



RESIDENTIAL DESIGN CODE VARIATIONS

- The provisions of Town Planning Scheme No.2 (TPS2) and State Planning Policy 3.1 - Residential Design Codes (R-Codes) are varied as detailed within the Detailed Area Plan (DAP).
- The requirements of the R-Codes and TPS2 must be satisfied as provided in this DAP, except where provided in accordance with the City's Planning Policy 3.3.20 - Residential Design Codes.
- Consultation with adjoining or other landowners to achieve a variation to the R-Codes, as provided for by this DAP is not required.

STREET SETBACKS

- Primary Street Setback - 2.0m minimum to 4.0m maximum (no average setback applies).

LOT BOUNDARY SETBACKS

- Side Boundary walls - Side boundary walls not higher than 3.5m with an average of 3m, for 75% of the length of the boundary to both sides, behind the Primary Street Setback.

STREETScape

- Garages and Carports - 4.5m minimum from Primary Street.
- Garage doors and supporting structures must not exceed 60% of the frontage for two storey dwellings in accordance with clause 5.2.2 of the R-Codes.
- Garage width on a single storey dwelling must not exceed 3.5m.

OPEN SPACE

Lot Size	Minimum Open Space (% of site)	Minimum Outdoor Living Area (m ²)
261m ² - 300m ²	35%	24m ² uncovered with a 4m minimum dimension

SITE PLANNING AND DESIGN

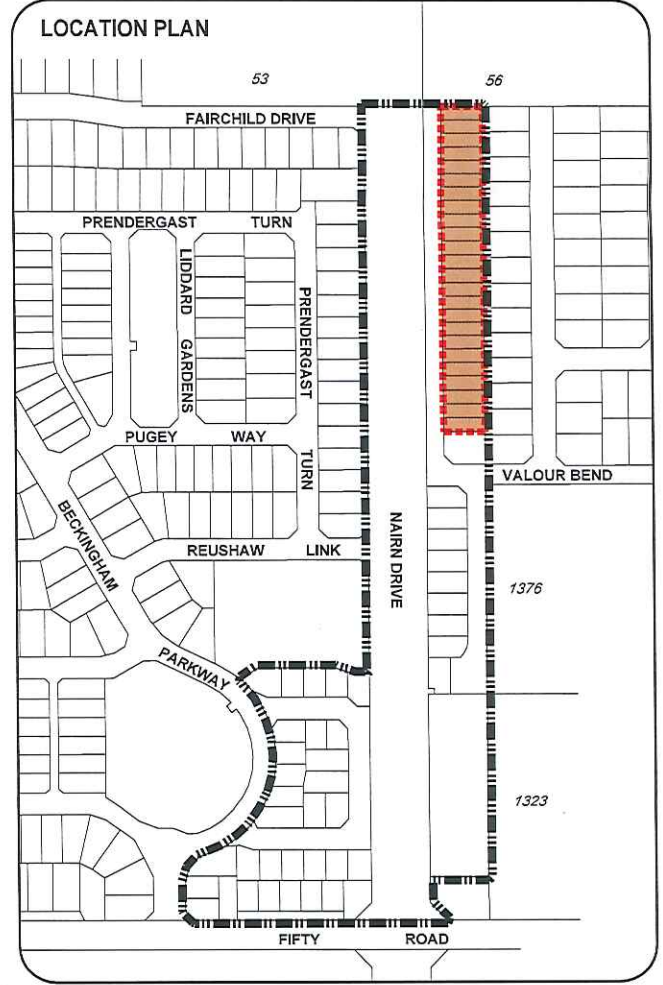
- Solar Access for adjoining sites - R-Codes requirements do not apply, other than where a lot abuts a lot
- greater than 350m² on its side boundary, in which case R-Code requirements apply.

OTHER PROVISIONS

Bushfire

- (i) Buildings on the lots identified as being Bushfire Prone Areas are to be constructed in accordance with AS3959. The approved Fire Management Plan, prepared by Strategen dated August 2014 and addendum dated 15th July 2015 requires the following Bushfire Attack Levels (BAL's)

Lot	BAL
498 - 499	BAL 19
500 - 504	BAL 12.5



LEGEND

- SUBJECT LOT
- LOTS SUBJECT TO THIS DETAILED AREA PLAN
- R40 LOTS
- PRIMARY STREET DWELLING ORIENTATION
- MINIMUM 2.0m PRIMARY STREET SETBACK LINE
- BUSHFIRE PRONE AREA
- FOOTPATH
- RETAINING WALL
- FINISHED LOT FILL LEVEL
- RECOMMEND GARAGE/CARPORT LOCATION
- STREET LIGHT LOCATION

0 10 20 30 40
 metres

CLIENT : BALDIVIS NORTH PTY LTD
 SCALE : 1:1,000 @ A3
 DATE : 17 July 2015
 PLAN No : 9009-4-001.dgn
 REVISION : D
 PROJECTION : PCG 94
 PLANNER : SLB
 DRAWN : BL

DETAILED AREA PLAN

Lot 9503 Fifty Road, BALDIVIS NORTH - STAGE 10C

CIVIC BOULEVARD
 WESTERN AUSTRALIA
 PO BOX 2142, ROCKINGHAM 6167 WA
 Telephone: 9258 0333 Fax: 9592 1705

DISCLAIMER

Information shown hereon is generated from data obtained from various sources.

The City of Rockingham cannot guarantee the currency, accuracy or completeness of any data and will not accept any responsibility for any errors or losses incurred as a result of the use of this information.

Users are advised to verify the currency, accuracy and completeness of the data with the source organisations or the relevant data custodian before making any decision.

This DAP has been approved under delegate authority by the City pursuant to clause 4.23.1(c)(i) of Town Planning Scheme No.2.

Manager, Statutory Planning,
 City of Rockingham.

21/7/15
 Date.

NOTES

This DAP was submitted to the City prior to the release of the RMD Codes.

This DAP was prepared by LB Planning.

Base data supplied by MNG / Water Corp.

Areas and dimensions shown are subject to final survey calculations.

All carriageways are shown for illustrative purposes only and are subject to detailed engineering design.