

NO.121 (LOT 2812) EXMOUTH DRIVE, BUTLER – CHILD CARE CENTRE

Form 1 – Responsible Authority Report

(Regulation 12)

DAP Name:	Metro Outer JDAP		
Local Government Area:	City of Wanneroo		
Applicant:	Mr Alessandro Stagno, Apex Planning		
Owner:	Fabcot Pty Ltd		
Value of Development:	\$2.25 million		
	☐ Mandatory (Regulation 5)		
	☑ Opt In (Regulation 6)		
Responsible Authority:	City of Wanneroo		
Authorising Officer:	Greg Bowering, Manager Approval Services		
LG Reference:	DA2022/1225		
DAP File No:	DAP/22/02339		
Application Received Date:	6 October 2022		
Report Due Date:	28 March 2023		
Application Statutory Process	90 Days with an additional 97 days		
Timeframe:			
Attachment(s):	Development Plans and Perspectives		
	2. Location Plan		
	3. ASP 87 Map		
	Main Street Realignment		
	5. Subdivision Plan		
	6. Deposited Plans		
	7. Design Review Panel Plans		
	8. Design Review Panel Minutes and		
	Applicant's Response		
	Traffic Impact Statement		
	10. Parking Management Plan		
	11. Access Statement		
	12. Waste Collection Plan		
In the Brown this A the M	13. Environmental Acoustic Assessment		
Is the Responsible Authority	☐ Yes Complete Responsible Authority		
Recommendation the same as the	⋈ N/A Recommendation section		
Officer Recommendation?			
	☐ No Complete Responsible Authority		
	and Officer Recommendation		
	sections		



Responsible Authority Recommendation

That the Metro Outer JDAP resolves to:

1. **Approve** DAP Application reference DAP/22/02339 and the accompanying plans (DA01/5, DA02/5, DA03/5, DA04/5, DA05/5, DA06/5, DA07/5 and supporting documents) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the City of Wanneroo District Planning Scheme No. 2, subject to the following conditions:

Conditions

- 1. Pursuant to clause 26 of the Metropolitan Region Scheme, this approval is deemed to be an approval under clause 24(1) of the Metropolitan Region Scheme.
- 2. The use of the premises is to be Child Care Centre as defined in the City of Wanneroo's District Planning Scheme No. 2 as follows:

"Child Care Centre: means premises used for the daily or occasional care of children in accordance with the Community Services (Child Care) Regulations 1988."

A change of use from that outlined above may require the approval of the City.

- 3. A maximum of **92 children** are permitted on the premises at any one time.
- 4. The hours of operation of the Child Care Centre must be between the hours of **6:30am to 6:30pm Monday to Friday**.
- 5. The development is to comply with the recommendations and measures outlined in the Environmental Noise Emission Report provided by Lloyd George Acoustics dated 3 October 2022. Recommended works must be completed prior to the commencement of the use.
- 6. Detailed landscaping and reticulation plans for the subject site, including landscaping on adjoining verges, must be lodged for approval by the City prior to commencement of any development. Planting and landscaping must be in accordance with the approved landscaping and reticulation plan and completed prior to occupation, to the satisfaction of the City.
- 7. Parking must be managed in accordance with the Parking Management Plan (attached) to ensure staff and visitor parking is appropriately managed.
- 8. Stormwater and any other water run-off from buildings or paved areas must be collected and retained on site.
- 9. The applicant is to demonstrate that onsite stormwater storage can accommodate major storm event (1 in 100 ARI) as per the City's requirements prior to issuance of a building permit, to the satisfaction of the City.



- 10. Detailed civil engineering drawings and specifications for works within the public road reserve (earthworks, parking, footpath, roads and drainage) must be lodged for approval to the City, prior to commencement of construction works. Construction works are to be undertaken in accordance with the approved engineering drawings and specifications to the satisfaction of the City.
- 11. All waste must be stored within the designated bin enclosure and collected from the site by a private contractor at the cost of the owner/occupier.
- 12. Detailed Waste Management Plan must be lodged for approval by the City prior to the occupation of the use. The plan is to detail how waste collection will be undertaken and managed to mitigate risks for both the private road, footpath users and waste contractors exposure to road traffic. The plan is to include, but is not limited to:
 - a. The frequency, duration, and location of waste collection:
 - Adequate measures be implemented during waste collection periods to minimise any adverse impacts associated with road traffic impacts on surrounding users;
 - c. Waste arrangements for contractors and sub-contractors;
 - d. Waste collection times;
 - e. Measures to minimise incorrect waste collection procedures; and
 - f. Any other matter required by the City.
- 13. All storage areas, external fixtures and building plant, including air conditioning units must be located so as to minimise any visual and noise impact on surrounding landowners and screened from view from streets, public places and adjacent properties to the satisfaction of the City.
- 14. Any illuminated signage must have any boxing or casing in which it is enclosed constructed of incombustible materials, must not comprise of flashing, pulsating, chasing or running lights, fluorescent, reflective or retro reflective colours or materials and must not have such an intensity as to cause annoyance to the public or illuminate beyond the extent of the lot boundaries.
- 15. Any graffiti applied to the external surfaces of the building must be removed within seven (7) days of it being applied, to the satisfaction of the City.
- 16. The applicant must undertake adequate measures to minimise any impacts of dust and sand drift from the site, to the satisfaction of the City.
- 17. A construction management plan must be submitted for approval to the City prior to an application for a building permit being made. The plan is to detail how construction of the development will be managed to minimise disruption to adjoining landowners. The plan will need to address the following:
 - a. The delivery times for materials and equipment to the site;
 - b. Storage of materials and the location and type of equipment on site;
 - c. Adequate measures be implemented during construction to minimise any adverse impacts caused by sand drift and dust from the site;
 - d. Parking arrangements for contractors and sub-contractors;
 - e. Construction times:
 - f. Measures to minimise noise impacts on surrounding residents; and
 - g. Any other matter required by the City.



The construction management plan is to be submitted to and approved by the City prior to the commencement of any development.

Advice Notes

- 1. This decision constitutes planning approval only and is valid for a period of four years (4) from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
- 2. In regards to the dust and sand drift condition, adequate measures to minimise any impacts of dust and sand drift from the site include all requirements as stipulated within the Department of Water and Environmental Regulation's 'A guideline for managing the impacts of dust and associated contaminants from land development sites, contaminated sites remediation and other related activities'.
- 3. The owner/applicant is to submit the "Certification of Compliance with Development Approval Conditions" form certifying that all of the conditions specified in the approval by the Council for the development of the land have been completed in accordance with the approved plans, and the certification is to be lodged with the Council within 14 days from the date of practical completion, and applies to all of the conditions, except for those conditions relating to ongoing compliance.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme			
Region Scheme Zone	Urban			
Local Planning Scheme	District Planning Scheme No. 2			
Local Planning Scheme Zone	Urban Development			
Structure Plan	Butler-Ridgewood Agreed Structure Plan No. 27			
Activity Centre Plan	Butler District Centre Activity Centre Agreed Structure Plan No. 87			
Structure Plan Zone	Centre			
Activity Centre Plan Zone	Commercial			
Use Class and permissibility:	Child Care Centre (D)			
Lots Size:	2215m ²			
Existing Land Use:	Vacant land			
State Heritage Register	No			
Local Heritage	⊠ N/A			
	□ Heritage List			
	☐ Heritage Area			
Design Review	□ N/A			
	□ State Design Review Panel			
	□ Other			
Bushfire Prone Area	No			
Swan River Trust Area	No			



Proposal:

The application proposes a Child Care Centre (CCC) located within the western half of Lot 2812 (development site), comprising the following:

- A single storey building with approximately 820m² of floor area;
- A maximum number of 92 children and 22 staff on site at any one time;
- No on-site parking bays proposed. The CCC will utilise the existing parking bays to the south side of the building;
- Operational hours of 6:30am to 6:30pm Monday to Friday; and
- Associated landscaping and signage.

The eastern half of the development site is proposed to include temporary landscaping as an interim approach, until such time that it is developed with a suitable/complementary land use.

The development plans and perspectives for consideration are included as **Attachment 1**.

Background:

The subject site is zoned "Urban Development" under the City's District Planning Scheme No. 2 (DPS 2), "Centre" under the Butler – Ridgewood Agreed Structure Plan No. 27 (ASP 27) and "Commercial" under Butler District Centre Activity Centre Structure Plan No. 87 (ASP 87).

The site is a triangular shaped lot bound by a Transperth 'Park n Ride' car park to the north, Exmouth Drive to the east, an internal 'Connector Road' to the west (Clipstone Parkway) and the Butler Central shopping centre car park to the south. The internal 'Connector Road' and the Butler Central shopping centre car park form part of Lot 2811.

The site is located immediately adjacent to the Butler Train Station to the east with associated high frequency bus stops, existing Supermarkets and a number of smaller retail tenancies to both the south and south-west of the subject site and multiple dwelling apartments are located to the west.

A Location Plan is included as Attachment 2.

Butler District Centre Activity Centre Structure Plan No. 87

ASP 87 was adopted in February 2013 and covers a linear area from Marmion Avenue to the west to the future Mitchell Freeway to the east along Butler Boulevard. ASP 87 is divided into six precincts, identified as Precincts A to F with the subject site contained entirely within Precinct A.

ASP 87 Structure Plan Map has been included as **Attachment 3**.

Previous JDAP approvals

Precinct A, being the 'centre core' area within ASP 87 has been subject to a number of previous JDAP approvals.



JDAP previously approved a large scale development on 26 May 2016 (JDAP ref: DAP/15/0964) for a Supermarket, Liquor Store and a number of small retail tenancies. The Supermarket and retail tenancies are located south of the subject site adjacent to Butler train station, while the Liquor Store is on the corner of Butler Boulevard and Camborne Parkway. It is important to note that in accordance with DAP/15/0964 the Main Street, as identified on the ASP 87 map, has been reoriented from a north-south linkage connecting Butler Boulevard with Clipstone Parkway, to an east-west linkage (Clayton Street) connecting the Butler train station with Camborne Parkway. Included in **Attachment 4** is an approved site plan from JDAP application DAP/15/0964 which defines the reorientation of the Main Street through the determination of that development application rather than through an amendment to the Structure Plan.

More recently, approval was granted by the JDAP on 8 June 2020 for a large format retail development that comprises six (6) separate buildings and 19 tenancies including uses such as showrooms, shops and a recreation centre (JDAP ref: DAP/19/01717). That development is located to the south-west of the subject site and has been designed to align with the east-west Main Street configuration.

Subdivision Application

On 27 February 2017, the Western Australian Planning Commission (WAPC) granted subdivisional approval (WAPC ref: 154564) for the creation of five (5) freehold lots from the parent lot, at the time being Lot 2810 (150) Camborne Parkway, Butler. The purpose of the subdivision was to facilitate the development of the JDAP approval (JDAP ref: DAP/15/0964 and later subsequent modification ref: DAP/15/00786) which includes the Shopping Centre and Liquor Store situated within Lots 1 & 5 of the subdivision plan. Lots 2, 3 & 4 depicted on the subdivision plan were identified on the JDAP approvals as future development sites.

A condition of the subdivision approval required a public access easement for vehicle access, circulation areas, car parking, access ways and pedestrian access to facilitate vehicular access between the 'main' lot (Lots 1 and 5) and the undeveloped vacant 'pad-sites' (Lots 2, 3 & 4). It is noted that the abovementioned condition has been executed and cleared.

A copy of the approved plan of subdivision (WAPC ref: 154564) is included as **Attachment 5**.

The configuration of the subject site is irregular in shape, which can be seen to have very limited capability for access to Exmouth Drive, and landlocked on the northern, western and southern boundary. These factors indicate that the subject site was created with an underlying intent to utilize the shared car parking on adjoining Lot 2811 (161) Butler Boulevard, Butler secured by way of permanent access easements, as seen in Deposited Plans included as **Attachment 6**. The legal right to access and parking between the subject site and the adjoining site Lot 2811 (161) Butler Boulevard, Butler is considered in the Traffic and Parking section of this report.

Local Development Plan

In accordance with clause 2.1 of the General Planning Requirements for ASP 87 a Local Development Plan (LDP) is required to be prepared prior to a development application being submitted. Clause 2.4 does allow the City to deem that a development application constitutes an LDP where the development addresses the



site's context, integration and co-ordination with the surrounding built form, as well as the applicable development requirements of the structure plan.

The City considers that this application has demonstrated a form of development that addresses the sites context, integration and co-ordination with the surrounding built form as identified within ASP 87 and as such, negate the need for the preparation of an LDP, for the following reasons:

- The application is seeking approval for built form and land uses which are mutually inclusive to the intent of ASP 87, SPP 4.2 and transit-oriented development principles; and
- The building on the southern and western boundary of the subject site and is separated from the adjoining site by approximately 1.5 metre walkway, encouraging co-ordination with the development to the south.

Legislation and Policy:

Legislation

Metropolitan Region Scheme (MRS) District Planning Scheme No. 2 (DPS 2)

State Government Policies

State Planning Policy 4.2: Activity Centres for Perth and Peel (SPP 4.2)
State Planning Policy 7.0: Design of the Built Environment (SPP 7.0)
State Planning Policy 5.4 Road and Rail Noise (SPP 5.4)
WAPC Planning Bulletin 72/2009: Child Care Centres (Planning Bulletin 72/2009)
WAPC Transport Impact Assessment Guidelines Volume 4 Individual Developments

Structure Plans/Activity Centre Plans

Butler – Ridgewood Agreed Structure Plan No. 27 (ASP 27) Butler District Centre Activity Centre Agreed Structure Plan No. 87 (ASP 87)

Local Policies

Local Planning Policy 2.3: Child Care Centres (LPP 2.3) Local Planning Policy 4.23: Design Review Panel (LPP 4.23) Local Planning Policy 4.6: Advertising Signs (LPP 4.6)

Consultation:

Public Consultation

The application was advertised for a period of 14 days as required under Clause 64(4) of DPS 2 and the Deemed Provisions, commencing on 24 October 2022 and concluding 7 November 2022.

Advertising was undertaken by way of letters to surrounding landowners/occupiers within approximately 200 metres of the subject site, a sign erected on site, a notice placed in the local newspaper and all development plans and reports being made available on the City's website.



Following completion of the advertising period, one (1) submission was received in support of the proposal. A summary of the submissions has not been provided as the submission did not include any comments.

Referrals/consultation with Government/Service Agencies

Design Review Panel

In accordance with LPP 4.23, the proposal was presented to the City's Design Review Panel (DRP) on 24 March 2022 prior to lodgement.

The proposal received generally positive feedback, referring to the centre as having a high-quality design, carefully integrated into its immediate context and providing high levels of amenity to occupants and visitors alike. The DRP did raise concerns regarding Principle 6 - Amenity and advised that the DRP's support is contingent on the following items being amended:

 All habitable spaces, including cot rooms to have access to immediate natural light and ventilation.

Following the meeting and prior to lodging the application, the following modifications were made to the plans in an effort to address the DRP's comments:

 All activity rooms and cot rooms have been relocated to have direct access to external doors and windows, which now provides access to natural light and ventilation. This includes the pop up skylight features on the western and southern wings of the building.

The original plans submitted for design review are included as Attachment 7.

The DRP minutes and the applicant's response to SPP 7.0 and the DRP's comments are included as **Attachment 8**.

It is noted that the original plans submitted to the DRP differ from the current plans at **Attachment 1**, with a different operator now secured for the Child Care Centre premises. Whilst the built form and scale of the proposal have remained consistent with the original plans, the amendments have resulted in changes to the assessment under Principle 1: Context and Character and Principle 10: Aesthetics of SPP 7.0, noting that the finish, colour, and pattern to the external façade of the Child Care Centre building have been modified in the amended plans.

Whilst the external finishes of the Child Care Centre building have been altered, the modifications reflect the branding of the new Child Care Centre operator. The materials, physical built form and scale of the proposed Child Care Centre remain unchanged. The City considers the modifications made by the applicant address the design-based concerns raised by DRP whilst delivering a high quality finish. The amended design, noting the change in intended operator suitably addresses the comments of the DRP.

Planning Assessment:

An assessment has been carried out against the provisions of LPP 2.3, ASP 87 and DPS 2. The following matters have been identified as key considerations for the determination of this application:



- Zoning & Land Use Permissibility;
- Traffic and Parking;
- Pedestrian Accessibility;
- Waste collection;
- Built Form;
- Noise;
- Landscaping; and
- Signage.

Zoning & Land Use Permissibility

The subject site is contained entirely within Precinct A of ASP 87, which is located in close proximity to the Butler train station. The objectives of Precinct A are outlined below:

- 1. Encourage a mix of intensive uses and treatments compatible with the rail station including:
 - a) uses that generate activity outside core business hours;
 - b) town squares and other open spaces as community meeting spaces;
 - c) retail uses such as specialty stores and supermarket uses;
 - d) enable the opportunity for temporary activities in the street, such as alfresco dining and external displays.
- Facilitate strong and direct pedestrian connectivity between the station and the Main Street, as well as ease of pedestrian movement across the Main Street.
- 3. Allow on street parking where possible and encourage on-site parking to be located behind buildings adjoining the Main Street and Exmouth Drive.
- 4. Allow for a combination of public and private realm spaces for community meeting, pedestrian activity and traffic flows

Child Care Centre is a 'D' or discretionary use in the Commercial zone.

The site is located immediately adjacent to Butler Train Station and bus port with the associated high frequency bus services within walking distance of the site, providing a high level of accessibility from surrounding areas. The development takes the form of a single story building including a landmark feature and external finishes expected within an activity centre.

The proposed Child Care Centre provides a community benefit and is an urban support service that is appropriately located within the centre core, and in close proximity to residential areas consistent with the locational criteria of LPP 2.3. The incorporation of a Child Care Centre in the town centre is considered appropriate as it contributes toward a mixture of different uses for the Butler community. Whilst the Child Care Centre does not operate outside core business hours, the hours of operation encourage and allows for residents to utilise the facility for multi propose trips given its convenient location in proximity to the existing shopping centre and train station.

The Child Care Centre has also been designed and configured to respond to the site context, situating high noise generating areas (outdoor play areas) away from the existing shopping centre, mimicking a "Main Street" built form outcome along the boundaries of the site and delivering a compact urban form that supports a reduction in travel demand, which is consistent with *State Planning Policy 4.2: Activity Centres*



for Perth and Peel. The eastern half of the development site contains an interim landscaping area which may allow for a combination of public and private realm spaces, and includes a proposed pedestrian pathway that directly connects between the station and the Centre, maximising the connectivity between the Child Care Centre and the train station.

The proposed Child Care Centre satisfies the objectives of Precinct A and has been designed to minimise potential impacts on the amenity of the nearby residential and commercial properties. Noise, parking and traffic impacts are discussed in the sections below.

Traffic and Parking

Traffic

In support of the proposal, a Traffic Impact Statement (TIS) was submitted with the application which carries out an assessment in accordance with the WAPC Transport Impact Assessment Guidelines (2016). The application includes the following findings:

- The Child Care Centre is anticipated to generate 82 vehicle trips in the AM peak period, and 66 vehicle trips in the PM peak period, resulting in an insignificant impact to the surrounding road network;
- The Child Care Centre is unlikely to generate significant additional vehicle trips on the road network as many of the vehicle trips for drop-off/pick-up would be part of a linked trip already being made on commute given the Centres location within an Activity Centre.

The City's Traffic and Transport Services have reviewed the TIS and have confirmed that the traffic generated from the development is not likely to have a significant impact to the surrounding road network, noting that site AM peak would occur before road network peak and the site PM peak is dispersed.

A copy of the TIS is included as **Attachment 9**.

Access to the Child Care Centre will be via the adjoining Lot 2811 (161 Butler Boulevard) through an existing legal right of carriageway identified as Access Easement 'C' on **Attachment 6**.

This easement provides a right of carriageway over existing driveways and parking areas within adjoining Lot 2811 to the west and south "with the full and free right, liberty, power and authority from time to time and at all times to go, pass and repass for all purposes and either with or without vehicles over, along and across the Easement Area".

Parking

The application proposes no onsite bays where a total of 37 bays are required in accordance with LPP 2.3.

The Centre proposes to utilise the existing Butler Centre car park on Lot 2811 (161) Butler Boulevard) which is located immediately south of the site to offset all 37 bays. The site has legal access to utilise and park vehicles including employees and



customers/visitors, secured by way of a Parking Easement identified as 'Z' on **Attachment 6**.

This easement relates to existing driveways and parking areas within Lot 2811 to provide "full and free right, liberty, power and authority from time to time and at all times for its authorised officers, employees, agents and other persons from time to time authorised by it (including without limitation customers and visitors of the businesses located on the Parent Lot) to go, pass and repass and to park vehicles, but not permanently or indefinitely, in bays marked for that purpose, over, along and across the Easement Area".

A parking calculation has been conducted for Lot 2811 to ensure that the proposed development will not compromise the availability for the existing parking bays to provide for the already approved land uses.

Analysis of JDAP approval (ref: DAP/15/00786) and subsequent City granted approvals have identified a car parking surplus of 138 bays. However this parking surplus was calculated over the original parent lot, which has since received WAPC approval to subdivide into individual freehold lots. Despite the subdivision approval, an easement still exists over the original parent lot which allows the newly subdivided lots to maintain the right of carriageway and public access easements over the existing parking areas, as seen in **Attachment 6**. This results in no changes to the calculated parking availability. Whilst sufficient bays are provided over the entire site, the City acknowledges that the utilisation of parking bays for this application will be segregated towards the north eastern portion of the Butler Centre car park (adjoining southern carpark of the subject site).

Nevertheless, whilst these existing parking bays are for the purpose of public use, the application proposes no onsite bays in lieu of the required 37 bays. This shortfall has been considered with regard to the following:

- The TIS notes that the activity centre location should result in a reduction in parking demand and concludes that the availability of parking bays within the adjoining Butler Central site should cater for the needs of the proposed development;
- The locality is well-serviced by public transport, noting that the Butler train station is located adjacent to the site in addition to the associated high frequency bus services;
- The site is within an activity centre and complements various non-residential land uses, creating opportunities for multi-use trips which may also reduce parking demand;
- The nature of Child Care Centre's typically operates on a pick-up/drop-off basis high turn-over rate and this results in a higher frequency of vacant parking bays;
- It is anticipated that a significant portion of parents/caretakers and staff will walk to the Child Care Centre or use alternative transport modes; and
- The proposal includes the provision of footpaths to connect the site from the northwest capturing the existing residential area and Butler train station to the east to facilitate access connection from the alternative transport modes.

