

4. BUILDING HEIGHT

- Minimum height of 2 storeys or equivalent parapet height for a single storey building. This shall be achieved by incorporating a ceiling height that facilitates the future addition of a mezzanine floor, so long as the mezzanine floor can achieve a level of at least 3.2m above the ground floor.
- Where 2 or more storeys are proposed, a minimum ground floor to first floor height of 3.2m with a minimum 3.0m ceiling height must be provided.
- Buildings should not exceed 3 storeys.

5. BUILT FORM & ORIENTATION

- All buildings must be oriented to front public streets, with main entrances and substantial transparent glazing to the street to achieve active building frontages. Building entries to internal boundaries will not be supported, with the exception of staff access and servicing.
- Articulate façades and exaggerate individual tenancies by providing indentations in the floor plan to create interest. Break down broad façades or blank walls to create variety and interest through architectural design features. Blank walls facing roads and public parkland are not acceptable (extract from *Planning Policy 3.2.4 - Baldivis Town Centre*).
- The corner of Safety Bay Road and Norseman Approach, being Lot 20, must be provided with a landmark architectural design feature, built up to the corner and with a height of at least 8m for 2 storey buildings or equivalent and 11m for 3 storey buildings. The landmark element shall extend a minimum of 30% along the façade from the corner parallel to Safety Bay Road. A building entry on this corner is preferable but not mandatory. Other corners should be emphasised through the inclusion of an architectural design feature and the extension of height above the general parapet height.
- Where a building abuts a street, cantilevered verandahs must be provided for the full length of each façade. The verandah must have a width of at least 2.5m and achieve a clearance of 2.75m above the pavement level with sufficient lighting provided to the underside of the verandah. Refer to *City of Rockingham - Street Verandahs Local Law 2000*.

6. PEDESTRIAN ACCESS

- Where buildings are not built up to the street, provision must be made for dedicated, contiguous and well-defined pedestrian access paths between the building entry and the street. Where dedicated car parking spaces for disabled persons are provided, pedestrian paths can incorporate 'shared spaces' provided adjacent to dedicated spaces in accordance with AS/NZS 2890.6:2009.
- Where buildings are not built up to the street, provision must be made for a pedestrian path along the entire frontage of the building, designed as a 'continuous accessible path of travel' in accordance with AS 1428.1—2009, and linked to the street and adjoining sites. This path must be provided with shelter, such as an awning or verandah, for the entire frontage of the building.
- Separate pedestrian paths should be provided between the carpark and the street, to facilitate customers' safe access to building entries.

7. MATERIALS & FINISHES

- Durable and low maintenance materials are recommended.
- A combination of materials and/or finishes shall be incorporated to add visual interest.
- Building colours and materials should adopt a neutral, earthy palette. Corporate branding may be incorporated however only up to a maximum of 50% of the façade is permitted.
- Unfinished walls, including boundary walls, shall not be left exposed where in public view.

8. SIGNAGE

- Signage additional to corporate branding must be consistent in colour, size and font.
- All building proposals must include a 'sign strategy' in accordance with the *City of Rockingham Planning Policy 3.3.1, Control of Advertisements*.

9. SERVICE & STORAGE AREAS

- Delivery, loading and storage areas are to be screened from public view by an enclosure which is complementary with the style and materials of the primary buildings.
- The use of the building side adjacent to the north-south public access easement for service vehicle access and service areas (such as bin storage areas) is preferred.

10. VEHICLE ACCESS & CAR PARKING

- The "public access easement" must be maintained in accordance with the Deed of Easement. In addition, measures shall be implemented to ensure that the east-west easement remains a low speed environment to prevent vehicles using the easement as a shortcut between Goulburn Road and Norseman Approach.
- Direct vehicle access onto Safety Bay Road will not be permitted.
- Car parking spaces and parking aisles must be provided by each owner in accordance with the Plan.
- The development must meet the minimum and maximum car parking requirements as set out in Table 3 and clause 4.15.1.1 of *City of Rockingham Town Planning Scheme No.2*. The number of on-street parking spaces may contribute towards the parking required for the site.

11. LANDSCAPING

- Landscaping of the verges adjacent to each lot must be installed by the purchaser.

12. BICYCLE PARKING & END-OF-TRIP FACILITIES

- Bicycle parking and end-of-trip facilities must be provided in accordance with the *City of Rockingham Planning Policy 3.3.14, Bicycle Parking and End-of-Trip Facilities*.

DETAILED AREA PLAN

The purpose of this Detailed Area Plan is to identify the requirements of the *City of Rockingham Town Planning Scheme No.2 (TPS2), Planning Policy 3.2.4, Baldivis Town Centre* and the Integrated Development Guide Plan for the Baldivis Town Centre that apply to the land bound by Safety Bay Road, Norseman Approach, Edwin Link and Goulburn Road, Baldivis, and apply certain other requirements that are particular to this land.

1. GENERAL PROVISIONS

TPS2 Zoning	'Baldivis Town Centre'
Permitted Land Uses	Refer to TPS2 Zoning Table

2. PREFERRED LAND USE

Preferred uses for Lots 19 and 20 are:

- Showrooms
- Offices
- Service industries

Preferred uses for Lots 21 and 22 are:

- Offices
- Professional consulting rooms
- Residential
- Eating and drinking premises
- Leisure and recreation
- Non-core retail
- Showrooms
- Mixed-use buildings

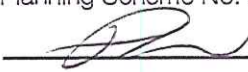
On Lots 21 and 22, the development of mixed use buildings, rather than single purpose buildings, is preferred.

3. BUILDING SETBACKS

Street Setbacks	Minimum	Maximum
Safety Bay Road (Lots 19 & 20)	One row of parking shall be provided as depicted on Plan.	
Norseman Approach (Lots 20 & 21)	Nil	1m
Edwin Link (Lots 21 & 22)	Nil	1m
Goulburn Road (Lots 22 & 19)	Nil (Lot 22)	One row of parking shall be provided as depicted on Plan. (Lot 19 only)

- All buildings are to be located within the Indicative Building Boundary as depicted on the Plan.
- All buildings shall have a contiguous frontage addressing each primary street.
- Minor indentations up to 1m are permitted to a maximum of 20% of the façades.

This Detailed Area Plan has been approved under delegation by the Manager, Statutory Planning under clause 4.23.1(c) of the City of Rockingham Town Planning Scheme No. 2:


Manager Statutory Planning

18 OCT 2011

Date