

Detailed Area Plan R-Codes Variations

The provisions addressed below relate to Baldvis Central Estate Lots 1234 - 1239 Colreavy Ramble (inclusive). The requirements of the Residential Design Codes are varied as shown on the Detailed Area Plan (DAP) and detailed in the following provisions. The requirements of the Residential Design Codes and the adopted Town Planning Scheme must be satisfied for all other matters:

- GENERAL PROVISIONS**
1. The prevailing Residential Design Coding is R30.
 2. The minimum open space requirement may be reduced from that specified in the Residential Design Codes to a minimum of 40% (i.e. 5% variation).
 3. All dwellings and ancillary development must be located within the building envelopes as depicted on the DAP and in accordance with the relevant 'SETBACKS' provisions.

- SETBACKS**
1. Colreavy Ramble shall be deemed the 'Primary Street Setback' for all lots.
 2. Setbacks for the dwelling, extensions and outbuildings shall be constructed in accordance with the following provisions:
 - i). Primary Street: 2.0 metre minimum dwelling setback
4.0 metre average dwelling setback.
 - ii). Side: Ground Floor:
A 1.5 metre minimum side 'solar' setback shall be provided along the northern boundary of all lots.
A nil (parapet) side setback is permissible on the southern boundary of lots where shown on the DAP. The side setback may be reduced to nil for 2/3 of the length of the boundary provided that the walls do not exceed a 3.0 metre maximum height.
First Floor:
A 2.0 metre minimum second storey 'solar' setback shall be provided along the southern boundary of all lots (except for corner lots with a southern boundary abutting a secondary street).
 - iii). Secondary Street: A 1.5 metre minimum setback shall be provided to all corner truncation and secondary street boundaries.
 - iv). Rear Laneway: 1.0 metre minimum to carports/garages, with no protrusion of eaves into the setback area
1.0 metre minimum setback to all other structures, including fencing above 750mm in height.

- CARPORT/GARAGE LOCATION**
1. All carports/garages must be accessed from the rear laneway. Access from Colreavy Ramble and/or secondary streets is prohibited.
 2. Lots 1234 and 1239 have designated (fixed) carport/garage locations, with the remainder of carport/garage locations being a recommendation only on the DAP.
- An alternative carport/garage location to the 'preferred' location shown on the DAP may be approved by the City of Rockingham's Manager, Statutory Planning, subject to a minimum setback of 1.0 metre from the rear boundary and the design meeting solar orientation principles, streetscape objectives and any other statutory requirements to the satisfaction of the City.

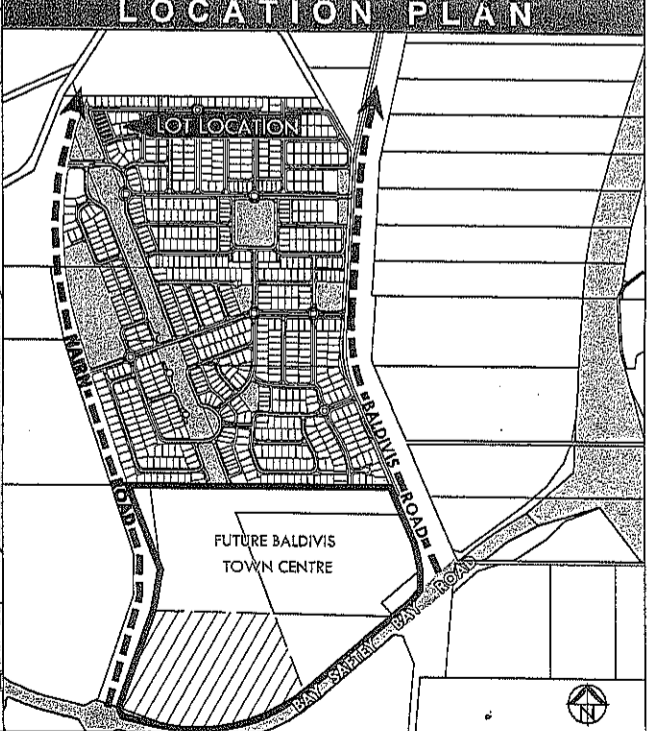
CONSULTATION
Consultation with adjoining or other landowners to achieve a variation to the Residential Design Codes provided for by the DAP is not required.

LEGEND

	Preferred garage location		Retaining Wall
	Designated garage location		No Access Strip
	Building Envelope		Feature Fencing
	Primary Dwelling Orientation		2m Second Storey Setback
			Staircase Access

This Detailed Area Plan has been approved by the Manager Statutory Planning.

Manager Statutory Planning 25/9/07
Date



Detailed Area Plan
Stage 15