Due to the alternative transport options, sufficient parking secured by legal agreements and the availability of public bays the proposed parking arrangements are supported.



The applicant has also provided a Parking Management Plan (PMP) to ensure that offsite parking arrangements are effectively managed. This includes:

- Allocation of parking bays to the entrance of the Centre will be available for parent pick-up/drop off use; and
- Operational details regarding staff shift handover times over non-peak periods.

The proposed PMP has been included as **Attachment 10**.

The PMP is considered to be appropriate for the development. It is recommended conditions be imposed to limit occupancy numbers for children and the future operation of the Child Care Centre to be undertaken in accordance with the PMP.

Pedestrian Accessibility

As previously mentioned, the site is landlocked on three boundaries (north, west and south), with the western and southern boundary bounded by Lot 2811 (161) Butler Boulevard, Butler. The original Child Care Centre design presented to DRP (**Attachment 7**) included a pedestrian connection between the existing pedestrian footpath on the western boundary and immediately in front of the entry to the building, delivering a direct access linkage to the Child Care Centre. However, this direct pedestrian linkage has since been relocated to the most north western corner of the site within the verge as landowner(s) consent to facilitate works on Lot 2811 was not able to be obtained.

In the context of addressing the "Legibility" of design principles of SPP 7.0, the applicant has modified the proposal to incorporate the following design considerations:

- Incorporation of a pedestrian pathway within the western side of the site; and
- Addition of an "Entry" located above the new pedestrian pathway facing the access street.

The incorporation of a pedestrian pathway and integration of an "Entry" sign above the new pedestrian connection does assist in improving legibility to the Centre's front entry for staff/customers arriving from the existing north western residential area, however it does not provide a recognisable or definable entry path for staff/customers utilizing the existing Butler Centre car park to the south. It can be also noted that the proposal includes the addition of bollards along the length of the southern boundary where the site abuts existing parking bays to prevent vehicles overhang and encroachment over the pedestrian footpath. However as an indirect consequence, the addition of bollards also restricts direct access from the southern carpark to the Centre for staff/customers requiring pram or wheelchair access. As such, pedestrian access from the southern carpark to the entry of the Centre can only be gained via the existing footpath either side of the carpark that connects to the proposed internal pedestrian pathway.

Furthermore, the development proposes to utilise the existing ACROD bays from the adjacent tenancy, which is located approximately 50 metres from the entry of the building (measured in a straight line from the ACROD bays to the entry). Whilst the applicant has provided a statement (**Attachment 11**) advising that the dimensions of the existing ACROD bays and associated access paths to the principal pedestrian entry will meet the requirements set out in the Disability (Access to Premises – Buildings) Standards 2010, National Construction Codes Volume 1 (2019) and relevant Australian Standards, the City acknowledges that the access routes available



to the entry of the Centre is undesirable and provides no clear direct or definable pedestrian routes.

The City acknowledges the proposed pedestrian access into the entry of the building is inefficient and impractical. Legibility of the development is pampered by the limited ability to undertake works on Lot 281. However, the applicant has attempted to mediate with the private landowner(s) of Lot 2811 on several occasions but has been unsuccessful. Recognising that the applicant has exhausted all avenues in trying to reach an agreement with the adjoining landowner(s), the outcome that has been presented for determination is supported.

Waste collection

The proposed development provides an internalised bin storage area at the north-western corner of the building, with waste receptacles proposed to be transported to the verge and collected by a private contractor along the private internal road (Cliptone parkway), as depicted in **Attachment 12**. The applicant has advised that a waste contractor will enter into a private agreement to provide services.

A Waste Management Plan has been provided (**Attachment 12**) in support of the waste collection arrangements.

It is recommended that the Waste Management Plan be approved as a condition of development approval subject to:

- Frequency and hours of operation;
- Duration and location of collection;
- Risk based assessment on impact for private road users footpath users and waste contractors;
- Any potential conflict at pedestrian crossing point;
- Any additional delivery of goods; and
- Any other matter required by the City.

Built Form

The table below outlines the relevant planning requirements which relate to the entire ASP area.

Butler District Centre Activity Centre Agreed Structure Plan No. 87			
Requirement	Proposal		
Development on landmark sites are to be designed in a manner that recognises the site's strategic location.	ASP map identifies the north-western portion of the site as a landmark site. A landmark feature has been proposed that incorporates elements of height, articulation, colour and materials.		
Service areas are to be predominately screened from public view.	Service areas are internal to the building and are screened from the public view.		
The minimum provision of a footpath on both sides of all streets, unless otherwise designated ASP map.	A footpath already exists along the western side of the site. A 1.5 metre pedestrian footpath has been proposed along the southern and western boundary of the site.		



Precinct A

ASP 87 envisages Precinct A to be a high density, high amenity activity centre that delivers on "Main Street" principles. It is noted that the site does not abut the "Main Street" designated under ASP 87 (**Attachment 3**) or the realigned "Main Street" determined though the JDAP approval (**Attachment 4**), and as such is not subject to the "Main Street" built form considerations. The application is considered to be consistent with ASP 87 for the following reasons:

- The proposal offers pedestrian links that support alternative modes of transport;
- The amount of glazing proposed along the southern boundary provides surveillance and visual interaction between the Centre and public realm;
- The building has a continuous frontage that adds amenity and interest from the existing shopping centre;
- There are opportunities for temporary activities that may be used in conjunction with the Centre, in the form of the interim landscape area, contributing towards a sense of vibrancy and activity; and
- The design of the proposal (reciprocal use of access and car parking) allows for the ability for parents/caregivers to utilise the existing land uses within the area;
- The proposal provides for a street edge setback of between nil to 1.5 metres along the western and southern boundary, which provides shade devices.

Noise

The applicant has provided an Acoustic Assessment, which concluded that the proposal will be fully compliant with the *Environmental (Noise) Regulations 1997* and accordingly there is considered to be no impact on surrounding land uses.

In addition, the site is located within close proximity to an existing passenger railway, being Butler Train Station. In accordance with *State Planning Policy 5.4 Road and Rail Noise*, where a noise-sensitive land use is proposed within the policy's trigger distance of specified transport routes, additional works/measures may be required to be undertaken on the development to minimise any adverse impact of road and rail noise on the noise-sensitive land-use. The Acoustic Assessment also includes an assessment against *State Planning Policy 5.4 Road and Rail Noise*, which concluded that the noise intrusion from the passenger railway is below outdoor noise target of State Planning Policy 5.4 Road and Rail Noise and therefore the proposal is acceptable from an acoustic point of view.

It is recommend that a condition be imposed requiring the recommendations in the Environmental Acoustic Assessment to be implemented and maintained to ensure ongoing compliance with the *Environmental Protection (Noise) Regulations* 1997.

The Environmental Acoustic Assessment has been included as Attachment 13.

Landscaping

Landscaping requirements are considered in accordance with Clause 4.17 of DPS 2, as outlined below. The applicant has also provided a Landscape Plan which is included at **Attachment 1**.



Requirement	Proposal	Assessment
Minimum 8% landscaping on the lot.	Excluding temporary parklet area Approximately 470m ² or 21.2% landscaping.	The proposed landscaping meets the City's requirements.
Site Area: 2,215m ²	1 0	
8% = 177.2m ²	Including temporary parklet area Approximately 700m² or	
	31.6% landscaping.	
1 tree per 4 bays	0 Parking bays proposed = 0 shade trees required	Provision of trees is not required as the proposal does not include any car parking bays being constructed on Lot 2811 (161) Butler Boulevard)
		1 tree per 4 bays have already been provided along the southern boundary on Lot 2811 as per DAP/15/00786.

<u>Signage</u>

The proposal has been considered against the provisions set out within the City's Local Planning Policy 4.6 – Advertising Signs (LPP 4.6). The proposal includes a total of six (6) wall signs within the development area. For ease of reference, each wall sign has been allocated a number shown on the elevation plan in **Attachment 1**. The proposed Wall Signs have been considered against LPP 4.6 below:

LPP 4.6 – Advertising Signs Provision (Wall Sign)

Provisions	Proposal
Be limited to a maximum of one sign	Northern Elevation: 1 x Sign
per tenancy, per street front	Eastern Elevation: 1 x Sign
	Southern Elevation: 2 x Signs
	Western Elevation: 2 x Signs
Not extend laterally beyond either	All six wall signs within building façade
end of the wall or protrude above	boundaries.
the top of the wall.	
Not exceed 25% in aggregate area	All six wall signs do not exceed 25% of the
on any one wall to a maximum of	area or 8sqm.
8sqm.	
Be integrated with the building	All six wall signs are considered to be
design	integrated well into the building design,
	matching with the proposed colours and
	finishes.

All six (6) wall signs are supported for the following reasons:

 Signs have been located in areas that include significant amount of blank space and as such partially soften the visual impact of bulk;



- The amount of signs per street frontage is not considered excessive in comparison to the length of building frontage and they are sufficiently spaced out so as not to not represent clutter;
- The signs are considered appropriate in size and have been integrated with the development; and
- The signage is incorporated into the design of the building and comprises colours, design and styling entirely consistent with the architectural approach adopted for the development.

The proposed signs are considered appropriate due to their size, distribution in the context of the site, with all content relating to the operation and branding of the proposal. The signs integrate well with the development and have not considered to be obtrusive, excessive or negatively impact or detract from the amenity of the surrounding area.

Conclusion:

The development application for a Child Care Centre is generally compliant with the relevant legislation and planning requirements.

The proposed Child Care Centre use is appropriate and compatible within the existing area and is a complementary land use in the Activity Centre, encouraging multi-use trips. The Child Care Centre is situated near convenient and different modes of transport, providing the ability to better utilise the existing transport infrastructure, as intended within an activity centre. The proposed Child Care Centre is also supported by technical traffic and acoustic reports.

The site is landlocked by private property on the southern boundary and attempts by the applicant to facilitate direct access to the Child Care Centre from the adjoining lot has been unsuccessful. The City acknowledges that the legibility of the Child Care Centre is not ideal for users of the Centre, however consent from the adjoining landowner(s) is not able to be obtained. On balance the poorly configured pedestrian access would not unreasonably impact the functionality of the Child Care Centre so as to warrant a different decision.

It is therefore recommended that the proposed development be approved, subject to conditions.

Attachment 1 - Development Plans PROPOSED CHILDCARE CENTRE 121 EXMOUTH DRIVE, BUTLER, W.A.



DRAWING REGISTER PLANNING

SHEET NUMBER	SHEET NAME	ISSUE	DESCRIPTION	DATE
DA01	TITLE SHEET	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
DA02	SITE PLAN	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
DA03	GROUND FLOOR PLAN	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
DA04	ROOF PLAN	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
DA05	ELEVATIONS	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
DA06	LANDSCAPING	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
DA07	SIGNAGE PACKAGE	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023





GENERAL NOTES			
DIMENSIONS TO BE VERIFIED ON SITE PRIOR TO			
COMMENCEMENT, PREPARATION OF SHOP			
DRAWINGS OR MANUFACTURING. FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALING.			
VERIFY LOCATION OF EXISTING SERVICES	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
BEFORE COMMENCEMENT.	4	DEVELOPMENT APPLICATION SET	01/12/2022
ALL CONSTRUCTION TO BE IN ACCORDANCE	3	DEVELOPMENT APPLICATION SET	21/11/2022
WITH THE NATIONAL CONSTRUCTION CODE OF	2	DEVELOPMENT APPLICATION SET	03/10/2022
AUSTRALIA, BUILDING ACT 1975 AS AMENDED, STANDARD BUILDING BY-LAWS AND RELEVANT	1	DEVELOPMENT APPLICATION SET	06/09/2022
AUSTRALIAN STANDARDS.	ISSUE	DESCRIPTION	DATE





CLIENT: JARRA PROPERTY

FILE: C:\Users\TEMP\Documents\J0000734_EXMOUTH DVE_BULTER_DA_23_sowmyaQCJDT.rvt

PROPOSED CHILDCARE CENTRE (92 places)

LOCA	TION:	
121	EXMOUTH DRIVE,	BUTLER, WA

DRAWING	TITLE:
TITLE	SHEET

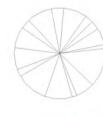
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D/A ISSUE



LEGEND



EXISTING TREE to be retained & protected (GREY)



PROPOSED TREE nominal location shown (GREEN)

CHILDCARE CENTRE ANALYSIS

OPERATION HOURS 6:30am to 6:30pm Monday to Friday with up to four days open on the weekend for open days

GROUP ROOM 1	0-24months	08 PLACES	2 EDUCATORS
GROUP ROOM 2	0-24months	04 PLACES	1 EDUCATORS
	24-36months	10 PLACES	2 EDUCATORS
GROUP ROOM 3	24-36months	15 PLACES	3 EDUCATORS
GROUP ROOM 4	24-36months	15 PLACES	3 EDUCATORS
GROUP ROOM 5	36+ months	20 PLACES	2 EDUCATORS
GROUP ROOM 6	36+ months	20 PLACES	2 EDUCATORS
			+ 2 STAFF

92 PLACES 17 STAFF (minimum at capacity)

AREA ANALYSIS

TOTAL SITE AREA: 2215m²
SITE COVERAGE: 817m² (37%)

GFA: 778m² gross leaseable area

m² gross leaseable area

1 SITE PLAN 1:200

GENERAL NOTES			
DIMENSIONS TO BE VERIFIED ON SITE PRIOR TO			
COMMENCEMENT, PREPARATION OF SHOP			
DRAWINGS OR MANUFACTURING. FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALING.			
VERIFY LOCATION OF EXISTING SERVICES	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
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AUSTRALIA, BUILDING ACT 1975 AS AMENDED, STANDARD BUILDING BY-LAWS AND RELEVANT	1	DEVELOPMENT APPLICATION SET	06/09/2022
AUSTRALIAN STANDARDS.	ISSUE	DESCRIPTION	DATE





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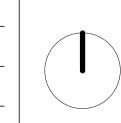
PROJECT:
PROPOSED CHILDCARE CENTRE (92 places)

FILE: C:\Users\TEMP\Documents\J0000734_EXMOUTH DVE_BULTER_DA_23_sowmyaQCJDT.rvt

LOCATION: 121 EXMOUTH DRIVE, BUTLER, WA

DRAWING TITLE: SITE PLAN

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DA02/5

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VERIFY LOCATION OF EXISTING SERVICES BEFORE COMMENCEMENT.

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DIMENSIONS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT.

5 DA SET UPDATED WESTERN BOUNDARY 22/02/2023

4 DEVELOPMENT APPLICATION SET 01/12/2022

DEVELOPMENT APPLICATION SET 03/10/2022

1 DEVELOPMENT APPLICATION SET 06/09/2022

ISSUE DESCRIPTION DATE

ARCHITECTS

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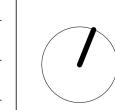
PROPOSED CHILDCARE CENTRE (92 places)

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LOCATION: 121 EXMOUTH DRIVE, BUTLER, WA

DRAWING TITLE:
GROUND FLOOR PLAN

	SCALE:	1 : 100@A1	DATE:	DEC 2022
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DA03/5

D/A ISSUE



1 ROOF PLAN
1:100

GENERAL NOTES

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VERIFY LOCATION OF EXISTING SERVICES BEFORE COMMENCEMENT.

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DIMENSIONS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT.

5 DA SET UPDATED WESTERN BOUNDARY 22/02/2023

4 DEVELOPMENT APPLICATION SET 01/12/2022

1 DEVELOPMENT APPLICATION SET 03/10/2022

1 DEVELOPMENT APPLICATION SET 06/09/2022

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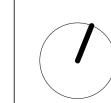
PROJECT:
PROPOSED CHILDCARE CENTRE (92 places)

FILE: C:\Users\TEMP\Documents\J0000734_EXMOUTH DVE_BULTER_DA_23_sowmyaQCJDT.rvt

LOCATION: 121 EXMOUTH DRIVE, BUTLER, WA

DRAWING TITLE: ROOF PLAN

	SCALE:	1 : 100@A1	DATE:	DEC 2022
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LEGEND



1 LANDSCAPING PLAN
1:200

GENERAL NOTES			
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DIMENSIONS TAKE PRECEDENCE OVER SCALING.			
VERIFY LOCATION OF EXISTING SERVICES	5	DA SET UPDATED WESTERN BOUNDARY	22/02/2023
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AUSTRALIAN STANDARDS.	ICCLIE	DESCRIPTION	DATE

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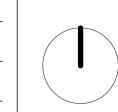
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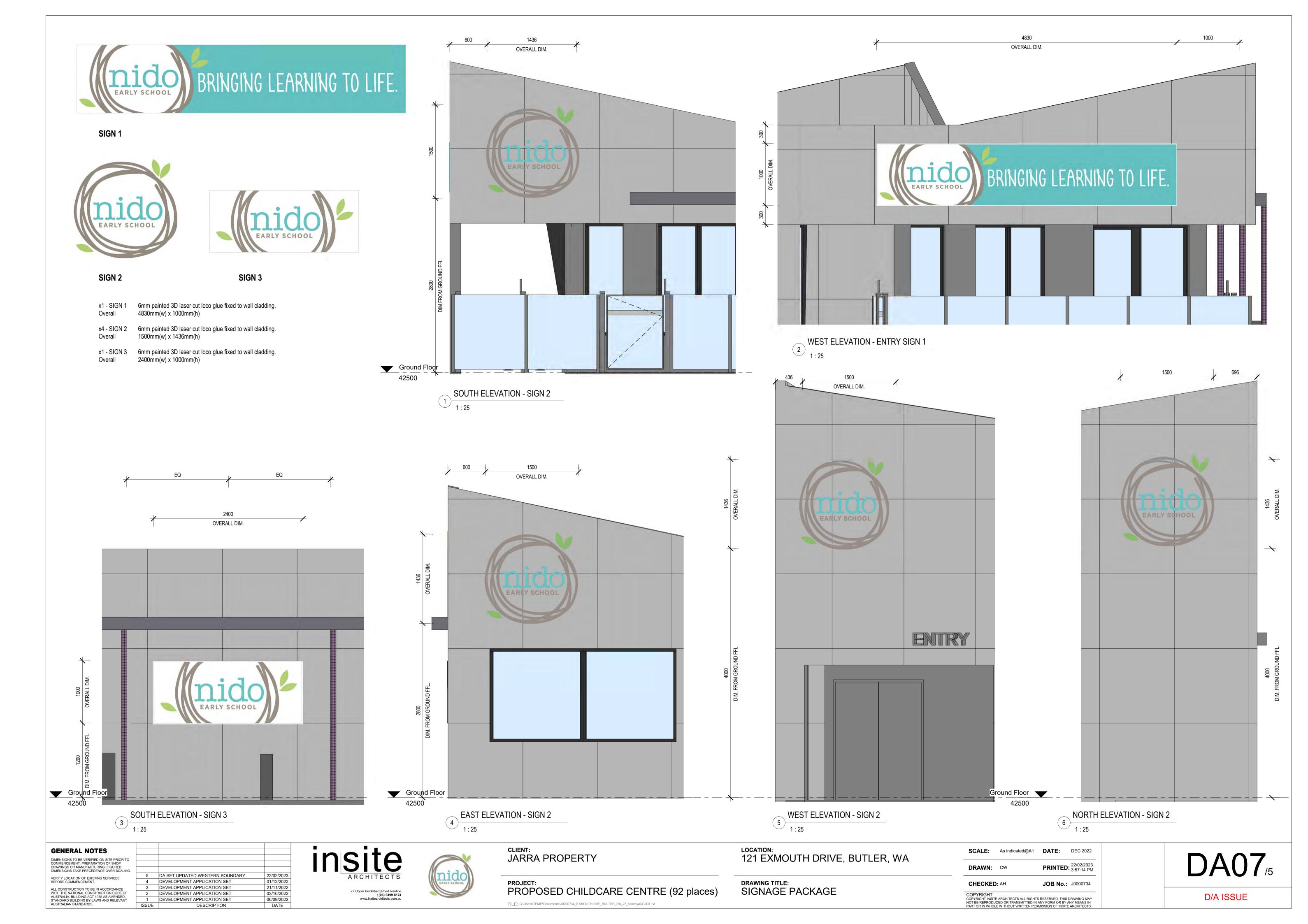
LOCATION: 121 EXMOUTH DRIVE, BUTLER, WA

