/service areas
- Indicative On-Street Car Parking

Indicative Built Form



# 03 PROMINENT LOCATIONS

Prominent Locations are identified in **Figure 3** and reflect important entries, spaces and interfaces in the Kennedy Bay Coastal Village precinct where the proponent must give additional design consideration.

For each Prominent Location a 'character statement' and 'matters to be addressed' have been outlined to guide development outcomes and assist in the design review process. Photo examples and additional diagrams are also provided.

**Figure 3: Prominent Streetscapes Map**



## 3.1 MAIN STREET INTERFACE

### CHARACTER STATEMENT

The main street is comprised of a urban residential terraces to the north and mixed use development on the southern side, growing in intensity as the street moves towards the coast and foreshore area. The overall built character should reflect the relaxed, coastal village vision of the local centre, with a variety of colours, forms, materials and expressions that respond to the surrounding context. The character of all development facing the main street shall typically be fine-grain, providing a high level of pedestrian interest.

### MATTERS TO BE ADDRESSED

- + Demonstrate an architectural response that utilises roofs and other overhangs to shade the façade below to create comfortable pedestrian environments. These should act in tandem with street trees to provide shade to the edges.
- + Demonstrate a commitment to quality of architectural resolution suitable for important main street façades. This should include materiality that is self-weathering to develop and improve character with age.
- + Mixed-use façades facing the main street have both landmark and social importance for Kennedy Bay. Demonstrate how the building's edges carefully consider this.
- + The main street's mixed-use development sites can capitalise on northern sunlight. Demonstrate how the building responds to this, favouring alfresco, restaurant, bar or hospitality uses with appropriate solar control.



## 3.2 FORESHORE INTERFACE

### CHARACTER STATEMENT

The foreshore areas are envisioned as a combination of passive and active uses with large gathering spaces for community events and pedestrian/cycle networks accompanied by soft coastal planting (refer to the **Kennedy Bay Landscape Masterplan** in the Appendices). The built form character should not dominate the foreshore area, but sit respectfully in the coastal context. Active uses should work with the public spaces to attract, invite and provide amenity for the local community.

### MATTERS TO BE ADDRESSED

- + The northern corners are highly visible from key public realm spaces and the wider foreshore area. Demonstrate how these corners provides a strong appropriate architectural and landscape response.
- + Demonstrate how active commercial uses can activate corners and take advantage of the landscaped public spaces provided.
- + Development sites will offer prominent views to the coast, demonstrate how it will be seen as a high quality landmark design
- + Development fronting the foreshore edge can capitalise on northern sunlight. Demonstrate how the building responds to this, favouring alfresco, restaurant, bar or hospitality uses with appropriate solar control.



## 3.3 POS INTERFACE

### CHARACTER STATEMENT

The central POS area will be a vibrant town square, encouraging activity and vitality, providing a prominent arrival to the civic hub. The areas of POS provide relief and greenery throughout the built form and provide transition and separation between uses. The relaxed urban streetscape should blend and engage with the POS to provide amenity for residents and visitors, with the opportunity for buildings to open out onto areas of landscaping.

### MATTERS TO BE ADDRESSED

- + Demonstrate the building's strong landscape connection to the POS,
- + Demonstrate how interesting and legible pedestrian connections are created between the building and the POS.
- + Development to be designed in accordance with CPTED principles and ensure safety and amenity is maximised along building edges, with the opportunity for uses that can promote activity and use of the POS area.





kennedy bay





## KENNEDY BAY COASTAL VILLAGE TOWN CENTRE PARKING STRATEGY

Prepared by PJA (Updated June 2024)

# APPENDICES

## LOTS SUBJECT TO RECIPROCAL PARKING ARRANGEMENTS

Non-residential development within the 400m walkable catchment shown below are subject to reciprocal parking easements.

For lots 2 and 3, the reciprocal parking easement would only apply if non-residential development were proposed.

\*Possible reciprocal parking easement over Golf Course Car Park, subject to further investigation.