DRAWING TITLE:
LANDSCAPING

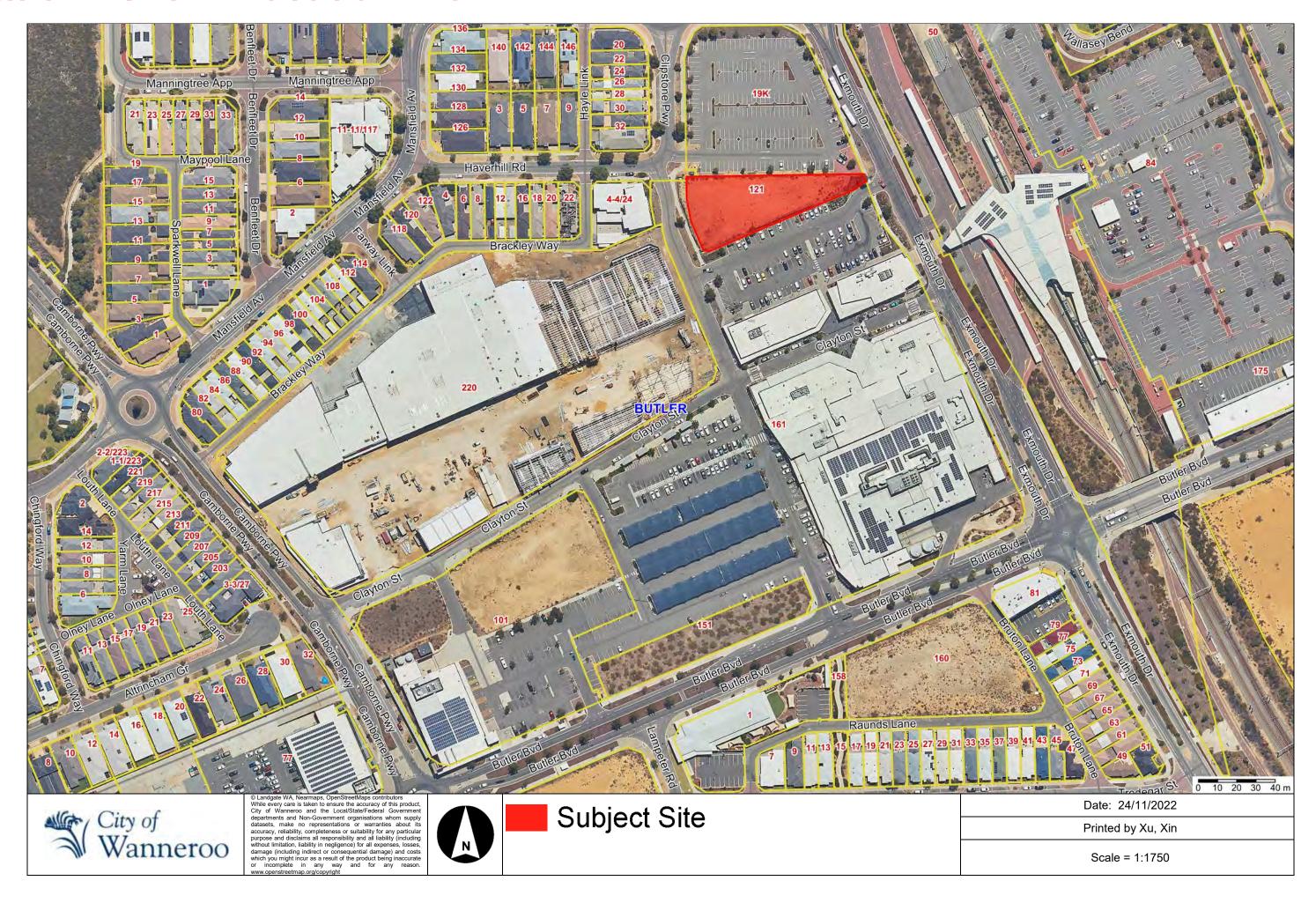
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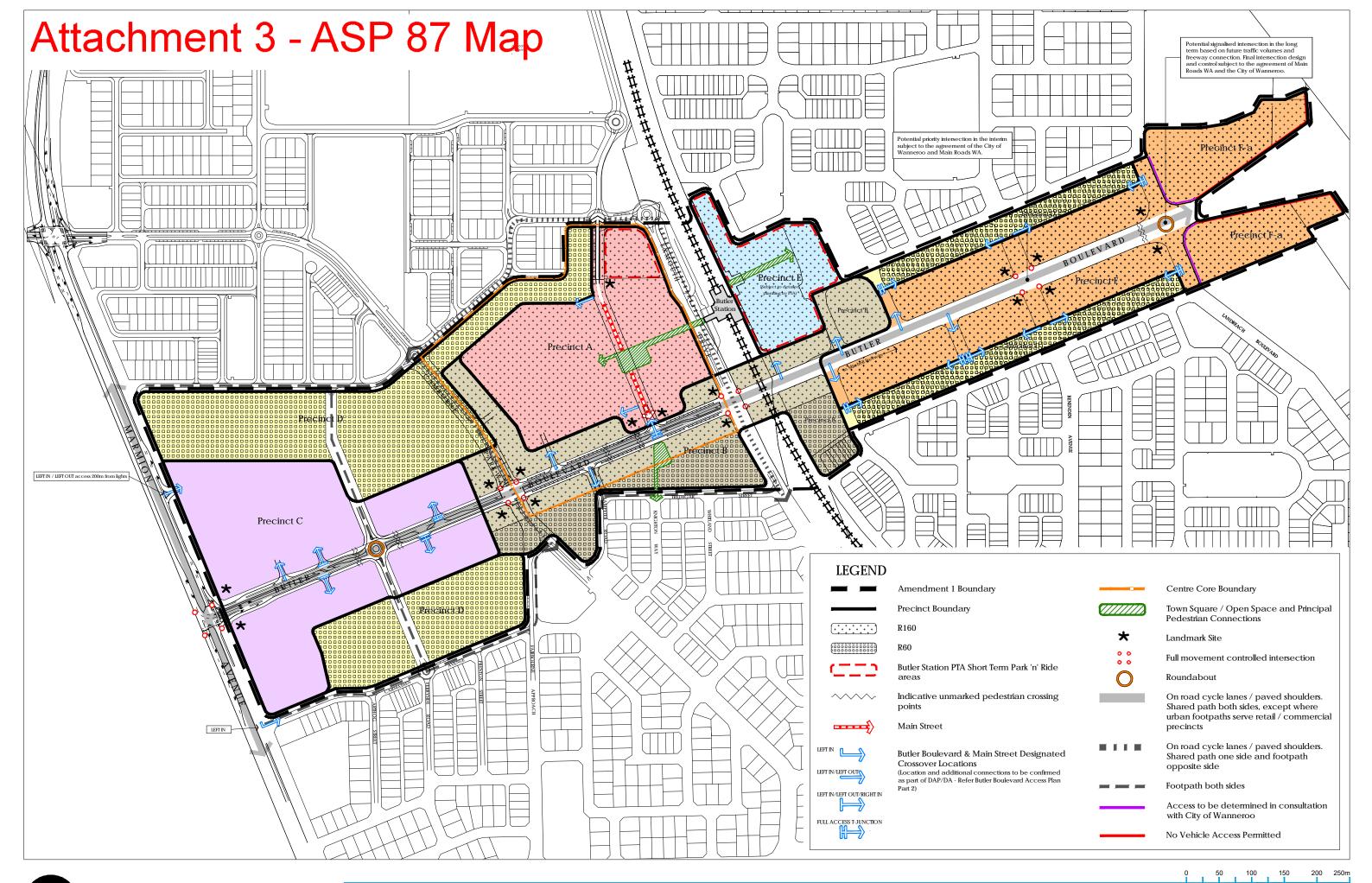




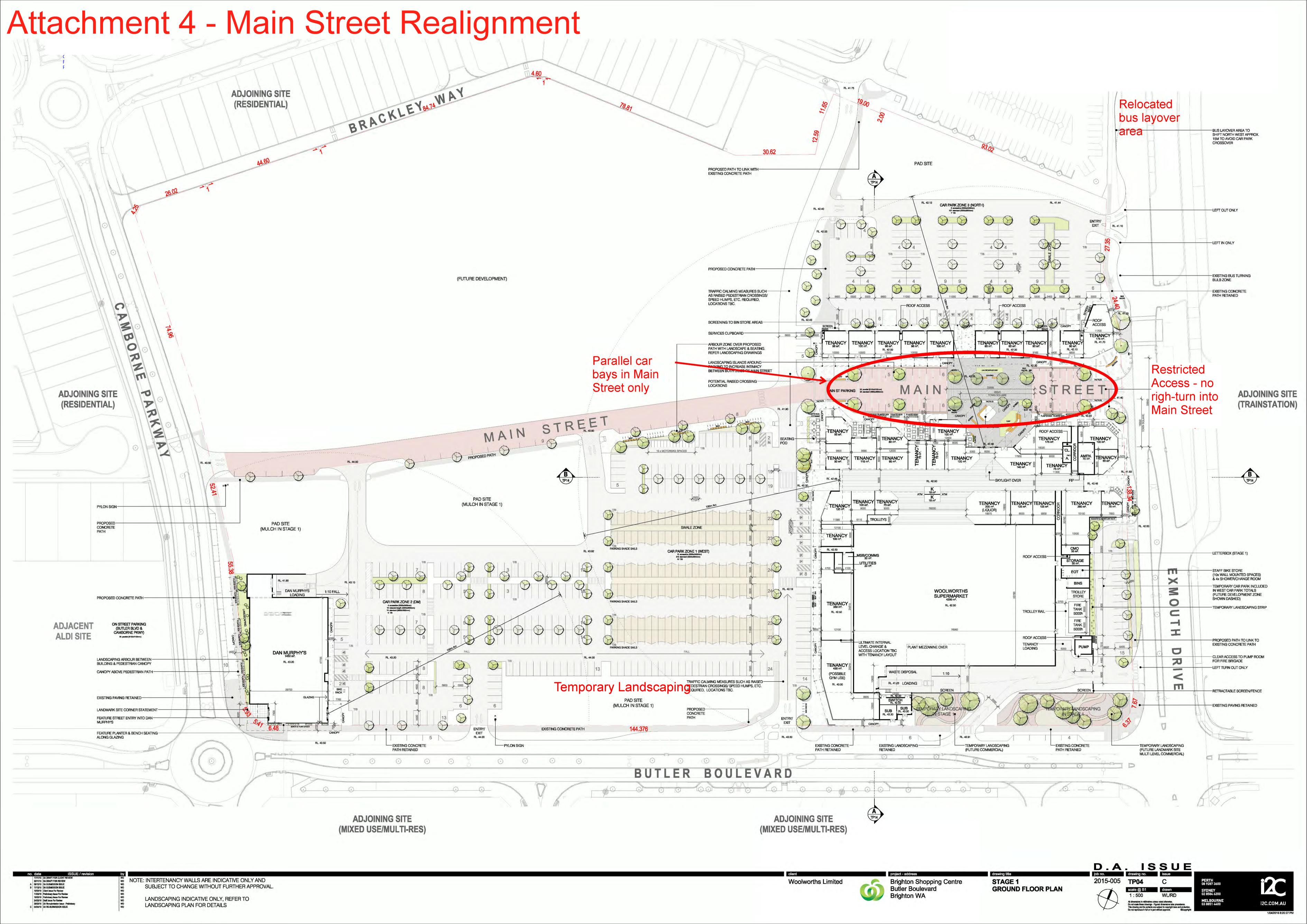


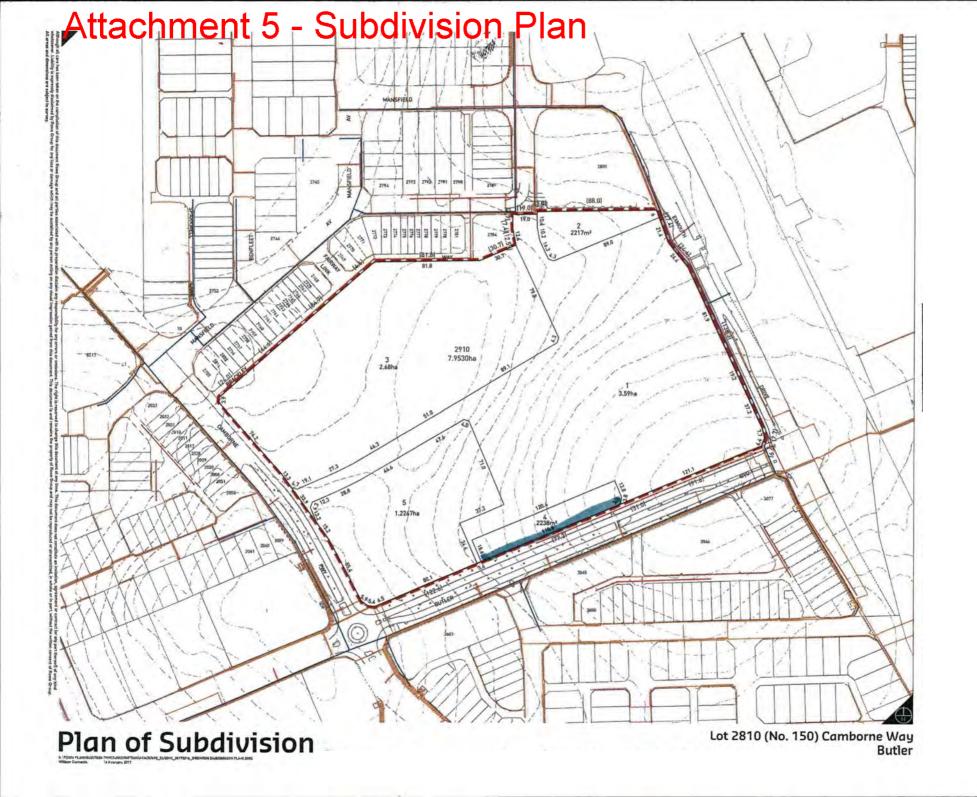
Attachment 2: Location Plan



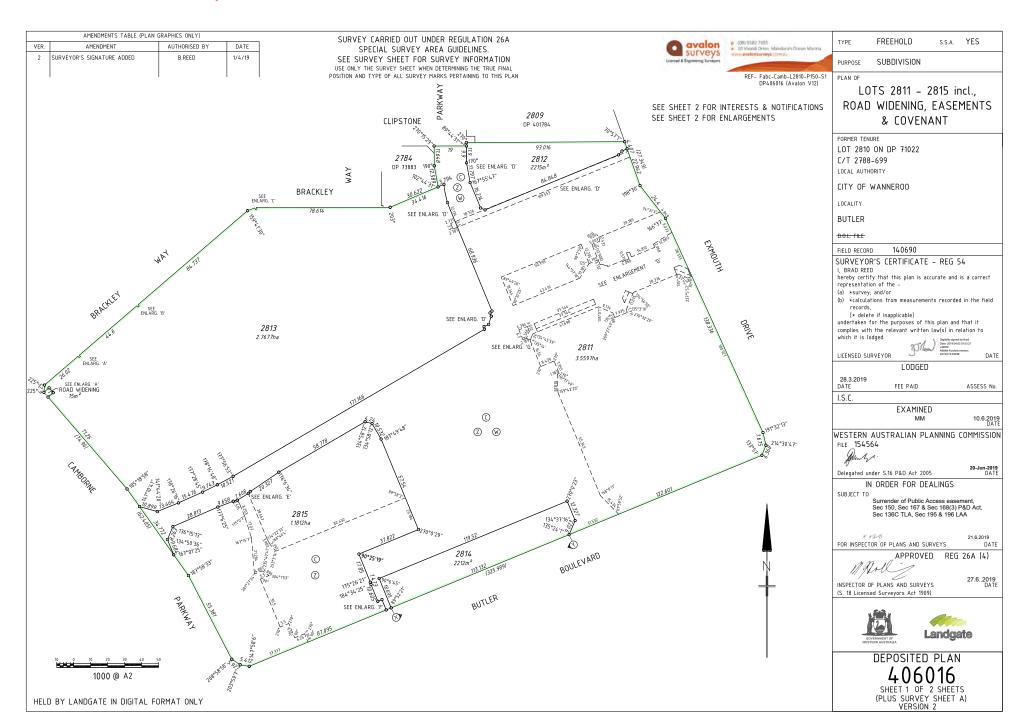


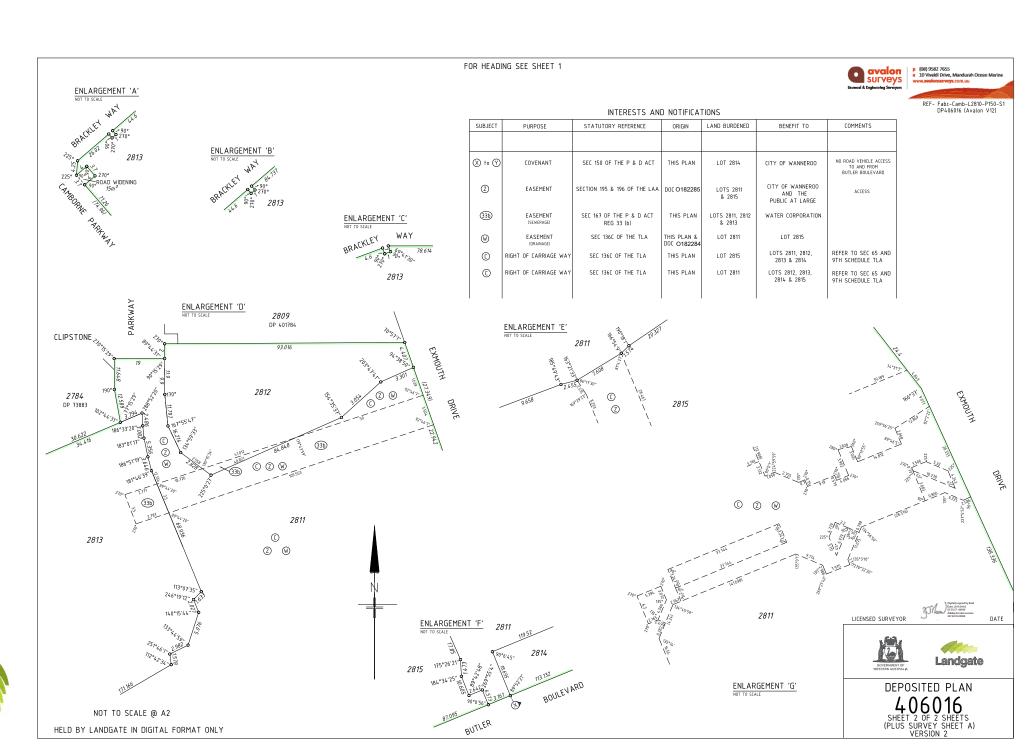


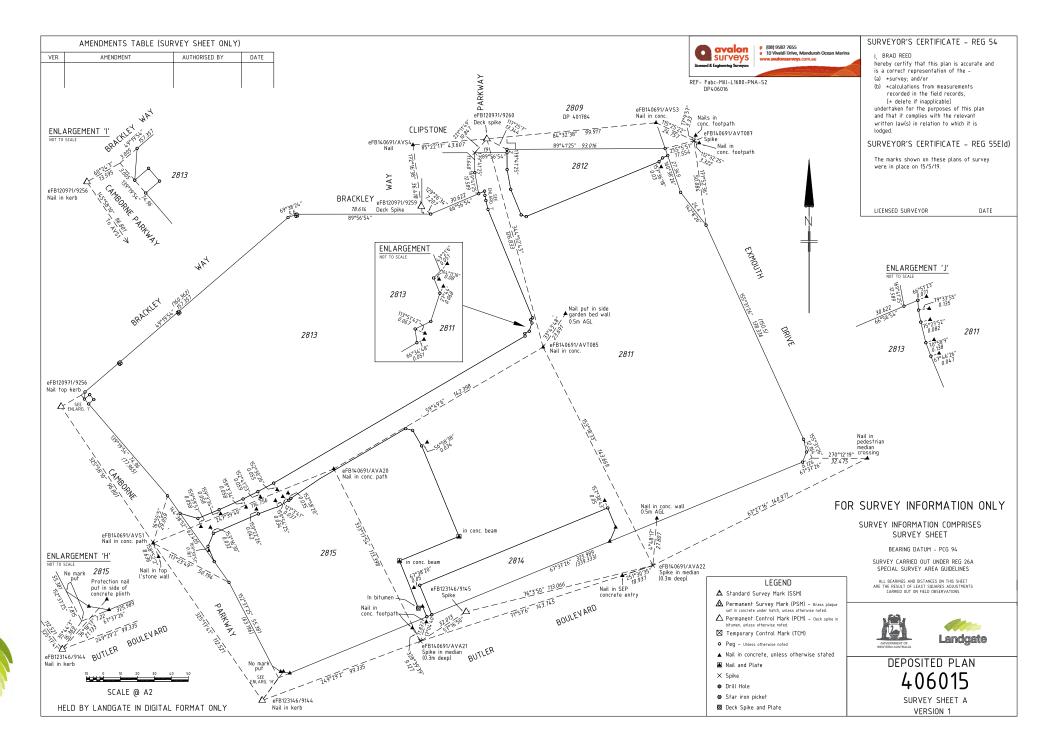




Attachment 6 - Deposited Plan

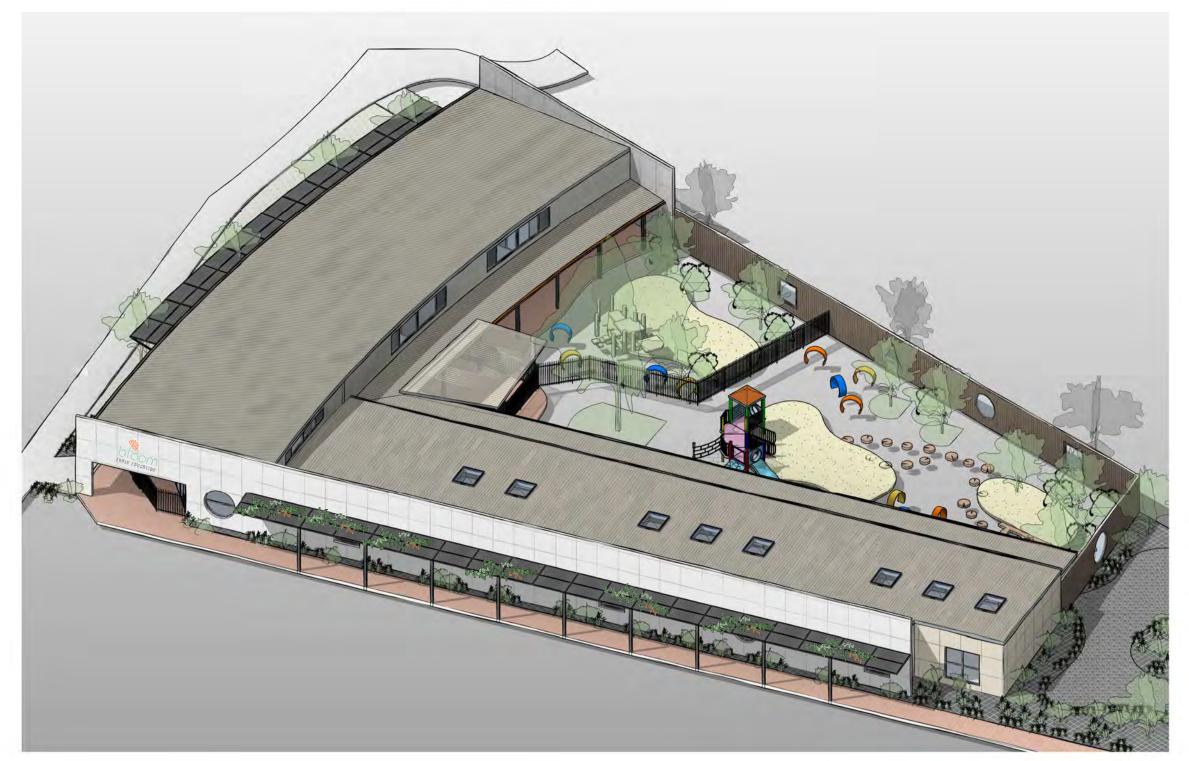








Attachment 7 - Plans as Presented to Design Review Panel











4 3D Internal Foyer

No			
NO	REVISION	DATE	
Α	Preliminary Issue	08/03/22	
В	DRP Issue	10/03/22	

	P	Н	A
17/210 Bagot Rd, Subiaco	6008 WA,	(08	3) 9388 1591

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BUTLER CHILDCARE 121 EXMOUTH DR, BUTLER

3D VIEWS



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DATE

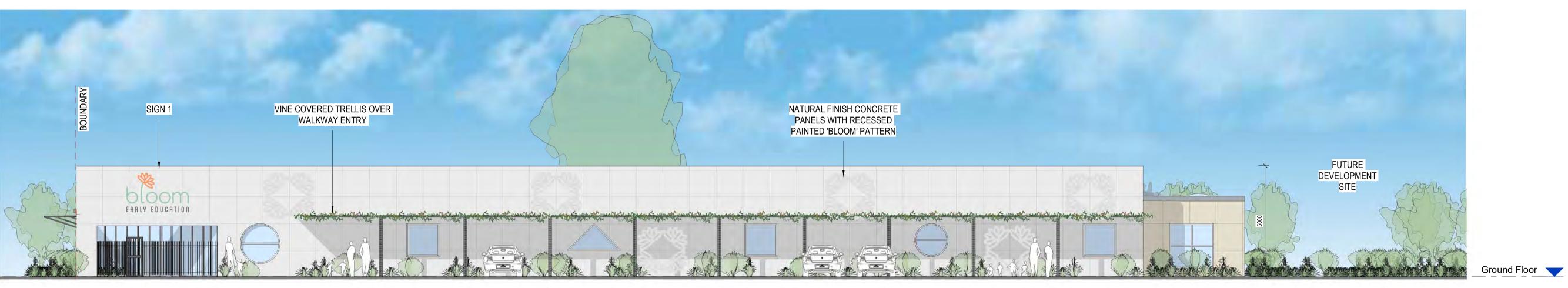
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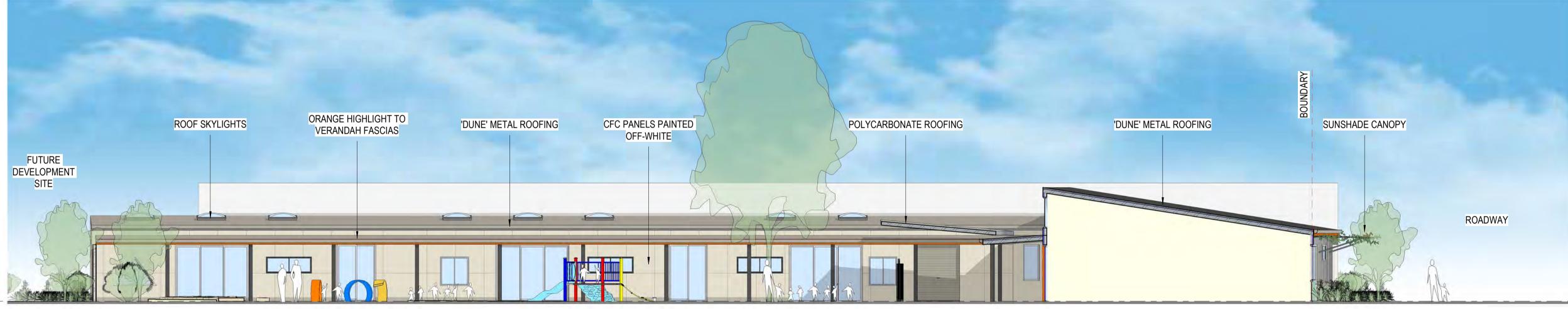
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08/03/22 Preliminary Issue DRP Issue 10/03/22

Ground Floor

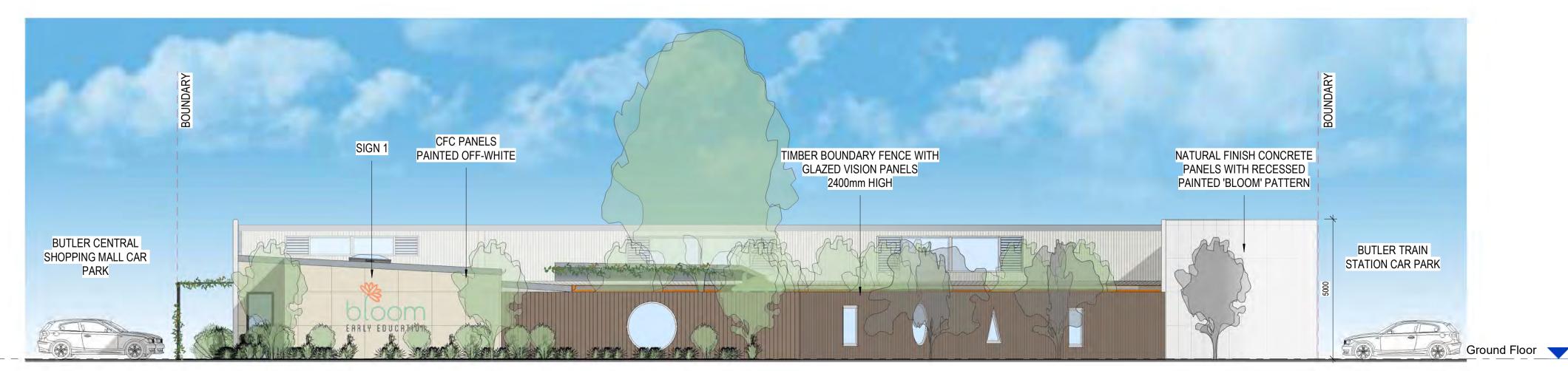


South Elevation
1:100



North Elevation (Internal)

1:100



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Ground Floor

SCALE BAR - 1:100

PROPERTY

BUTLER CHILDCARE

121 EXMOUTH DR, BUTLER Drawing Title

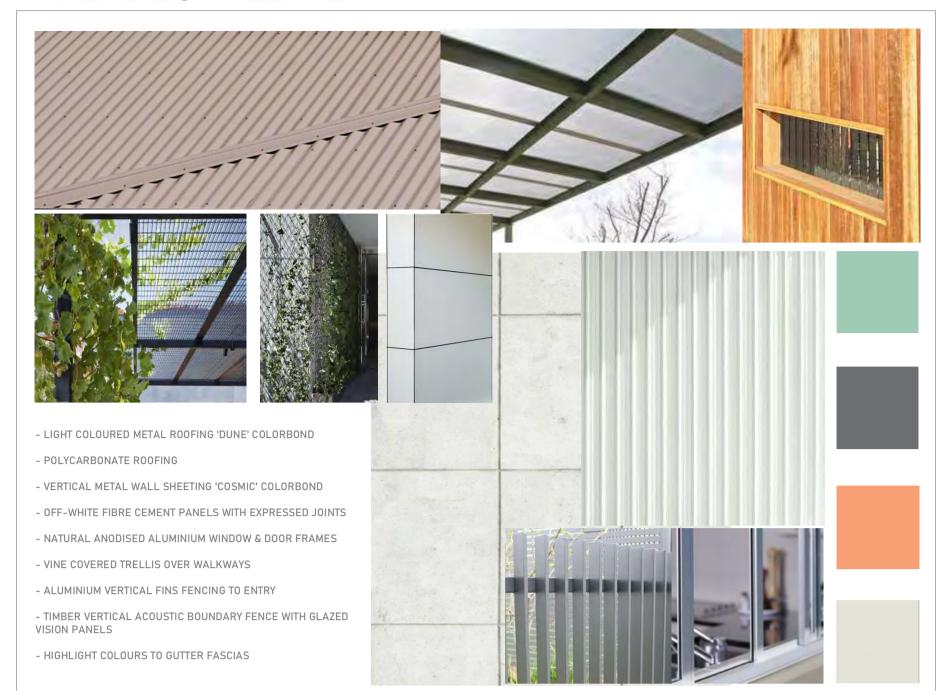
GRAPHIC ELEVATIONS SHEET - 1

Current Rev Date Scale @ A1 Job Number 1:100 2130 10/03/22 Drawing Number Revision SD-10

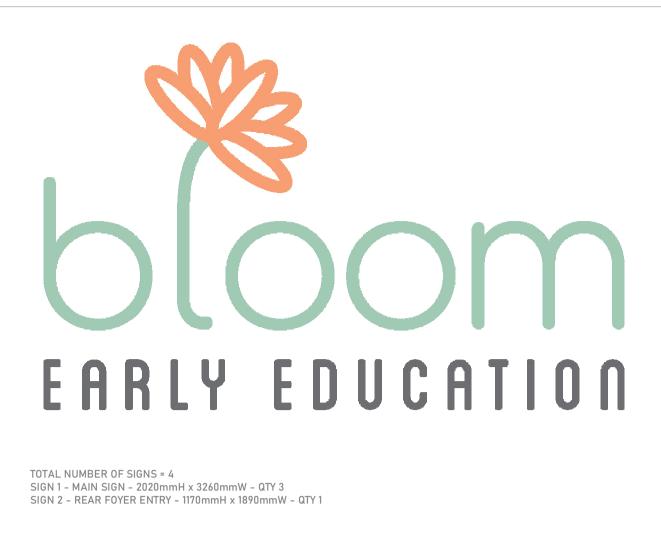
East Elevation

1:100

MATERIALS PALETTE



SIGNAGE

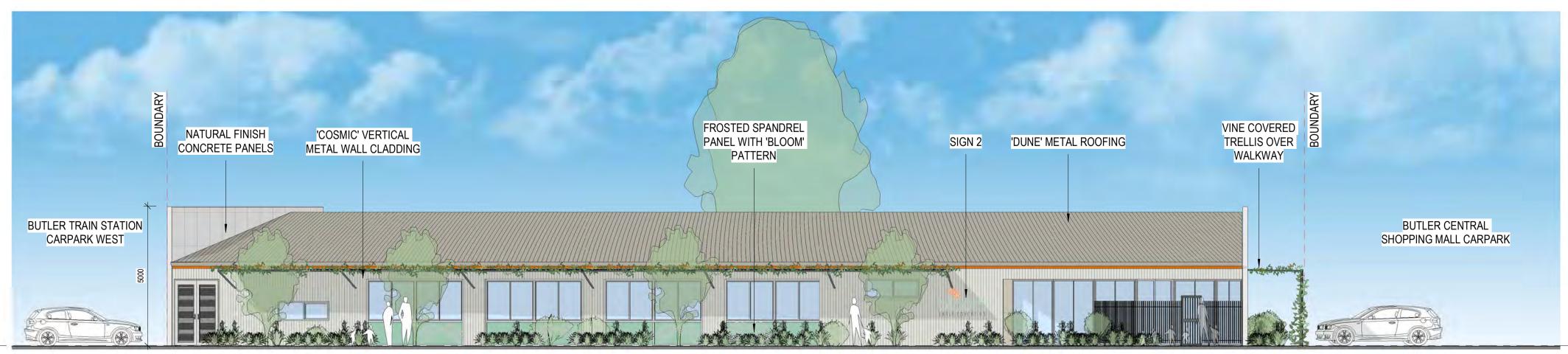


Ground Floor



East Elevation (Internal)

1:100



West Elevation
1:100

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BUTLER CHILDCARE 121 EXMOUTH DR, BUTLER

Drawing Title **GRAPHIC ELEVATIONS** SHEET - 2

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Ground Floor

DATE

08/03/22

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Preliminary Issue

0-2 INTERNAL

2-3 INTERNAL

ENTRY AND CIRCULATION

KINDY-INTERNAL

COTS

OFFICE

SERVICES

STORAGE

TOILETS

DATE

08/03/22

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BUTLER CHILDCARE 121 EXMOUTH DR, BUTLER

Drawing Title

GROUND FLOOR

Current Rev Date	Job Number	Scale @
10/03/22	2130	1:
Drawing Number		Revi
SD-02		

SITE INFORMATION SITE AREA: SITE COVERAGE (PR): PLOT RATIO: OPEN SPACE: PERCENTAGE 2215m² 982m² 0.44 1233m² 56%

SOFT LANDSCAPING

CARPARK LANDSCAPING: PLAY AREA LANDSCAPING: FUTURE DEVELOPMENT LANDSCAPING: N/A 694m² 512m² **TOTAL** 1206m²

TREES PROPOSED

NEW TREES (IN PLAY AREA): NEW TREES (IN VERGE AREA): NEW TREES (IN FUTURE DEVELOPMENT AREA):

CARPARK BAYS - WITHIN SHOPPING MALL CAR PARK

STAFF PARKING: VISITORS PARKING: **TOTAL CAR PARKING** 25

CHILDCARE YIELD

No. OF CHILDREN: No. OF STAFF: OUTDOOR PLAY AREA: INDOOR PLAY AREA: 694m² 291m²



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SCALE BAR - 1:100 SD-



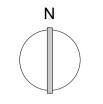
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No

NO REVISION DATE

١	DRP Issue	10/03/22



LANDSCAPE PALETTE



ANIGOZANTHOS FLAVIDU Yellow Kangaroo Paw 0.5-1.5m (AnF)



ANIGOZANTHOS MANGLESII Red-Green Kangaroo Paw 0.5-1.5m (AnM)



ANIGOZANTHOS HYBRID
Landscape Orange Kangaroo
Paw
0.5–1.5m



LOMANDRA TANIKA 0.5m high (LoT)



CONOSTYLIS CANDICANS Grey Cottonheads 0.05-0.4m high (CoC)



MELALEUCA Little Nessie 1-2m



MELALEUCA VIRIDIFLORA Broad Leaf Paperbark 3-10m (MeV)



CUPANIOPSIS ANACARDIOIDES Carrotwood, Brush Deal, Tuckeroo 9-12 m (CuA)



ACACIA TUMIDA Sickle Leaf Wattle 3-5m (AcT)



HARDENBERGIA VIOLACEA Snow White Creeper (HaVw)



HARDENBERGIA VIOLACEA False Sarsaparilla (HaVp)



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Project

BUTLER CHILDCARE

121 EXMOUTH DR, BUTLER

Drawing Title

LANDSCAPE PALETTE

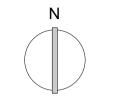
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10/03/22	2130	1:200
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NO REVISION DATE Preliminary Issue 08/03/22



NOTE: SITE DIMENSIONS SITE DIMENSIONS ARE BASED ON LANDGATE DEPOSIT PLAN 406016



17/210 Bagot Rd, Subiaco 6008 WA,

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BUTLER CHILDCARE 121 EXMOUTH DR, BUTLER

Drawing Title

SITE PLAN

Current Rev Date Job Number Scale @ A1 1:250 SD-01

Design review report and recommendations						
Design quality evaluation						
	Supported					
	Pending further attention					
	Not supported					
	Insufficient information to evaluate					
Strengths of the	Experienced and quality design team.					
proposal	A master plan that delivers active streetscape engagement.					
	Functional planning and arrangement of spaces.					
	Functional centrally located, generous and legible entry.					
	Instructive context and character study.					
	A master plan that delivers high levels of solar access and a north facing external play space/courtyard.					
	High levels of natural ventilation including operable roof lights to south wing and sectional high light/level windows to west wing.					
	Contextually appropriate native landscape planting including walkway trellis					
	Generous southern covered walkway mediating between the car park and the entry facilitating legible and safe access.					
	Appropriate and playful facade approach and thoughtfully designed fencing.					
	High levels of potential deep soil planting.					
	Proximity to train station.					
	 Landscaped strip adjacent to footpath enhances the public domain and provides privacy buffer. 					
	Veranda shade structure to courtyard.					
	Aesthetic approach and colour palette.					
Principle 1 - Context and character	Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.					
	Instructive context and character study with contextually appropriate native landscape planting including walkway trellis.					
	A master plan that delivers active streetscape engagement with associated passive surveillance.					
	The northwest corner presentation is dominated by a concrete blade wall with signage and an embossed pattern enclosing the bin store. The wall will be visually dominant and its success with be contingent on a high-quality finish.					
	The western leading edge of the two blade walls might be in danger of appearing thin and insubstantial and present in a 'stage-set' manner.					
	The fencing to the north and south appears to be unnecessarily high.					
	The high light windows on the west wing work well from both a visual and amenity point of view.					
	The roof lights to the south wing provide access to natural light and ventilation. However, it may be worth considering applying the same high light window					



	treatment to the south wing which may intern provide a built form and elevation with greater civic presence.
Recommendation	Carefully consider the finish, texture and pattern of the north and south blade walls to ensure a successful elevation and response to context and character.
	Consider returning the western leading edges of the two blade walls to create the sense of mass at the south and north ends of the street elevation. This may also assist with creating an enhanced sense of civic scale.
	Consider reducing the fence height to the north and south where possible.
	Consider extending the high-light window profile to the southern wing.
Principle 2 - Landscape quality	Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.
	The proposal is preliminary in nature and has not yet included a landscape professional or landscape proposal in detail.
Recommendation	Appoint a landscape professional to provide a comprehensive and high- quality landscape design proposal
Principle 3 - Built form and scale	Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.
	The built form and scale are considered appropriate.
	However, the capacity to enhance the physical presence of the built form within the streetscape at the northwest corner is required to achieve a landmark element as per Butler District Centre Structure Plan ASP 87. See commentary under Principle 1-Context and character.
Recommendation	Consider returning the western leading edges of the two blade walls to create the sense of mass at the south and north ends of the street elevation. This may also assist with creating an enhanced sense of civic scale.
Principle 4 - Functionality and build quality	Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.
	 The proposal does not yet adequately describe how services, including AC condensers, will be managed where they are fully screened from the public and private domain. Consider a pram storage area.
Recommendation	Illustrate the location of HVAC equipment, including AC condensers, and how they will be screened from the public and private domain.
	 A roof plan and sections would be instructive and assist with understanding the arrangement of functional equipment.
Principle 5 - Sustainability	Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.



	The proposal is preliminary in nature and does not yet include an ESD professional or sustainable design strategy.
Recommendation	Appoint an ESD professional to assist with developing a coherent, ambitious and effective ESD design strategy including a commitment to solar PV's,
Principle 6 - Amenity	Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.
7	The proposal incorporates high levels of occupant amenity in all respects except for one: The west cot room does not incorporate access to natural light or ventilation. A childcare centre is often the first public building a child will experience or occupy for any length of time. It is therefore considered essential that all habitable rooms incorporate access to an openable window and natural light where possible. Whilst it is understood that cot rooms are quiet spaces the ability to purge cot rooms with natural ventilation and to access natural light are considered essential.
Recommendation	Provide natural light and ventilation to all habitable rooms including cot rooms.
Principle 7 - Legibility	Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.
	The master plan arrangement has delivered a legible, generous and safe entry sequence and experience.
Recommendation	 Consider aligning the entry with the street edge. Consider incorporating an extended canopy to provide additional pedestrian amenity to Clipstone Parkway. Consider providing elements of weather protection to the pedestrian path within the arbour structure.
Principle 8 - Safety	Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.
	The project has the capacity for high levels of streetscape and above ground passive surveillance.
Recommendation	None
Principle 9 - Community	Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.
	 In a project of this scale the most important contribution to the broader community is a high-quality design that provides active and engaged streetscapes with passive surveillance. The current proposal demonstrates the potential for such an outcome.
Recommendation	Consider providing seating adjacent to the front door entry to facilitate a



waiting area.					
Principle 10	Good design is the product of a skilled, judicious design process that results in attractive				
Aesthetics and inviting buildings and places that engage the senses.					
	The aesthetic approach including form, colour, texture and materiality is of a high quality.				
Recommendation	Carefully consider the finish, texture and pattern of the north and south blade walls to ensure a successful elevation and response to context and character.				



Concluding Remarks

- The proposal is a high-quality childcare design, carefully integrated into its immediate context providing high levels of amenity to occupants and visitors alike.
- However, Panel support is contingent on ensuring that all habitable spaces, including cot rooms, have access to immediate natural light and ventilation.

Design Review progress						
Su	upported					
Pe	ending further attention					
No	ot yet supported					
Ye	et to be addressed					
		DR1	DR2	DR3		
Principle 1 -	- Context and character					
Principle 2 -	- Landscape quality					
Principle 3 -	- Built form and scale					
Principle 4 - Functionality and build quality						
Principle 5 - Sustainability						
Principle 6 -	Principle 6 - Amenity					
Principle 7 - Legibility						
Principle 8 - Safety						
Principle 9 - Community						
Principle 10	- Aesthetics					

Signed by Chairperson – (Dominic Snellgrove)

Dated: 26-03-22



Design Review Panel recommendation

Applicant response

Principle 1: Context and character

- Carefully consider the finish, texture and pattern of the north and south blade walls to ensure a successful elevation and response to context and character.
- Consider returning the western leading edges of the two blade walls to create the sense of mass at the south and north ends of the street elevation. This may also assist with creating an enhanced sense of civic scale.
- Consider reducing the fence height to the north and south where possible.
- Consider extending the high-light window profile to the southern wing.
- The north and south blade walls employ a combination of vertical metal wall sheeting ('cosmic' Colorbond) in a light coastal colour tone. This arrangement accentuates the height at both ends of the building to enhance the corner response. The blade walls were also enhanced by increasing the height at the corners to reinforce the building's corner statement.
- The leading edges of the two blade walls are accentuated with increased height at the corner to create sense of mass and civil scale. This is enhanced through the use of vertical metal wall sheeting material.
- Fence height has been reduced to 1.8m.
- Highlight window profile at southern wing has been changed to pop-up skylights which will allow substantial northern light into the building.

Principle 2: Landscape quality

 Appoint a landscape professional to provide a comprehensive and high quality landscape design proposal.

A conceptual landscape design has been formulated for the purpose of this planning application. A landscape professional will be appointed to produce a detailed landscape planting plan at building permit stage of the project, as per standard practice.

Principle 3: Built form and scale

- Consider returning the western leading edges of the two blade walls to create the sense of mass at the south and north ends of the street elevation. This may also assist with creating an enhanced sense of civic scale.
 - The north and south blade walls employ a combination of vertical metal wall sheeting ('cosmic' Colorbond) in a light coastal colour tone. This arrangement accentuates the height at both ends of the building to enhance the corner response. The blade walls were also enhanced by increasing the height at the corners to reinforce the building's corner statement.
 - The leading edges of the two blade walls are accentuated with increased height at the corner to create sense of mass and civil scale. This is enhanced through the use of vertical metal wall sheeting material.

Principle 4: Functionality and build quality

- Illustrate the location of HVAC equipment, including AC condensers, and how they will be screened from the public and private domain.
- A roof plan and sections would be instructive and assist with understanding the arrangement of functional equipment.
- The development plans confirm AC condensers are provided on the roof, behind the north-western roof feature. This makes efficient use of an architectural feature which screens the units and provides acoustic attenuation.



Principle 5: Sustainability

Appoint an ESD professional to assist with developing a coherent, • ambitious and effective ESD design strategy including a commitment to solar PV's.

Solar PVs are provided on the roof, as depicted on the development plans. An ESD statement can be provided at building permit stage, if considered necessary.

Principle 6: Amenity

Provide natural light and ventilation to all habitable rooms including cot rooms.

- All activity rooms have direct access to external doors and windows, which provide access to natural light and ventilation. This includes the pop up skylight features on the western and southern wings of the building.
- Both cot rooms have external windows which provide natural light and ventilation.

Principle 7: Legibility

- Consider aligning the entry with the street edge.
- Consider incorporating an extended canopy to provide additional pedestrian amenity to Clipstone Parkway.
- Consider providing elements of weather protection to the pedestrian path within the arbour structure.
- The entry is aligned with the street edge and the southern car park.
- The awning has been extended to cover the entire southern walkway for the extent of the building. The awning has been revised to be more of a shade structure, but with incorporated greenery and landscape.

Principle 8: Safety

None

Noted.

Principle 9: Community

Consider providing seating adjacent to the front door entry to facilitate Seating is provided within the foyer. a waiting area.

Principle 10: Aesthetics

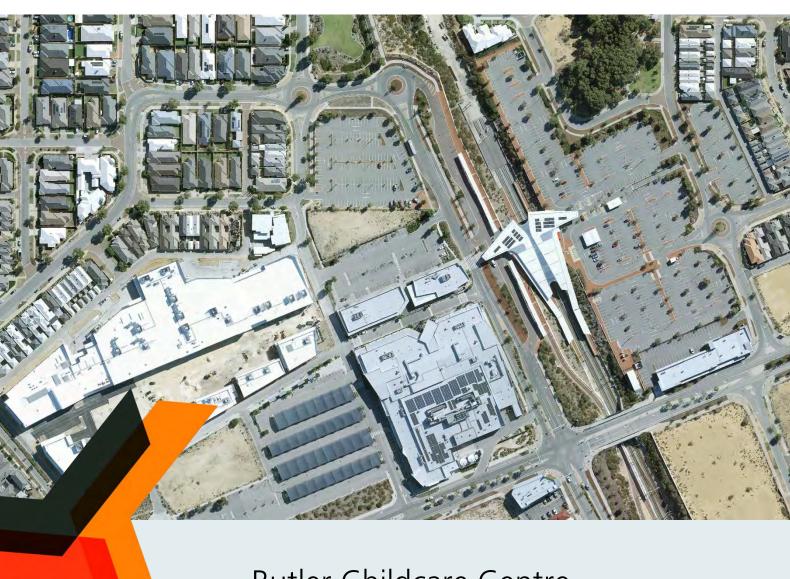
Carefully consider the finish, texture and pattern of the north and • south blade walls to ensure a successful elevation and response to context and character.

- The north and south blade walls employ a combination of vertical metal wall sheeting ('cosmic' Colorbond) in a light coastal colour tone. This arrangement accentuates the height at both ends of the building to enhance the corner response. The blade walls were also enhanced by increasing the height at the corners to reinforce the building's corner statement.
- The leading edges of the two blade walls are accentuated with increased height at the corner to create sense of mass and civil scale. This is enhanced through the use of vertical metal wall sheeting material.

Attachment 9 - Traffic Impact Statement



web: www.flyt.com.au



Butler Childcare Centre (121 Exmouth Drive, Butler)

TRANSPORT IMPACT STATEMENT



DDOJECT	04442 744 FLVT TDC 0002 D						
PROJECT	81113-744-FLYT-TRS-0002 Rev4						
Revision	Description	Originator	Review	Date			
0	Draft	MDR	CXS	23/06/2022			
1	Final	MDR	CXS	06/09/2022			
2	Revised Final	MDR	CXS	07/09/2022			
3	Revised Plans Final	MDR	CXS	03/10/2022			
4	Final Report to address CoW comments	MDR	CXS	22/11/2022			

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1. INTRODUCTION

1.1 Development Proposals

This Transport Impact Statement (TIS) has been prepared by Flyt in support of the proposed development of the current vacant land at 121 Exmouth Drive, Butler – into a Childcare Centre.

The proposed development of the existing vacant land into a Childcare Centre can be summarised as comprising:

- Childcare Centre to accommodate 92 children
 - o 22 children between the ages of 0-2 years
 - o 30 children between the ages of 2-3 years
 - o 40 children of kindergarten (kindy) age
- Childcare Centre to be serviced under the following staff arrangements:
 - o 15 educators (Monday-Friday, full time)
 - o Additional part-time educators covering staff breaks (Monday-Friday, 10am-3pm)
 - o 1 chef (Monday-Friday, half day)
 - o 1 centre area manager (visits site once or twice a week between 10am-3pm)

The proposed Butler Childcare Centre will have 92 children on-site, with 15 full-time 'room ratio' educators plus additional staff to perform support functions for the facility. Support staff can include a cook, an area manager, and approximately 5 'lunch cover' staff that attend to cover the full time educator's breaks, though it is noted that the number of lunch cover staff can vary.

1.2 Site Context

The site of the proposed Butler Childcare centre is located at 121 Exmouth Drive in Butler. The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Mall car park to the south and Butler Central Shopping Mall access road to the west.

The site is conveniently located to access Butler Station (300m walk from the entrance of the proposed Childcare Centre to the Station platforms) and Butler Central Shopping Mall (150m walk from the entrance of the proposed Childcare Centre to the entrance to the Shopping Centre).

As shown in Figure 1 the site of the proposed Butler Childcare Centre is conveniently located to access a range of facilities within a 400m walk catchment including; Butler Station, Butler Central Shopping Mall, large format retail units (currently under construction), Clipstone Park, Wilton Park, Manningtree Park, Halesworth Park and John Butler Primary School.







1.3 Transport Impact Statement

This TIS has been prepared in accordance with the WA Planning Commission's (WAPC) *Transport Impact Assessment Guidelines – Volume 4 Individual Developments* (2016). The Guidelines promote a three level assessment process, where the required level of assessment is dependent on the likely level of impact, as follows (as shown in Figure 2):

- <u>Low impact</u> less than 10 peak hour trips, no assessment required.
- <u>Moderate impact</u> between 10 and 100 peak hour trips, Transport Impact Statement required.
- <u>High impact</u> more than 100 peak hour trips, full Transport Impact Assessment required.

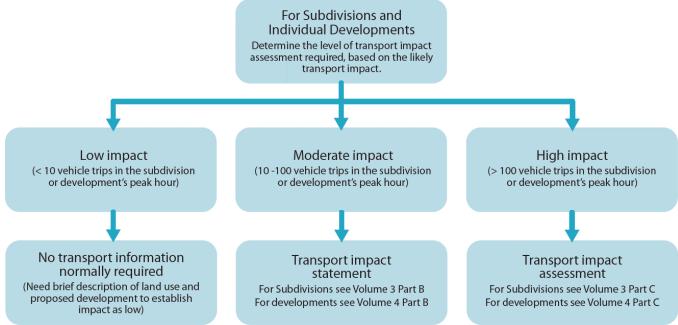


Figure 2 Level of transport impact assessment required (source: WAPC Transport Impact Assessment Guidelines, 2016)

As set out in this report, the traffic attributable to the proposed development has been determined to be less than 100 vehicle trips in the operating peak hour, therefore the required level of assessment is a TIS.

1.4 Report Structure

The report is structured as required by the *Transport Impact Assessment Guidelines* (WAPC, 2016), with the following sections:

- Proposed development
- Vehicle access and parking
- Provision for service vehicles
- Traffic volumes
- Frontage streets
- Public transport access
- Pedestrian access and amenity
- Bicycle access and amenity
- Site specific issues
- Safety issues
- Summary



PROPOSED DEVELOPMENT

The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Mall car park to the south and Butler Central Shopping Mall access road to the west – as shown in Figure 3.



Figure 3 Detailed location of the proposed Butler Childcare Centre (aerial image source: MetroMap)

The proposed development of the existing vacant land into a Childcare Centre can be summarised as comprising:

- Childcare Centre to accommodate 92 children
 - o 22 children between the ages of 0-2 years
 - o 30 children between the ages of 2-3 years
 - o 40 children of kindergarten (kindy) age
- Childcare Centre to be serviced under the following staff arrangements:
 - o 15 educators (Monday-Friday, full time)
 - o Additional part-time educators covering staff breaks (Monday-Friday, 10am-3pm)
 - o 1 chef (Monday-Friday, half day)
 - o 1 centre area manager (visits site once or twice a week between 10am-3pm)

The proposed Butler Childcare Centre will have 92 children on-site, with 15 full-time 'room ratio' educators plus additional staff to perform support functions for the facility. Support staff can include a cook, an area manager, and approximately 5 'lunch cover' staff that attend to cover the full time educator's breaks, though it is noted that the number of lunch cover staff can vary.

Figure 4 shows the site plan for the proposed Butler Childcare Centre and Figure 5 shows the site floor plan for the proposed Butler Childcare Centre.





Figure 4 Site plan for the proposed Butler Childcare Centre (source: Peter Hobbs Architects, 2022)



Figure 5 Site floor plan for the proposed Butler Childcare Centre (source: Peter Hobbs Architects, 2022)

VEHICLE ACCESS AND PARKING

3.1 Vehicle Access

The proposed Butler Childcare Centre is located at 121 Exmouth Drive in Butler. The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Mall car park to the south and Butler Central Shopping Mall access road to the west.

The proposed Childcare Centre site benefits from access in respect of easements outlined in the Lots Certificate of Title (Volume 2969 / Folio 53) and Deposited Plan (406016) with the land notable affected by the following easements:

- 'C' on Deposited Plan right of carriageway as per Section 136C of the Transfer of Land Act benefit to Lot 2812 [site of the proposed Butler Childcare Centre].
- 'Z' on Deposited Plan access for public at large as per Section 195 & 196 of the Land Administration Act refer to the attached easement document which confirms Z is also a public parking easement [see figure below].

Figure 6 shows the location of the proposed Butler Childcare Centre and an extract from Deposited Plan (406016) and the easement afforded to the Lot of the proposed Childcare Centre in respect of 'C' and 'Z'.

Figure 7 to Figure 10 show the routes of vehicle access to the proposed Butler Childcare Centre site car parking from the Butler Central Shopping Mall Existing Connector Road and from Exmouth Drive.





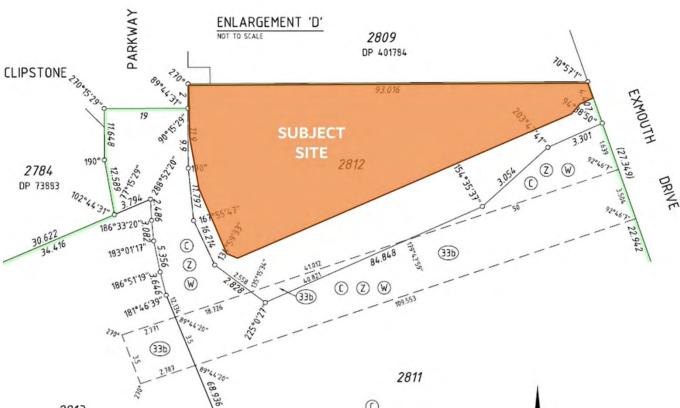


Figure 6 Vehicle access arrangement and site easements for the site of the proposed Butler Childcare Centre (source: Deposited Plan 406016, Landgate)



Figure 7 and Figure 8 show the route of vehicle access to the proposed Butler Childcare Centre site car parking from the Butler Central Shopping Mall Existing Connector Road.



Figure 7 View along the Existing Connector Road southbound on approach to the access into the Butler Central Shopping Mall northern car park (source: Google Streetview)



Figure 8 Existing access from the Existing Connector Road into the Butler Central Shopping Mall northern car park – the Butler Childcare Centre is proposed to occupy the vacant Lot on the left side of the Streetview image (source: Google Streetview)

Figure 9 and Figure 10 show the route of vehicle access to the proposed Butler Childcare Centre site car parking from Exmouth Drive.



Figure 9 View along Exmouth Drive southbound on approach to the access into the Butler Central Shopping Mall northern car park (source: Google Streetview)



Figure 10 Existing access from Exmouth Drive into the Butler Central Shopping Mall northern car park – the Butler Childcare Centre is proposed to occupy the vacant Lot in the centre of the Streetview image (source: Google Streetview)



3.2 On-Site Parking

The City of Wanneroo Local Planning Policy 2.3 *Child Care Centres* (June 2019) outlines the following traffic, access and parking requirements:

- "5.1. Child Care Centres should ideally be located on Neighbourhood Connector roads.
- 5.2. Parking areas should be located in front of buildings or easily visible from the entrance to the site.
- 5.3. Disabled parking bays should be located in close proximity to the pedestrian entrance to the site.
- 5.4. Parking is to be provided on site at a rate of one parking bay for each staff member, in addition to the required number of bays as outlined in the table below.

Children	Bays Required
<25	5
26-30	6
31-38	7
39-46	8
47-54	9
>54	9 bays, plus 1 per 8 children accommodated in excess of 54

5.5. Pedestrian access within the site is to be provided from the parking area to the entrance of the building and link into existing or future neighborhood pedestrian or cycle networks."

Based on the above requirements under the City of Wanneroo Local Planning Policy the proposed Butler Childcare Centre has the following off-street car parking requirements:

- Staff parking = 15 bays required for 15 full-time staff
 - = Additional part-time/occasional staff to utilise parent parking outside of peak drop-off / pick-up periods
- Parent parking = 14 bays required
 TOTAL PARKING = 29 BAYS REQUIRED

The proposed Butler Childcare Centre is located adjacent to Butler Central Shopping Centre Mall northern car park. The northern car park contains 94 standard parking bays and 4 universal access parking bays – a total of 98 parking bays. The northern car park is not the main car park for the Shopping Centre, with covered parking bays available in the main park located to the west of the Shopping Centre. In addition, the Shopping Centre has retail outlets which present towards the main car park, whereas the Shopping Centre has blank walls presenting to the northern car park.

It is anticipated that the proposed Butler Childcare Centre would operate from 6.30am-6.30pm Monday-Friday, whereas the Shopping Centre has core trading hours of 9am-5.30pm Monday, Tuesday, Wednesday, Friday and 9am-8pm Thursday. Major tenant Woolworths operates 8am-9pm Monday-Friday.



The 14 parking bays in the northern Shopping Centre car park located nearest to the entrance of the proposed Butler Childcare Centre would be utilised by parents dropping-off and picking-up children at the Childcare Centre. These bays will provide direct access to the footpath to be constructed along the southern side of the Childcare Centre.

It is expected that the following factors will reduce the demand of parent parking at the site:

- Activity Centre location creating opportunities for multi-use trips.
- Higher density area resulting in significant number of families within the walkable catchment.
- Proximity to the train station and associated parking areas, particularly the likelihood of parents combining drop off / pick up with their daily commute via bus or rail.
- Extent of public train station parking available, which may be used by parents commuting and using the childcare facility.

In particular, it is expected that given the proximity of the proposed Butler Childcare Centre to Butler Station, that some parents will park to the north of the Childcare Centre in the Transperth Park and Ride car park as part of their commute by train from Butler Station. These parents are likely to walk their children to the Childcare Centre from the Transperth car park as part of their walk to the Station.

These above factors are all unique to the proposed site of the Butler Childcare Centre site and would not apply under a typical Childcare Centre scenario. With these factors considered, parent parking demand is expected to be reduced.

Of the 15 full-time staff it is expected that some may drive to the site and others will car pool or use public transport to access the site. In this regard, it is noted that the site is a short 3 minute walk from Butler Station providing access to bus and train services. As such, the site is conveniently located to be accessed by public transport services – as outlined in Section 7.

Additional support staff would attend the site over the course of a day to perform various support functions. These staff are not present at the site for a whole day and are typically in attendance outside of the peak pick-up / drop-off periods (when there is no parent parking demand). Ample opportunity for support staff to attend the site is expected, given there is a significant amount of parking available in the immediate area, as well as excellent access to alternate modes of transport.

3.3 Off-Site Parking

No off-site parking is proposed as part of the Development Application for the proposed Butler Childcare Centre facility.



4. PROVISION FOR SERVICE VEHICLES

4.1 Deliveries

The proposed Butler Childcare Centre facility is not a delivery-intensive land use. Deliveries will be made by vans or similar sized vehicles and would occur during the day within the bays immediately adjacent the Childcare Centre. No specific loading areas/bays are proposed or required to support the Childcare Centre development.

4.2 Waste Collection

A separate Waste Management Plan has been prepared to support the Development Application for the Butler Childcare Centre. Please see the Waste Management Plan for all details of the planned waste collection regime for the development proposal.



TRAFFIC VOLUMES

5.1 Existing Traffic Generated by Proposed Site

The site of the proposed Butler Childcare Centre is located at 121 Exmouth Drive in Butler. The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Mall car park to the south and Butler Central Shopping Mall access road to the west.

The site of the proposed Childcare Centre is vacant land and therefore does not generate any existing traffic movements.

5.2 Trip Generation of Proposed Development

It is proposed that the Childcare Centre will operate with 18 staff and 92 registered children. The following assumptions have been made in relation to traffic that would be generated by this proposal.

- The Childcare Centre will operate with the following staffing arrangements:
 - o 15 staff would work on-site Monday-Friday in a full-time educator role.
 - Additional staff would work on-site Monday-Friday in a part-time role typically working half day 10am-3pm.
 - o 1 staff would work on-site once or twice a week as a visiting area manager between 10am-3pm.
 - o A conservative assumption has been made that 14 staff will independently drive to the site each day and 1 member of staff will car pool or use public transport. This may not be the case in operation with more staff likely to car pool or use public transport.
 - o The 15 full-time educator staff would be required on-site Monday-Friday to care for the full complement of 92 children this is based on a required ratio of educators per child.
 - o The additional part-time staff would be required on-site Monday-Friday during the middle of the day.
 - o The area manager will typically visit the site once or twice a week during the middle of the day.
 - o Based on the Childcare Centre's anticipated operating hours from 6.30am to 6.30pm it is expected that 2 members of staff would be rostered to start work during the developments AM peak hour and finish work during the development PM peak hour, with all other staff travelling to or from the site outside of the development peak hours.
- The Childcare Centre will cater for 92 registered children:
 - o At a range of childcare centres across Perth that Fly have surveyed, typically:
 - 15% of children are walked to the childcare centre by parents and do not generate a vehicle trip. These children are either walked from their home within a walkable catchment of the childcare centre or they use public transport services to access the childcare centre.
 - 25% of children are from families with more than one child attending the childcare centre therefore 25% of the children generate 0.5 inbound vehicles trips and 0.5 outbound vehicle trips (assuming these families have two children attending the childcare centre).
 - 60% of children generate 1 inbound vehicle trip and 1 outbound vehicle trip.
 - Of those children arriving at the childcare centre by private vehicle, typically:
 - 60% of children are dropped off during the AM peak hour
 - 50% of children are pick-up during the PM peak hour

Based on maximum attendance at the Childcare Centre on a given weekday and applying the typical childcare centre mode splits outlined above, it is estimated that a maximum total of 82 vehicle trips to/from the site will occur during the developments AM peak hour and 66 vehicle trips to/from the site will occur during the development PM peak hour — as outlined in Table 1.



Table 1 Proposed Childcare Centre development peak hour vehicle trips based on the proposed operations of the Childcare Centre

Type of Total Number		AM Peak Hour Vehicle Movements		PM Peak Hour Vehicle Movements				
User	On-Site	Generating Vehicle Trips	IN	OUT	TOTAL	IN	OUT	TOTAL
Staff (full-time)	15	15 (1.0 trips)	2	0	2	0	2	2
Staff (part-time)	3	3 (1.0 trips)	0	0	0	0	0	0
Children	92	14 (0.0 trips)	0	0	0	0	0	0
		23 (0.5 trips)	7	7	14	6	6	12
		55 (1.0 trips)	33	33	66	28	28	56
Total Staff	110	14 (0.0 trips)	0	0	0	0	0	0
& Children		23 (0.5 trips)	7	7	14	6	6	12
		73 (1.0 trips)	35	33	68	26	28	54
TOTAL VEHICLE TRIPS		42	40	82	32	34	66	

5.3 Traffic Impacts of Proposed Development

The proposed Butler Childcare Centre is unlikely to generate significant additional vehicle trips on the road network – many of the vehicle trips to drop-off and pick-up children from the Childcare Centre would be part of a linked trip already being made. The majority of linked trip will be part of the parents commute to their place of work and/or school drop-off / pick-up of older children.

The proposed Childcare Centre will generate more vehicle movements during the developments AM peak hour (more concentrated child drop-off activity) as opposed to during the developments PM peak hour (more dispersed child pick-up activity).

Whilst the AM peak hour drop-off activity at the proposed Childcare Centre is more concentrated than the PM peak hour, the site benefits from far lower levels of localised vehicle activity associated with Butler Central Shopping Mall during the AM peak hour, as well as the traffic volumes on Butler Boulevard indicating lower levels of traffic on the local road network during the AM peak hour compared to the PM peak hour. The AM peak traffic represents 43% of combined AM and PM peak hour movements, compared to the PM peak traffic representing 57% of combined AM and PM peak hour movements.

Traffic volume data for the traffic signal controlled intersection of Butler Boulevard and Exmouth Drive was sourced from the Main Roads WA Traffic Maps Portal (https://trafficmap.mainroads.wa.gov.au/map). The most recent available traffic data is for the period 21-25 February 2022. Note: this is the nearest location to the proposed Childcare Centre site with traffic data available on the Main Roads WA Traffic Maps Portal.

The average total weekday (Monday-Friday) peak hour traffic movements through the Butler Boulevard and Exmouth Drive intersection are outlined below:

- AM peak hour (0800-0900) 981 vehicles (43% of the combined AM and PM peak hour movements)
- PM peak hour (1600-1700) 1,296 vehicles (57% of the combined AM and PM peak hour movements)

Given the proximity of Butler Station it would be expected that a number of existing park and ride vehicle trips to Butler Station would include in the future a walk from/to Butler Station's Car Park West (adjacent to the proposed Childcare Centre) and include parents dropping-off/picking-up their children from the Childcare Centre.



It is expected that the following factors will reduce the demand of parent parking at the site:

- Activity Centre location creating opportunities for multi-use trips.
- Higher density area resulting in significant number of families within the walkable catchment.
- Proximity to the train station and associated parking areas, particularly the likelihood of parents combining drop off / pick up with their daily commute via bus or rail.
- Extent of public train station parking available, which may be used by parents commuting and using the childcare facility.

These above factors are all unique to the proposed site and would not apply under a typical Childcare Centre scenario. With these factors considered, parent parking demand is expected to be reduced.

In summary:

- The level of vehicle trips generated by the proposed Butler Childcare Centre is focused on generation of vehicle trips associated with children drop-off and pick-up movements. Staff movements are generally outside of peak periods as the educators have to be on-site to cater for the arrival of children and they cannot leave the facility until certain ratios of educators to children are achieved. In addition, some staff are likely to use alternate forms of transport, such as car pooling or public transport.
- Some drop off and pick up movements will be undertaken by foot or involve trips with multiple children being dropped-off or picked-up. In addition, not all movements will be made in the development AM or PM peak hour.
- The majority of traffic movements generated by the site are expected to be a slight redistribution of existing trips on the network as part of a linked trip primarily as part of a parents existing commute and/or school drop-off/pick-up of older children.
- The PM peak period, when the Shopping Centre and surrounding network is at its busiest and is therefore the key assessment period, will not see a significant impact as a result of traffic from this site. With the majority of traffic movements generated by the site expected to be a slight redistribution of existing trips on the network as part of a linked trip primarily as part of a parents existing commute.



6. FRONTAGE STREETS

The proposed Butler Childcare Centre has a southern frontage to the Butler Central Shopping Mall northern car park and a western frontage to the Existing Connector Road providing north-south access through the Shopping Centre site.

Both the Existing Connector Road and aisle through the Shopping Centre's northern car park are private roadways which provide for right of carriageway purposes to those accessing the Shopping Centre and associated land uses. As such, these private roadways are not categorised under the Main Roads WA road network hierarchy.

Exmouth Drive which runs north-south between the proposed Butler Childcare Centre site/Butler Central Shopping Mall site and Butler Station, is categorised as a Local Distributor Road under the Main Roads WA road network hierarchy.

Camborne Parkway which runs north-south to the west of the Butler Childcare Centre site and the new larger format retail units that are currently under construction, is also categorised as a Local Distributor Road under the Main Roads WA road network hierarchy.

Butler Boulevard which runs east-west to the south of Butler Central Shopping Mall is categorised as a Distributor A road under the Main Roads WA road network hierarchy – and is the highest order road in proximity of the proposed Butler Childcare Centre site.

All other roads in proximity of the proposed Butler Childcare Centre site are categorised as Access Roads under the Main Roads WA road network hierarchy.

The road hierarchy surrounding the proposed Butler Childcare Centre site is shown in Figure 11



Figure 11 Road hierarchy surrounding the proposed Butler Childcare Centre site (source: Main Roads WA)



Most streets in vicinity of the proposed Butler Childcare Centre site operate under a typical default 50km/h urban speed limit.

There are no road corridors in proximity of the proposed Butler Childcare Centre that operate with a higher posted speed limit than the default 50km/h urban speed limit.

The private roadways which provide for right of carriageway purposes to those accessing the Shopping Centre and associated land uses are designed to elicit low vehicle speeds with significant 'friction' along the roadways with slow moving traffic accessing and exiting parking areas and parking bays.

The speed zoning surrounding the proposed Butler Childcare Centre site is shown in Figure 12.



Figure 12 Speed zoning surrounding the proposed Butler Childcare Centre site (source: Main Roads WA)



7. PUBLIC TRANSPORT ACCESS

7.1 Existing Public Transport Services

The proposed Butler Childcare Centre site is conveniently located to be accessed by public transport services – these include bus and train services from Butler Station.

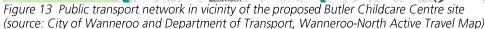
From the entrance of the proposed Childcare Center bus services from Butler Station can be accessed with a 200m-250m walk (2-3 minute walk time) and train services from Butler Station can be accessed with a 300m walk (3-4 minute walk time).

<u>Bus services</u> – bus routes from Butler Station provide connections to the residential areas to the north of the proposed Childcare Centre via Hollington Boulevard and Santorini Promenade, and connection to the residential areas to the southeast and southwest of the proposed Childcare Centre via Benenden Avenue, Landbeach Boulevard (to the southeast) and Camborne Parkway (to the southwest).

<u>Train services</u> – train service operate from Butler Station and provide connections south to Joondalup, Stirling, Leederville, Perth Underground, Elizabeth Quay and through routed train services to Canning Bridge, Murdoch, Cockburn, Rockingham and Mandurah.

Figure 13 shows the location of the proposed Butler Childcare Centre site in relation to public transport routes.







The closest bus stops to the proposed Butler Childcare Centre site are shown in Figure 14. The bus stands at Butler Station are a 200-250m walk from the site, via the proposed new 1.5m footpath along the southern side of the Childcare Centre and across the raised pedestrian crossing over Exmouth Drive to access Butler station.

Butler Station also provides train services to the south towards central Perth, with the station platforms a 300m walk from the Childcare Centre site – and accessed via the same safe high quality pedestrian route as used to access the bus stands.



Figure 14 Walk distance from the proposed Butler Childcare Centre site to the closest bus and train services from Butler Station (aerial image source: MetroMap)

7.1.1 Bus Services

Bus routes serving the bus stands at Butler Station provide connections to the residential areas to the north and south of the proposed Butler Childcare Centre site. The following key bus routes are those that are most likely to be used by parents with their children to access the Childcare Centre:

Bus Route 483

- o Route 483 operates between Clarkson Station and Alkimos via Merriwa and Butler Station. In proximity of the proposed Childcare Centre site the bus route operates via Camborne Parkway, Butler Boulevard, Butler Station, Mansfield Avenue, Hollington Boulevard and Santorini Promenade.
- o Weekday bus services operate between 5am and 10pm operating on a 10 minute frequency across a 2 hour AM and PM peak period and operating on a 30 minute frequency outside of these times.
- o Saturday bus services operate between 6am and 10pm operating on a 60 minute frequency.
- o Sunday bus services operate between 7am and 8pm operating on a 60 minute frequency.



• Bus Route 484

- o Route 484 operates between Clarkson Station and Alkimos via Ridgewood and Butler Station. In proximity of the proposed Childcare Centre site the bus route operates via Benenden Avenue, Butler Station, Butler Boulevard, Landbeach Boulevard and Shepperton Drive.
- Weekday bus services operate between 5am and 8pm operating on a 10 minute frequency across a 2 hour AM and a 12 minute frequency across a 2 hour PM peak period – and operating on a 30 minute frequency outside of these times.
- Saturday bus services operate between 6am and 8pm operating on a 60 minute frequency.
- o Sunday bus services operate between 7am and 7pm operating on a 60 minute frequency.

7.1.2 Train Services

Butler Station is an end of line station on the Joondalup Line and provide connections south towards central Perth. From Butler Station the following key locations can be accessed without the need to transfer train services/train lines; Joondalup, Stirling, Leederville, Perth Underground, Elizabeth Quay and through routed train services to Canning Bridge, Murdoch, Cockburn, Rockingham and Mandurah.

Butler Station provides access to the following rail services:

- Butler Station services operate towards Perth (38 minute journey time):
 - o Weekday rail services operate between 5am and 11.30pm operating on a 10 minute frequency during the morning and afternoon peak periods and operating on a 15 minute frequency at other times (other than after 8pm where services operate on a 30 minute frequency).
 - o Saturday rail services operate between 5.30am and 11.30pm operating on a 15 minute frequency (other than after 8pm where services operate on a 30 minute frequency).
 - o Sunday rail services operate between 7am and 11.30pm operating on a 15 minute frequency (other than after 7pm where services operate on a 30 minute frequency).



8. PEDESTRIAN ACCESS AND AMENITY

8.1 Existing Pedestrian Network

The proposed Butler Childcare Centre site is located in a central area of Butler adjacent to Butler Station and Butler Central Shopping Mall and has good levels of pedestrian connectivity with footpaths on both sides of most surrounding streets.

The Walk Score walkability assessment tool considers the proposed Butler Childcare Centre site to be "cardependent" where almost all errands require a car, with a walk score of 19 out of 100. The 15-minute walkable catchment is shown in Figure 15, which includes destinations such as Butler Station, Butler Central Shopping Mall and John Butler Primary College.

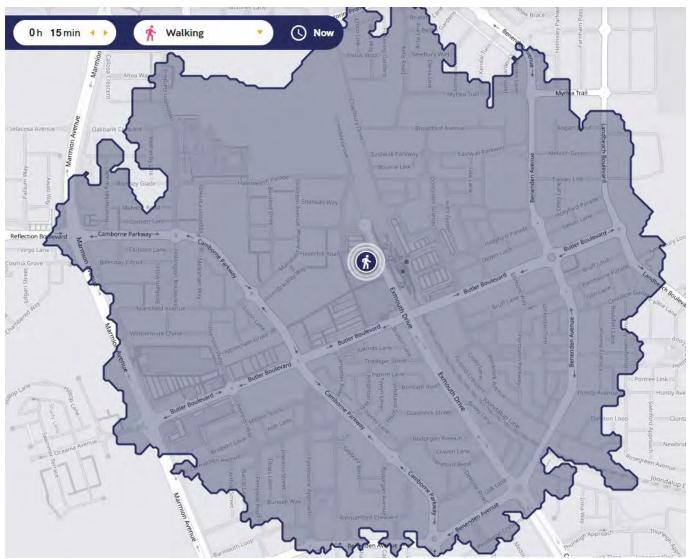


Figure 15 TravelTime Map 15 minute walking catchment from the proposed Butler Childcare Centre site (source: TravelTime)



The Department of Planning, Lands and Heritage (DPLH) recently released the Urban Tree Canopy Dashboard which provides an interactive snapshot of the extent of tree canopy coverage across the Perth and Peel regions. The urban tree canopy is an essential part of creating healthy, liveable neighbourhoods, where more dense and mature tree canopies can support active travel along walking and cycling paths.

The Perth Metropolitan area has an average of 12% canopy cover from trees over 3m tall in street blocks. In 2018, the street blocks in the City of Wanneroo had 7% canopy cover from trees over 3m tall, resulting in 93% of the street block area without any canopy cover. In 2018, the street blocks in the suburb of Butler had 2% canopy cover from trees over 3m tall, resulting in 98% of the street block area without any canopy cover – as shown in Figure 16.

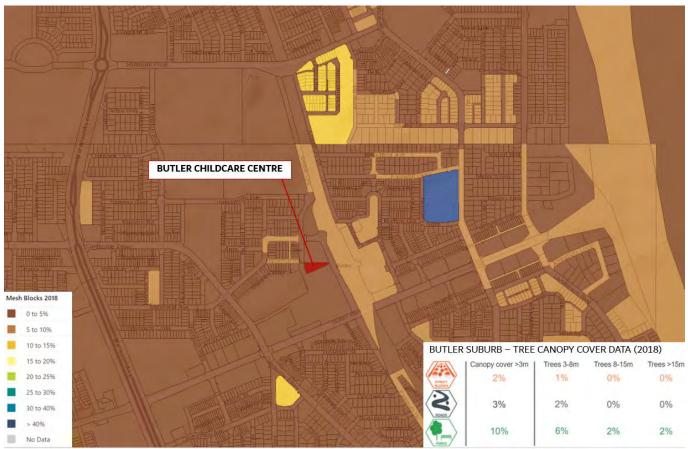


Figure 16 Central Butler urban tree canopy (source: Department of Planning, Lands and Heritage)

The very low street block tree canopy cover with trees greater than 3m high, is reflective of the recent development and build-out of the Butler suburb. Over time street trees planted as part of the development of the suburb will grow and mature and increase the tree canopy cover.

8.2 Development Proposals

The development proposals for the Butler Childcare Centre include a proposed 1.5m wide footpath along the southern side of the Childcare Centre building – providing a footpath connection between the existing footpaths along the Existing Connector Road and Exmouth Drive.

The proposed 1.5m wide footpath would be covered alongside the proposed Childcare Centre building. The 1.5m wide footpath includes the width reduction due to retained vegetation/landscaping to provide a clear 1.5m wide path.

The new footpath connection would enable pedestrians to walk safely between the proposed Childcare Centre and Butler Station and Butler Central Shopping Centre.

Figure 17 shows the location of the proposed connecting footpath infrastructure.





Figure 17 Connecting footpath infrastructure for the Butler Childcare Centre (base plan source: Peter Hobbs Architects, 2022)



BICYCLE ACCESS AND AMENITY

9.1 Existing Cycle Network

The proposed Butler Childcare Centre site has a good level of bicycle accessibility, with the existing cycle network shown in Figure 18.

A Principal Shared Path (PSP) runs along the western side of the Joondalup Line rail corridor and provides access from the south to Butler Station at the end of the Joondalup Line. From the PSP the local path network provides access to the proposed Childcare Centre.

Butler Boulevard to the south of the proposed Childcare Centre includes regular shared paths on both the northern and southern sides of the corridor.

A network of regular shared paths to the north of the proposed Childcare Centre provide a connections between residential areas to the north and the Childcare Centre site.



Figure 18 Existing bicycle network surrounding the proposed Butler Childcare Centre site (source: City of Wanneroo and Department of Transport, Wanneroo-North Active Travel Map)



A heatmap of cycle activity in the vicinity of the proposed Butler Childcare Centre site is shown in Figure 19. This is produced by cyclists tracking their trips using the commercial product Strava. The local regular shared paths in the residential areas to the north of the Childcare Centre site are well used bicycle routes, as well as higher order bicycle routes along Butler Boulevard and the Joondalup Line PSP.



Figure 19 Strava heatmap for cycling in vicinity of the proposed Butler Childcare Centre site (source: Strava)

9.2 Future Cycle Network

Between 2018-2020 the Department of Transport worked with 33 local governments across Perth and Peel on the Long Term Cycle Network (LTCN) project. The LTCN project has been a collaboration between State and local governments to agree on an aspirational network of bicycle routes that link parks, schools, community facilities and transport services, to make cycling a convenient and viable option for more people and more journeys.

The aim of the project was to develop an aspirational blueprint to ensure State and local governments work together towards the delivery of one continuous cycling network providing additional transport options, recreational opportunities and support for tourism and commercial activity.



In June 2020 the City of Wanneroo Council endorsed their LTCN – from July 2020 the LTCN is eligible for the City to seek grant funding support from DoT to deliver bicycle infrastructure along the identified routes – as shown in Figure 20.

The identified LTCN shows the long term goal to extend the Primary Route along the Joondalup Line (the existing PSP) north of Butler Station with the northern extension of the rail line. This would extend the northern cycling catchment of the site to residential catchments along the rail corridor to the north of the Childcare Centre site.

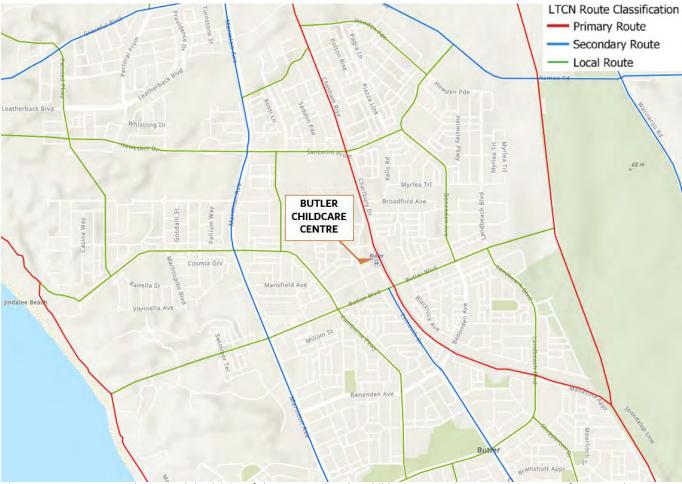


Figure 20 Long Term Cycle Network in vicinity of the proposed Butler Childcare Centre (source: Department of Transport)



10. SITE SPECIFIC ISSUES

There are no additional site specific issues that are required to be addressed as part of this TIS.



11. SAFETY ISSUES

11.1 Crash History

In the five-year period ending November 2020, there were the following reported crashes within immediate proximity of the proposed Butler Childcare Centre site:

- Exmouth Drive x4 crashes
 - o x1 midblock crash south of Exmouth Drive roundabout single vehicle collision with an animal resulting in major vehicle property damage.
 - o x1 midblock crash to the north of the access to the existing Butler Central Shopping Mall northern car park collision between two vehicles, one exiting on-street parking bay resulting in major vehicle property damage.
 - o x1 midblock crash to the south of the access to the existing Butler Central Shopping Mall northern car park collision between two vehicles, one exiting on-street parking bay resulting in minor vehicle property damage.
 - o x1 intersection crash at the intersection of Exmouth Drive and the existing Butler Central Shopping Mall Central Avenue collision between two vehicles resulting in minor vehicle property damage.

No other crashes were reported during the five-year period ending November 2020 on any other boundary road to the proposed Butler Childcare Centre site.

Figure 21 shows the recorded location of the reported crashes (blue dots) in relation to the proposed Butler Childcare Centre site.



Figure 21 Crash Summary for five-year period ending November 2020 in proximity to the proposed Butler Childcare Centre site (source: Main Roads WA)



12. SUMMARY

12.1 Development Proposals

This TIS has been prepared by Flyt in support of the proposed development of the current vacant land at 121 Exmouth Drive, Butler – into a Childcare Centre.

The proposed development of the existing vacant land into a Childcare Centre can be summarised as comprising:

- Childcare Centre to accommodate 92 children
 - o 22 children between the ages of 0-2 years
 - o 30 children between the ages of 2-3 years
 - o 40 children of kindergarten (kindy) age
- Childcare Centre to be serviced under the following staff arrangements:
 - o 15 educators (Monday-Friday, full time)
 - o Additional part-time educators covering staff breaks (Monday-Friday, 10am-3pm)
 - o 1 chef (Monday-Friday, half day)
 - o 1 centre area manager (visits site once or twice a week between 10am-3pm)

The proposed Butler Childcare Centre will have 92 children on-site, with 15 full-time 'room ratio' educators plus additional staff to perform support functions for the facility. Support staff can include a cook, an area manager, and approximately 5 'lunch cover' staff that attend to cover the full time educator's breaks, though it is noted that the number of lunch cover staff can vary.

12.2 Vehicle Access and Parking

The proposed Butler Childcare Centre is located at 121 Exmouth Drive in Butler. The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Mall car park to the south and Butler Central Shopping Mall access road to the west.

The proposed Butler Childcare Centre will have approximately 18 staff and 92 children on-site. The site has the following parking requirements und the City of Wanneroo Local Planning Policy 2.3:

• Staff parking bays 15 bays (for full-time staff)

Parent drop-off / pick-up parking bays
 TOTAL REQUIRED PARKING BAYS
 29 BAYS

The proposed Butler Childcare Centre is located adjacent to Butler Central Shopping Centre Mall northern car park. The northern car park contains 94 standard parking bays and 4 universal access parking bays – a total of 98 parking bays.

The 14 parking bays in the northern Shopping Centre car park located nearest to the entrance of the proposed Butler Childcare Centre would be utilised by parents dropping-off and picking-up children at the Childcare Centre. These bays will provide direct access to the footpath to be constructed along the southern side of the Childcare Centre.

It is expected that the following factors will reduce the demand of parent parking at the site:

- Activity Centre location creating opportunities for multi-use trips.
- Higher density area resulting in significant number of families within the walkable catchment.
- Proximity to the train station and associated parking areas, particularly the likelihood of parents combining drop off / pick up with their daily commute via bus or rail.
- Extent of public train station parking available, which may be used by parents commuting and using the childcare facility.



These above factors are all unique to the proposed site of the Butler Childcare Centre site and would not apply under a typical Childcare Centre scenario. With these factors considered, parent parking demand is expected to be reduced.

Of the 15 full-time staff it is expected that some may drive to the site and others will car pool or use public transport to access the site. In this regard, it is noted that the site is a short 3 minute walk from Butler Station providing access to bus and train services. As such, the site is conveniently located to be accessed by public transport services.

Additional support staff would attend the site over the course of a day to perform various support functions. These staff are not present at the site for a whole day and are typically in attendance outside of the peak pick-up / drop-off periods (when there is no parent parking demand). Ample opportunity for support staff to attend the site is expected, given there is a significant amount of parking available in the immediate area, as well as excellent access to alternate modes of transport.

12.3 Provision for Service Vehicles

The proposed Butler Childcare Centre facility is not a delivery-intensive land use. Deliveries will be made by vans or similar sized vehicles and would occur during the day within the bays immediately adjacent the Childcare Centre. No specific loading areas/bays are proposed or required to support the Childcare Centre development.

A separate Waste Management Plan has been prepared to support the Development Application for the Butler Childcare Centre. Please see the Waste Management Plan for all details of the planned waste collection regime for the development proposal.

12.4 Traffic Impact

The proposed Butler Childcare Centre it estimated to generate a total of 82 vehicle trips to/from the site during the developments AM peak hour and 66 vehicle trips to/from the site will occur during the development PM peak hour.

The proposed Butler Childcare Centre is unlikely to generate significant additional vehicle trips on the road network – many of the vehicle trips to drop-off and pick-up children from the Childcare Centre would be part of a linked trip already being made. The majority of linked trip will be part of the parents commute to their place of work and/or school drop-off / pick-up of older children.

The Childcare Centre will generate more vehicle movements during the developments AM peak hour (more concentrated child drop-off activity) as opposed to during the developments PM peak hour (more dispersed child pick-up activity).

Whilst the AM peak hour drop-off activity at the proposed Childcare Centre is more concentrated than the PM peak hour, the site benefits from far lower levels of localised vehicle activity associated with Butler Central Shopping Mall during the AM peak hour, as well as the traffic volumes on Butler Boulevard indicating lower levels of traffic on the local road network during the AM peak hour compared to the PM peak hour. The AM peak traffic represents 43% of combined AM and PM peak hour movements, compared to the PM peak traffic representing 57% of combined AM and PM peak hour movements.

It is expected that the following factors will reduce the demand of parent parking at the site:

- Activity Centre location creating opportunities for multi-use trips.
- Higher density area resulting in significant number of families within the walkable catchment.
- Proximity to the train station and associated parking areas, particularly the likelihood of parents combining drop off / pick up with their daily commute via bus or rail.
- Extent of public train station parking available, which may be used by parents commuting and using the childcare facility.

These above factors are all unique to the proposed site and would not apply under a typical Childcare Centre scenario. With these factors considered, parent parking demand is expected to be reduced.



12.5 Public Transport Access

The proposed Butler Childcare Centre site is conveniently located to be accessed by public transport services – these include bus and train services from Butler Station.

From the entrance of the proposed Childcare Center bus services from Butler Station can be accessed with a 200m-250m walk (2-3 minute walk time) and train services from Butler Station can be accessed with a 300m walk (3-4 minute walk time).

<u>Bus services</u> – bus routes from Butler Station provide connections to the residential areas to the north of the proposed Childcare Centre via Hollington Boulevard and Santorini Promenade, and connection to the residential areas to the southeast and southwest of the proposed Childcare Centre via Benenden Avenue, Landbeach Boulevard (to the southeast) and Camborne Parkway (to the southwest).

<u>Train services</u> – train service operate from Butler Station and provide connections south to Joondalup, Stirling, Leederville, Perth Underground, Elizabeth Quay and through routed train services to Canning Bridge, Murdoch, Cockburn, Rockingham and Mandurah.

12.6 Pedestrian Access

The proposed Butler Childcare Centre site is located in a central area of Butler adjacent to Butler Station and Butler Central Shopping Mall and has good levels of pedestrian connectivity with footpaths on both sides of most surrounding streets.

The sites 15-minute walkable catchment includes destinations such as Butler Station, Butler Central Shopping Mall and John Butler Primary College – as well as residential areas fringing central butler.

The development proposals for the Butler Childcare Centre include a proposed 1.5m wide footpath along the southern side of the Childcare Centre building – providing a footpath connection between the existing footpaths along the Existing Connector Road and Exmouth Drive.

The proposed 1.5m wide footpath would be covered alongside the proposed Childcare Centre building. The 1.5m wide footpath includes the width reduction due to retained vegetation/landscaping to provide a clear 1.5m wide path.



Attachment 10 - Parking Management Plan

81113-744-FLYT-TEN-0006



Technical Note Parking Management Plan	
Project	Butler Childcare Centre (121 Exmouth Drive, Butler)
Date Issued	21 November 2022

1. INTRODUCTION

This Parking Management Plan has been prepared by Flyt in support of the proposed development of a Childcare Centre at 121 Exmouth Drive, Butler. The proposed site is located between the Public Transport Authority's Butler Station car park (to the north) and the Butler Central Shopping Centre and car park (to the south).

The development does not propose to provide additional parking (noting the development site is constrained and unable to provide onsite parking), instead the northern section of the Buter Shopping Centre car park (currently with 94 standard bays and a further 4 accessible bays) will be utilised. The northern car park is intended to provide parking for the surrounding Activity Centre land uses, rather than providing parking within individual sites. In this way the parking is shared between land uses, and visitors can park once but visit many land uses (multi-purpose trips).

Given the proximity of the site to Butler Station, there are expected to be parking reduction factors for both staff and parents resulting in lesser parking demand than a typical childcare centre

This introduction forms the first section of the Parking Management Plan of which there are a further 6 sections:

- Development proposals
- Site context
- Parking allocation
- Parking management strategies
- Management of pedestrians and cyclists

2. DEVELOPMENT PROPOSALS

The proposed development of the existing vacant land into a Childcare Centre can be summarised as comprising:

- Childcare Centre to accommodate 92 children
 - 22 children between the ages of 0-2 years
 - 30 children between the ages of 2-3 years
 - 40 children of kindergarten (kindy) age
- Childcare Centre to be serviced under the following staff arrangements:
 - 15 educators (Monday-Friday, full time)
 - Additional part-time educators covering staff breaks (Monday-Friday, 10am-3pm)
 - 1 chef (Monday-Friday, half day)
 - 1 centre area manager (visits site once or twice a week between 10am-3pm)

Figure 1 shows the site plan for the proposed Butler Childcare Centre and Figure 2 shows the site floor plan for the proposed Butler Childcare Centre.





Figure 1 Site plan for the proposed Butler Childcare Centre (source: Peter Hobbs Architects, 2022)



Figure 2 Site floor plan for the proposed Butler Childcare Centre (source: Peter Hobbs Architects, 2022)



The proposed Butler Childcare Centre would operate from 6.30am-6.30pm Monday-Friday. Based on the operation of other childcare centres with similar opening hours, children are typically dropped off between 6:30 and 9:30am, with the busiest hour being 7:30 – 8:30am. In the afternoon children are typically collected between 3:30 and 6:30pm, with the afternoon peak hour being 4:30-5:30pm.

3. SITE CONTEXT

The proposed Butler Childcare Centre site is located at 121 Exmouth Drive, Butler. The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Centre car park to the south and Butler Central Shopping Centre access road to the west.

The site is conveniently located to access Butler Station (a 300m walk from the entrance of the proposed Childcare Centre to the Station platforms) and Butler Central Shopping Centre (a 150m walk from the entrance of the proposed Childcare Centre to the entrance to the Shopping Centre).

As shown in Figure 3, the proposed Butler Childcare Centre is conveniently located with a range of facilities within a 400m walking catchment including; Butler Station, Butler Central Shopping Centre, large format retail units (currently under construction), Clipstone Park, Wilton Park, Manningtree Park, Halesworth Park and John Butler Primary School.

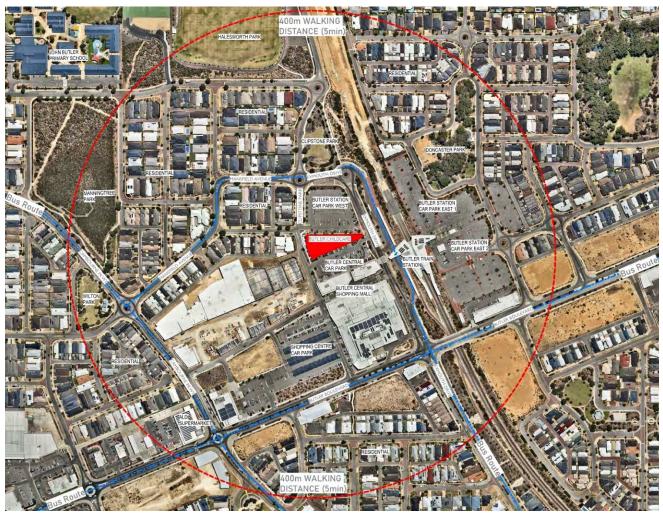


Figure 3 Location of the proposed Butler Childcare Centre (source: Peter Hobbs Architects, 2022)



The site has boundaries with the Public Transport Authority Butler Station car park to the north, Exmouth Drive to the east, Butler Central Shopping Centre car park to the south and Butler Central Shopping Centre access road to the west – as shown in Figure 4.



Figure 4 Detailed location of the proposed Butler Childcare Centre (aerial image source: MetroMap)

3.1 Public Transport

The proposed Butler Childcare Centre site is conveniently located for access to public transport services – these include bus and train services from Butler Station.

From the entrance of the proposed Childcare Center, bus services from Butler Station can be accessed with a 200m-250m walk (2-3 minute walk time) and train services from Butler Station can be accessed with a 300m walk (3-4 minute walk time).

<u>Bus services</u> – bus routes from Butler Station provide connections to the residential areas to the north of the proposed Childcare Centre via Hollington Boulevard and Santorini Promenade, and connection to the residential areas to the southeast and southwest of the proposed Childcare Centre via Benenden Avenue, Landbeach Boulevard (to the southeast) and Camborne Parkway (to the southwest).

<u>Train services</u> – train service operate from Butler Station and provide connections south to Joondalup, Stirling, Leederville, Perth Underground, Elizabeth Quay and through routed train services to Canning Bridge, Murdoch, Cockburn, Rockingham, and Mandurah.

Figure 5 shows the location of the proposed Butler Childcare Centre site in relation to public transport routes.





Figure 5 Public transport network in vicinity of the proposed Butler Childcare Centre site (source: City of Wanneroo and Department of Transport, Wanneroo-North Active Travel Map)

The closest bus stops to the proposed Butler Childcare Centre site are shown in Figure 6. The bus stands at Butler Station are a 200-250m walk from the site, via the proposed new 1.5m footpath along the southern boundary of the Childcare Centre and across the raised pedestrian crossing over Exmouth Drive to access Butler station.

Butler Station also provides train services to the south towards central Perth, with the station platforms a 300m walk from the Childcare Centre site – and accessed via the same safe high quality pedestrian route as used to access the bus stands.





Figure 6 Walking distance from proposed Childcare Centre to bus and train services at Butler Station (aerial image source: MetroMap)

3.1.1 Bus Services

Bus routes serving the bus stands at Butler Station provide connections to the residential areas to the north and south of the proposed Butler Childcare Centre site. The following key bus routes are those that are most likely to be used by parents with their children to access the Childcare Centre:

• Bus Route 483

- Route 483 operates between Clarkson Station and Alkimos via Merriwa and Butler Station. In proximity of the proposed Childcare Centre site the bus route operates via Camborne Parkway, Butler Boulevard, Butler Station, Mansfield Avenue, Hollington Boulevard and Santorini Promenade.
- Weekday bus services operate between 5am and 10pm operating on a 10 minute frequency across a
 2 hour AM and PM peak period and operating on a 30 minute frequency outside of these times.
- Saturday bus services operate between 6am and 10pm operating on a 60 minute frequency.
- Sunday bus services operate between 7am and 8pm operating on a 60 minute frequency.

Bus Route 484

- Route 484 operates between Clarkson Station and Alkimos via Ridgewood and Butler Station. In proximity of the proposed Childcare Centre site the bus route operates via Benenden Avenue, Butler Station, Butler Boulevard, Landbeach Boulevard and Shepperton Drive.
- Weekday bus services operate between 5am and 8pm operating on a 10 minute frequency across a 2 hour AM and a 12 minute frequency across a 2 hour PM peak period – and operating on a 30 minute frequency outside of these times.
- O Saturday bus services operate between 6am and 8pm operating on a 60 minute frequency.
- O Sunday bus services operate between 7am and 7pm operating on a 60 minute frequency.



3.1.2 Train Services

Butler Station is currently the end of line station on the Joondalup Line and provides connections south towards central Perth (construction of the 14.5km northern extension to Yanchep is underway and expected to be operational by late 2023). From Butler Station the following key locations can be accessed without the need to transfer train services/train lines; Joondalup, Stirling, Leederville, Perth Underground, Elizabeth Quay and through routed train services to Canning Bridge, Murdoch, Cockburn, Rockingham, and Mandurah.

Butler Station provides access to the following rail services:

- Butler Station services operate towards Perth (38 minute journey time):
 - Weekday rail services operate between 5am and 11.30pm operating on a 10 minute frequency during the morning and afternoon peak periods and operating on a 15 minute frequency at other times (other than after 8pm where services operate on a 30 minute frequency).
 - Saturday rail services operate between 5.30am and 11.30pm operating on a 15 minute frequency (other than after 8pm where services operate on a 30 minute frequency).
 - Sunday rail services operate between 7am and 11.30pm operating on a 15 minute frequency (other than after 7pm where services operate on a 30 minute frequency).



4. PARKING ALLOCATION

The proposed Butler Childcare Centre will have 92 children on-site, with 15 full-time 'room ratio' educators plus additional staff to perform support functions for the facility. Support staff can include a cook, an area manager, and approximately 5 'lunch cover' staff that attend to cover the full time educator's breaks, though it is noted that the number of lunch cover staff can vary. The site has the following parking requirements under the City of Wanneroo Local Planning Policy 2.3:

Staff parking bays
 15 bays (for full-time staff)

Parent drop-off / pick-up parking bays
 TOTAL REQUIRED PARKING BAYS
 29 BAYS

The proposed Butler Childcare Centre is located adjacent to the Butler Central Shopping Centre northern car park. The northern car park currently contains 94 standard parking bays and 4 universal access parking bays – a total of 98 parking bays., however the development proposes to convert two existing bays into an accessible parking bay with an adjacent shared space. This will leave the northern car park with 92 standard bays and 5 accessible bays – a total of 97 parking bays.

The northern car park is intended to provide parking for the surrounding Activity Centre land uses, rather than providing parking within individual sites. In this way the parking is shared between land uses, and visitors can park once but visit many land uses (multi-purpose trips).

The northern car park is not the main car park for the Shopping Centre, with covered parking bays available in the main car park located to the west of the Shopping Centre, much closer for access to the major tenant Woolworths. The northern car park is the closest parking area for access to the tenants of the northern buildings of the shopping centre, with tenants 3 Sheets Inn, Butler Laundrette, Best & Less and Butler Imaging. The tenants of the northern building face onto the main street (the continuation of Central Avenue), with blank walls presenting to the northern car park. The layout and pedestrian entries to the shopping centre (including the northern buildings) are shown in Figure 7.



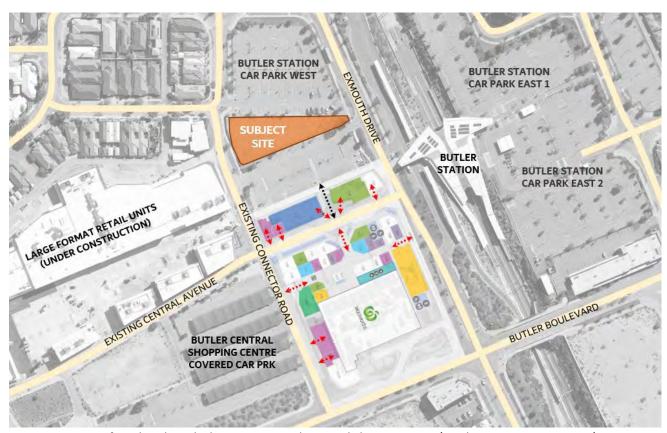


Figure 7 Location of Woolworths and other tenants at Butler Central Shopping Centre (aerial image source: MetroMap)

The proposed Butler Childcare Centre would operate from 6.30am-6.30pm Monday-Friday. Based on the operation of other childcare centres with similar opening hours, children are typically dropped off between 6:30 and 9:30am, with the busiest hour being 7:30-8:30am. In the afternoon children are typically collected between 3:30 and 6:30pm, with the afternoon peak hour being 4:30-5:30pm.

Butler Central Shopping Centre has core trading hours of 9am-5.30pm Monday, Tuesday, Wednesday, Friday and 9am-8pm Thursday. Major tenant Woolworths operates 8am-9pm Monday-Friday. The operating hours of the four tenants of the northern buildings of the Butler Shopping Centre are outlined in Table 1.

Table 1 Operating hours of tenants in northern shopping centre buildings

Shopping Centre		Opening Hours Wed Thurs Fri		 Potential Conflict with Childcare 	
Tenant	Mon – Wed				
3 Sheets Inn	11:30 – 21:00	11:30 – 22:00	11:30 – 23:30	Opens well after the childcare centre,	
(pub)	11:30 – 21:00	11:30 – 22:00	11:30 – 23:30	peak times do not coincide	
Best & Less	9:00 – 17:30	10:00 – 18:00	9:00 – 17:30	Opens after the childcare centre	
(discount clothes)	9:00 – 17:30	10:00 – 18:00	9:00 - 17:30	morning peak period	
Butler Laundrette	7:00 – 21:00	7:00 – 21:00	7.00 24.00	Minimal parking demand throughout	
Butier Laundrette	7:00 – 21:00	7:00 – 21:00	7:00 – 21:00	the day	
Duales Imaging				Opens after the childcare centre	
Butler Imaging	8:30 - 16:30 8:30 -	8:30 - 16:30	8:30 - 16:30	morning peak period and closes	
(medical imaging)				before the afternoon peak	



With the exception of the laundrette, the tenants of the northern buildings of the Butler Shopping Centre open after the predicted morning peak of the childcare centre. The medical imaging tenant closes before the predicted afternoon peak of the childcare centre.

The following factors will reduce the demand of parent parking at the site:

- The Activity Centre location creates opportunities for multi-purpose trips.
- The surrounding higher density residential area results in a significant number of families within the walkable catchment.
- The proximity to the train station and associated parking areas, particularly the likelihood of parents combining drop off / pick up with their daily commute via bus or rail.
- The extent of public train station parking available (as the childcare centre is between the Butler Station car
 park and the actual station), which commuting parents may use, while walking children to the childcare facility.

Given the proximity of the proposed Childcare Centre to Butler Station, some parents will park to the north of the Childcare Centre in the Transperth Park and Ride car park as part of their commute by train from Butler Station. These parents are likely to walk their children to the Childcare Centre from the Transperth car park as part of their walk to the Station.

Even though it is likely that the demand for parent parking will be lower at this site than a typical childcare centre, it is proposed that the 14 parking bays in the northern car park closest to the entrance of the Butler Childcare Centre be utilised by parents dropping-off and picking-up children. These bays will provide direct access to the footpath which will be constructed adjacent to the southern boundary of the Childcare Centre. The two closest bays will be converted into an accessible bay with an adjacent shared space.

Of the 15 full-time staff it is expected that some may drive, car pool, walk or cycle, and others will use public transport to access the site. The Childcare centre is a short 3 minute walk from Butler Station's bus and train services. Additional staff would attend the site over the course of a day to perform various support functions. These staff are not present at the site for a whole day and are typically in attendance outside of the peak pick-up / drop-off periods (when there is no parent parking demand).

It is not proposed to allocate specific bays to staff, rather staff will be able to use unoccupied bays within the northernmost section of the car park (not the parent drop off and pick up bays). As the childcare centre will open before any of the shopping centre's northern building tenancies, most bays within the car park will be unoccupied prior to the arrival of staff.

The proposed parking bay allocation for parents across the northern car park are shown in Figure 8.





Figure 8 Proposed parking bay allocation for the Butler Childcare Centre (base plan source: Peter Hobbs Architects, 2022)



5. PARKING MANAGEMENT STRATEGIES

5.1 Control of Access

The proposed Childcare Centre site benefits from access and use of shopping centre parking spaces in respect of easements outlined in the Lots Certificate of Title (Volume 2969 / Folio 53) and Deposited Plan (406016) with the land notable affected by the following easements:

- 'C' on Deposited Plan right of carriageway as per Section 136C of the Transfer of Land Act benefit to Lot 2812 [site of the proposed Butler Childcare Centre].
- 'Z' on Deposited Plan access for public at large as per Section 195 & 196 of the Land Administration Act refer to the attached easement document which confirms Z is also a public parking easement [see figure below].

Figure 9 shows the location of the proposed Butler Childcare Centre and an extract from Deposited Plan (406016) and the easement afforded to the Lot of the proposed Childcare Centre in respect of 'C' and 'Z'.

Figure 10 to Figure 13 show the routes of vehicle access to the proposed Butler Childcare Centre site car parking from the Butler Central Shopping Centre Existing Connector Road and from Exmouth Drive.







Figure 9 Vehicle access arrangement and site easements for the site of the proposed Butler Childcare Centre (source: Deposited Plan 406016, Landgate)



Figure 10 and Figure 11 show the route of vehicle access to the proposed Butler Childcare Centre site car parking from the Butler Central Shopping Centre Existing Connector Road.



Figure 10 View along the Existing Connector Road southbound on approach to the access into the Butler Central Shopping Centre northern car park (source: Google Streetview)



Figure 11 Existing access from the Existing Connector Road into the Butler Central Shopping Centre northern car park – the Butler Childcare Centre is proposed to occupy the vacant Lot on the left side of the Streetview image (source: Google Streetview)



Figure 12 and Figure 13 show the route of vehicle access to the proposed Butler Childcare Centre site car parking from Exmouth Drive.



Figure 12 View along Exmouth Drive southbound on approach to the access into the Butler Central Shopping Centre northern car park (source: Google Streetview)



Figure 13 Existing access from Exmouth Drive into the Butler Central Shopping Centre northern car park – the Butler Childcare Centre is proposed to occupy the vacant Lot in the centre of the Streetview image (source: Google Streetview)

5.2 Management of Parking Bay Use

As the northern car park is available for shared parking for all surrounding land uses, there is no need to allocate or identify bays with specific uses.

An ACROD bay and associated shared space will be clearly marked and signposted as per the requirements of Australian Standards 2890 Parking Standards Part 6: Off-Street Parking for People with Disabilities. Use of the ACROD bay will be limited to ACROD permit holders only.



The typical staff parking operations can be outlined as follows:

- Full-time staff arrival and departure from the site is staggered based on their shift patterns, which will be based
 on the required educator to children ratio for each room. All full-time staff are typically on-site between 9am3.30pm.
- Additional part-time/occasional support staff (i.e., educator lunch cover) will occur outside of peak dropoff/pick-up periods for children – when the on-site parent parking is not utilised.
- The site chef attends for half a day and departs after the conclusion of lunch service.
- The centre area manager attends once or twice a week outside of peak drop-off/pick-up periods for children –
 when the on-site parent parking is not utilised.

The typical parent parking requirements can be outlined as follows:

- Typically, the drop-off or pick-up of children from Childcare Centres takes 10-12 minutes.
- Using an average time of 12 minutes for drop-off or pick-up the 14 parent parking bays on-site (excluding the 1 ACROD bay) would turnover approximately 5 times during the peak hour and accommodate approximately 70 vehicle movements.

If parents choose alternative modes of transport to access the site to drop-off or pick-up their children, this will serve to reduce the use of on-site parent parking bays.

Outside of the drop-off and pick-up peak hour periods, there is no demand for parent parking.

5.3 Key Parking Reduction Strategies

Staff will be encouraged to access the Childcare Centre site by non-private vehicle modes through a series of measures:

- The operator supports local employment and will give due consideration to local job applicants, as this may secure flexibility in staff commute via alternate modes of transport.
- Staff will be encouraged to take advantage of public transport and will be provided with an information pack containing details of public transport routes and timetables at the time of induction.
- The operator will encourage and facilitate staff car-pooling by a parking roster and notice board in the staff room.



6. MANAGEMENT OF PEDESTRIANS AND CYCLISTS

The development proposals for the Butler Childcare Centre include a proposed 1.5m wide footpath along the southern side of the Childcare Centre building – providing a footpath connection between the existing footpaths along the Existing Connector Road and Exmouth Drive.

The new footpath connection would enable pedestrians to walk safely between the proposed Childcare Centre and Butler Station and Butler Central Shopping Centre. It would also allow parents and carers walking with small children to walk between the proposed Childcare Centre and the proposed allocation of parent parking bays without having to walk through the car park.

An additional zebra crossing is proposed within the car park northern aisle.

Figure 14 shows the location of the proposed connecting footpath infrastructure.

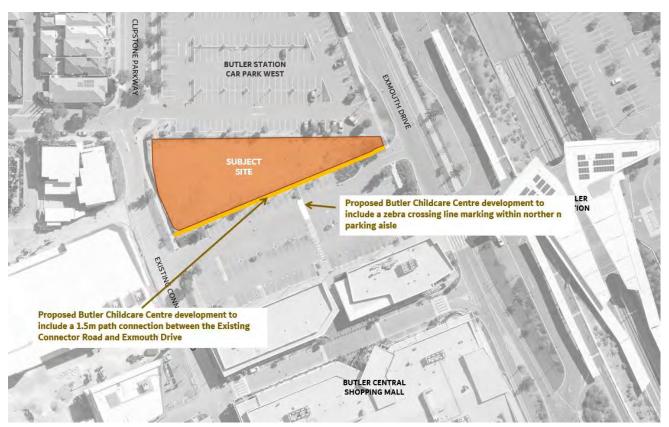


Figure 14 Connecting footpath infrastructure for the Butler Childcare Centre (base plan source: Peter Hobbs Architects, 2022)

Attachment 11 - Access Statement

FEBRUARY 15, 2023

121 EXMOUTH DRIVE
BUTLER, WA
ACCESS STATEMENT

PREPARED BY JORDAN BUSHER



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Company Information

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Document Control

File	Version	Date	Review
2022/112_JarraProp_AccStat121ex_V1	1	15/02/2023	JB

Statement of Qualifications

Francis Consulting is an Australian consultancy that is dedicated to the provision of independent, reliable advice pertaining to access. Our proficiency is in matters relating to the provision of equitable and dignified access for people who have a disability. Francis Consulting delivers consulting services to the design and construction industry, as well as local and state government authorities.

I hereby certify that I am an appropriately qualified and competent professional practicing in the relevant area to which I am qualified. I have recognised practical experience in the area of work assessing disability access compliance and hold suitable insurance policies relating to my work.

Reviewed by: Jordan Busher on 15 February 2023

Position: Director, Access Consultant at Francis Consulting

Qualification: Certificate IV in Access Consulting

Bachelor of Urban and Regional Planning

Jordan Busher is a qualified Access Consultant and Town Planner with over 8 years' experience in the development sector. Jordan has gained experience both locally and internationally within both the public and private sectors and has a keen interest and thorough understanding of Disability Access, Town Planning and Specialist Disability Accommodation.

Introduction

Thank you for the opportunity to provide Disability Access Consulting services applicable to the preparation of an Access Statement for works located at the proposed childcare establishment located at 121 Exmouth Drive, Butler WA (herein referred to as the 'subject site'). Francis Consulting has been tasked with reviewing and responding to a query relating to universal accessible vehicle parking bays adjacent to the Class 9b childcare establishment, neighbouring the existing Woolworths shopping centre. The following Access Statement relates to this identified issue only and no further or other aspect of this project was reviewed at this time.

Access Statement

I am writing to provide Jarra Property with a statement regarding the proposed use of universal accessible car parking bays from the adjacent tenancy for a development application at 121 Exmouth Drive, Butler WA. As an access consultant, I have reviewed the provided plans for the proposed development and confirm that the reciprocal use of these parking bays is satisfactory from an access point of view for people with disabilities.

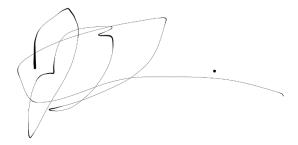
I have conducted an access assessment of the proposed development site and the adjacent shopping centre tenancy to identify any existing accessibility barriers from parking to principal pedestrian entry point. In addition, I have provided advice to Scott Ferguson, Jarra Property on the design of the universal accessible car parking bays, including the location, dimensions, and number required to meet the standards.

The proposed situation is not considered to have an impact upon the impending building application. In regard to the existing disabled bays located within the Woolworths car park, I can confirm that these are capable of complying with the relevant Australian standard for off-street vehicle parking for people with disabilities, AS2890.6 2009. Jarra Property have confirmed that the dimensions of the bays and associated access paths to the principal pedestrian entry will meet the minimum requirements set out in the standard and are in accordance with the Disability (Access to Premises – Buildings) Standards 2010.

Furthermore, Francis Consulting is of the professional opinion that this arrangement will not impact a future building application.

In conclusion, I am confident that the proposed use of universal accessible car parking bays from the adjacent tenancy for the development project at 121 Exmouth Drive, Butler is capable of meeting the relevant section of NCC Volume 1 (2019) and relevant Australian Standards and will not impact a future building application. Please do not hesitate to contact me if you have any questions or require further information.

Sincerely,



Jordan Busher - JFB Consulting (WA) Pty Ltd t/a Francis Consulting Access Consultant

This report is owned, and all rights reserved to JFB CONSULTING (WA) PTY LTD. The use of this report and all professional opinions provided within said report may not be used for any other purpose, unless written authorisation is provided. Refer to the Building Certifier for all areas of compliance. This report does not confirm compliance, unless otherwise specifically detailed within this report, with the following: BCA 2019, OHS Risk of injury for staff regulations, Work Health and Safety Act, The Disability (Access to Premises – Buildings)

Standards 2010, or determining compliance with the Federal Disability Discrimination Act 1992.

Attachment 12 - Waste Collection Location



LEGEND



EXISTING TREE to be retained & protected



PROPOSED TREE nominal location shown

CHILDCARE CENTRE ANALYSIS

OPERATION HOURS 6:30am to 6:30pm Monday to Friday with up to four days open on the weekend for open days

GROUP ROOM 3 24-36months GROUP ROOM 5 36+ months GROUP ROOM 6 36+ months

92 PLACES 17 STAFF

1 EDUCATORS 2 EDUCATORS

2 EDUCATORS 2 EDUCATORS + 2 STAFF

AREA ANALYSIS

TOTAL SITE AREA: 2215m² SITE COVERAGE: 817m² (37%)

GFA: 778m² gross leaseable area

1 SITE PLAN
1:200

GENERAL NOTES			
DIMENSIONS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT, PREPARATION OF SHOP			
DRAWINGS OR MANUFACTURING. FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALING.			
VERIFY LOCATION OF EXISTING SERVICES BEFORE COMMENCEMENT.	4	DEVELOPMENT APPLICATION SET	01/12/2022
ALL CONSTRUCTION TO BE IN ACCORDANCE	3	DEVELOPMENT APPLICATION SET	21/11/2022
WITH THE NATIONAL CONSTRUCTION CODE OF	2	DEVELOPMENT APPLICATION SET	03/10/2022
AUSTRALIA, BUILDING ACT 1975 AS AMENDED, STANDARD BUILDING BY-LAWS AND RELEVANT	1	DEVELOPMENT APPLICATION SET	06/09/2022
AUSTRALIAN STANDARDS.	ISSUE	DESCRIPTION	DATE

insite



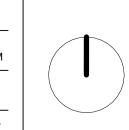
CLIENT: JARRA PROPERTY

PROPOSED CHILDCARE CENTRE (92 places) FILE: C:\Users\TEMP\Documents\J0000734_EXMOUTH DVE_BULTER_DA_23_sowmyaQCJDT.rvt

LOCATION: 121 EXMOUTH DRIVE, BUTLER, WA

DRAWING TITLE: SITE PLAN

SCALE:	As indicated@A1	DATE:	DEC 2022
DRAWN:	SS	PRINTED:	1/12/2022 3:28:32 PM
CHECKED	: AH	JOB No.:	J0000734
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D/A ISSUE

Attachment 12 - Waste Managment Plan



Waste Management Plan

Lot 2812 (121) Exmouth Drive, Butler

Prepared for Ladybug Eleven Pty Ltd c/- Apex Planning

3 October 2022

Project Number: TW22067



DOCUMENT CONTROL

Version	Description	Date	Author	Reviewer	Approver
1.0	First Approved Release	8/09/2022	AB	RH	DP
2.0	Second Approved Release	3/10/2022	AB	RH	DP

Approval for Release

Name	Position	File Reference
Rachel Hayton	Project Manager – Waste Management Consultant	TW22067-02_Waste Management Plan_2.0
Signature		

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Executive Summary

Ladybug Eleven Pty Ltd is seeking development approval for the proposed childcare development located at Lot 2812 (121) Exmouth Drive, Butler (the Proposal).

To satisfy the conditions of the development application the City of Wanneroo (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

A summary of the bin size, numbers, collection frequency and collection method is provided in the below table.

Proposed Waste Collection Summary

Waste Type	Generation (L/week)	Bin Size (L)	Number of Bins	Collection Frequency	Collection
		Bin Stora	age Area		
Refuse	1,432	240L	Three	Two times each week	Private Contractor
Recycling	1,432	240L	Three	Two times each week	Private Contractor

A private contractor will service the bins directly from the Bin Storage Area from the kerbside utilising the laneway located off the corner of Haverhill Road and Clipstone Parkway.

The building manager/caretaker will oversee the relevant aspects of waste management at the Proposal.



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Diagram 1: Waste Collection Location

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Figure 1: Locality Plan

Figure 2: Bin Storage Area



1 Introduction

Ladybug Eleven Pty Ltd is seeking development approval for the proposed childcare development located at Lot 2812 (121) Exmouth Drive, Butler (the Proposal).

To satisfy the conditions of the development application the City of Wanneroo (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

The Proposal is bordered by Butler Station Carpark to the north, Exmouth Drive to the east, commercial properties to the south and residential properties to the west, as shown in Figure 1.

1.1 Objectives and Scope

The objective of this WMP is to outline the equipment and procedures that will be adopted to manage waste (refuse and recyclables) at the Proposal. Specifically, the WMP demonstrates that the Proposal is designed to:

- Adequately cater for the anticipated volume of waste to be generated;
- Provide adequately sized Bin Storage Area, including appropriate bins; and
- Allow for efficient collection of bins by appropriate waste collection vehicles.

To achieve the objective, the scope of the WMP comprises:

- Section 2: Waste Generation;
- Section 3: Waste Storage;
- Section 4: Waste Collection;
- Section 5: Waste Management; and
- Section 6: Conclusion.



2 Waste Generation

The following section shows the waste generation rates used and the estimated waste volumes to be generated at the Proposal.

2.1 Proposed Tenancies

The anticipated volume of refuse and recyclables is based on the floor area (m²) of the Group Rooms, Cot Rooms, Reception, Kitchen, Meeting Room and Staff Room at the Childcare Centre – 409m².

2.2 Waste Generation Rates

In order to achieve an accurate projection of waste volumes for the Proposal, consideration was given to the City of Melbourne's *Guidelines for Preparing a Waste Management Plan* (2021), as they contain contemporary estimates of waste generated from Childcare Centres.

Table 2-1 shows the waste generation rates which have been applied to the Proposal.

Table 2-1: Waste Generation Rates

Tenancy Use Type	City of Melbourne Guideline Reference	Refuse Generation Rate	Recycling Generation Rate
Childcare Centre	Childcare	350L/100m ² /week	350L/100m ² /week

2.3 Waste Generation Volumes

Waste generation is estimated by volume in litres (L) as this is generally the influencing factor when considering bin size, numbers and storage space required.

Waste generation volumes in litres per week (L/week) adopted for this waste assessment is shown in Table 2-2. It is estimated that the Childcare Centre will generate 1,432L of refuse and 1,432L of recyclables each week.

Table 2-2: Estimated Waste Generation

Childcare Centre	Area (m²)	Waste Generation Rate (L/100m²/week)	Waste Generation (L/week)
Refuse	409	350	1,432
Recycling	409	350	1,432
		Total	2,864



3 Waste Storage

Waste materials generated within the Proposal will be collected in the bins located in the Bin Storage Area, as shown in Figure 2, and discussed in the following sub-sections.

3.1 Internal Transfer of Waste

To promote positive recycling behaviour and maximise diversion from landfill, internal bins will be available throughout the Proposal for the source separation of refuse and recycling.

These internal bins will be collected by the staff/cleaners at least once each day and transferred to the Bin Storage Area for consolidation into the appropriate bins. These bins will be transferred through the Proposal utilising the dedicated service walkways/corridors. This internal servicing method may be conducted outside of main operational hours to mitigate disturbances to staff/visitors.

All bins will be colour coded and labelled in accordance with Australian Standards (AS 4123.7) to assist staff and cleaners to dispose of their separate waste materials in the correct bins.

3.2 Bin Sizes

Table 3-1 gives the typical dimensions of standard bins sizes that may be utilised at the Proposal. It should be noted that these bin dimensions are approximate and can vary slightly between suppliers.

Table 3-1: Typical Bin Dimensions

Dimensions	Bin Sizes		
	240L	660L	1,100L
Depth (mm)	730	780	1,070
Width (mm)	585	1,260	1,240
Height (mm)	1,060	1,200	1,300
Area (mm²)	427	983	1,327

Reference: SULO Bin Specification Data Sheets

3.3 Bin Storage Area Size

To ensure sufficient area is available for storage of the bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in Table 2-2, bin sizes in Table 3-1 and based on collection of refuse and recyclables twice each week.

Based on the results shown in Table 3-2 the Bin Storage Area has been sized to accommodate:

- Three 240L refuse bins; and
- Three 240L recycling bins.

Table 3-2: Bin Requirements for Bin Storage Area

Waste Stream	Waste Generation	Number of Bins Required		
	(L/week)	240L	660L	1,100L
Refuse	1,432	3	2	1
Recycling	1,432	3	2	1



The configuration of these bins within the Bin Storage Area is shown in Figure 2. It is worth noting that the number of bins and corresponding placement of bins shown in Figure 2 represents the maximum requirements assuming two collections each week of refuse and recyclables.

3.4 Bin Storage Area Design

The design of the Bin Storage Area will take into consideration:

- Smooth impervious floor sloped to a drain connected to the sewer system;
- Taps for washing of bins and Bin Storage Area;
- Adequate aisle width for easy manoeuvring of bins;
- No double stacking of bins;
- Doors to the Bin Storage Area self-closing and vermin proof;
- Doors to the Bin Storage Area wide enough to fit bins through;
- Ventilated to a suitable standard;
- Appropriate signage;
- Undercover where possible and be designed to not permit stormwater to enter the drain;
- Located behind the building setback line;
- Bins not to be visible from the property boundary or areas trafficable by the public; and
- Bins are reasonably secured from theft and vandalism.

Bin numbers and storage space within the Bin Storage Area will be monitored by the building manager/caretaker during the operation of the Proposal to ensure that the number of bins and collection frequency is sufficient.



4 Waste Collection

A private waste collection contractor will service the Proposal and provide the Childcare Centre with three 240L bins for refuse and three 240L bins for recyclables.

The private contractor will collect refuse and recyclables twice each week utilising a rear loader waste collection vehicle directly from the Bin Storage Area from the kerbside utilising the laneway located off the corner of Haverhill Road and Clipstone Parkway.

The private contractor's rear loader waste collection vehicle will travel with left hand traffic flow on the laneway and pull up on the kerbside next to the Bin Storage Area for servicing, as marked in Diagram 1.

Private contractor's staff will ferry bins to and from the rear loader waste collection vehicle and the Bin Storage Area during servicing. The private contractor will be provided with key/PIN code access to the Bin Storage Area to facilitate servicing, if required.

It is proposed that servicing will be conducted outside of normal operating hours to mitigate impacts on local traffic movements during peak traffic hours.

Once servicing is complete the private contractor's rear loader waste collection vehicle will continue in a forward motion on the laneway, moving with traffic flow.



Diagram 1: Waste Collection Location



4.1 Bulk and Speciality Waste

Bulk and specialty waste materials will be removed from the Proposal as they are generated. Removal of these wastes will be monitored by the building manager/caretaker, who will liaise with staff and cleaners to assist with the removal of these wastes, as required.

Sanitary wastes will be collected in situ. A suitable qualified sanitary waste collection and disposal provider will be engaged to determine storage and collection requirements.



5 Waste Management

The building manager/caretaker will be engaged to complete the following tasks:

- Monitoring and maintenance of bins and the Bin Storage Area;
- Cleaning of bins and Bin Storage Area, when required;
- Ensure all staff/cleaners at the Proposal are made aware of this WMP and their responsibilities thereunder;
- Monitor staff/cleaners behaviour and identify requirements for further education and/or signage;
- Monitor bulk and speciality waste accumulation and assist staff/cleaners with its removal, as required;
- Regularly engage with staff/cleaners to develop opportunities to reduce waste volumes and increase resource recovery; and
- Regularly engage with the private contractors to ensure efficient and effective waste service is maintained.



6 Conclusion

As demonstrated within this WMP, the Proposal provides a sufficiently sized Bin Storage Area for storage of refuse and recyclables, based on the estimated waste generation volumes and suitable configuration of bins. This indicates that an adequately designed Bin Storage Area has been provided, and collection of refuse and recyclables can be completed from the Proposal.

The above is achieved using:

- Three 240L refuse bins, collected two times each week; and
- Three 240L recycling bins, collected two times each week.

A private contractor will service the bins directly from the Bin Storage Area from the kerbside utilising the laneway located off the corner of Haverhill Road and Clipstone Parkway.

The building manager/caretaker will oversee the relevant aspects of waste management at the Proposal.



Figures

Figure 1: Locality Plan

Figure 2: Bin Storage Area



LANEWAY EXMOUTH DRIVE

Bin Storage Area



Legend:

Bin Storage Area

3 x 240L refuse (730mm x 585mm)

3 x 240L recycling (730mm x 585mm)



ASSET MANAGEMENT
CIVIL ENGINEERING
ENVIRONMENTAL SERVICES
SPATIAL INTELLIGENCE
WASTE MANAGEMENT
NOISE MANAGEMENT

Ladybug Eleven Pty Ltd

I	OTES	
1.	This drawing is the property of Talis Consultants Pty Ltd. It is a confidential document and must not be copied,	
	used, or its contents divulged without prior written consent.	E

Ltd. ded, ten B 18/07/22 \$ SECOND ISSUE DP LC

A 18/07/22 \$ FIRST ISSUE RH

Lot 2812 (121) Exmouth Drive, Butler

	Drawn by:	DP	Job No: TV	V22067
	Checked by:	AB	File No: TW22	2067DW
in Storage Area	Approved by:	DP	Fig. No:	Rev:
	Scale: NTS		002	В



Assets | Engineering | Environment | Noise | Spatial | Waste

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Attachment 13 - Environmental Acoustic Assessment



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Environmental Noise Assessment

Lot 2812 (#121) Exmouth Drive, Butler Proposed Childcare Centre

Reference: 22057268-01B

Prepared for:

Ladybug Eleven Pty Ltd c/- Apex Planning



Report: 22057268-01B

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Date:	Rev	Description Prepared By		Verified
23-Aug-22	-	Issued to Client	Matt Nolan	Matt Moyle
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Appendices

A Development Plans

B Terminology

1 INTRODUCTION

It is proposed to construct a childcare centre (CCC) at Lot 2812 (#121) Exmouth Drive, Butler (refer *Figure 1-1*). The proposed centre, shown in site plan *Figure 1-2*, will consist of the following:

- Six internal teaching spaces capable of accommodating up to 92 children, grouped as follows:
 - o Group 1: 8 places for children aged 0-2 years,
 - o Group 2: 4 places for children aged 0-2 years, 10 places for children aged 2-3 years,
 - o Group 3: 15 places for children aged 2-3 years,
 - o Group 4: 10 places for children aged 2-3 years, 5 places for children aged 3-4+ years,
 - o Group 5: 20 places for children aged 3-4+ years,
 - o Group 6: 20 places for children aged 3-4+ years,
- Outdoor play areas located on the north side of the building.
- Amenities and associated mechanical plant such as:
 - o Kitchen with rangehood and exhaust fan assumed to be located on the roof above,
 - o Various exhaust fans (toilets, laundry) assumed to be located on the roof above, and
 - o AC plant assumed to be located on the roof above.
- Car parking used for the CCC on the south side of the lot.

It is noted that noise sensitive premises are in the vicinity of the subject site, with nearby residential properties located to the north and east of the CCC.

This report presents the assessment of the noise emissions from child play, car doors closing in the car park and mechanical plant associated with the childcare centre against the prescribed standards of the *Environmental Protection (Noise) Regulations 1997* (the Regulations) based on the development drawings shown in *Appendix A*.

The proposed hours of operation are 6.30am to 6.30pm Monday to Friday. Therefore, staff and parents can arrive and park before 7.00am, which is during the night-time period of the Regulations. It is assumed outdoor child play would not occur until after 7.00am.

An assessment of noise intrusion resulting from the 13.8 km extension of the passenger railway from Butler Station to Yanchep — refer *Figure 1-1* to the childcare centre has also been undertaken in accordance with *State Planning Policy 5.4 Road and Rail Noise.* Lloyd George Acoustics provided a transportation noise assessment to PTA for this future rail extension. Details from this have been incorporated into the noise assessment.

Appendix B contains a description of some of the terminology used throughout this report.

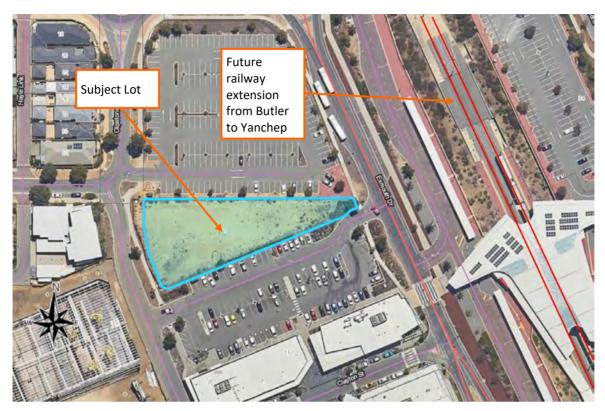


Figure 1-1 Project Locality (PlanWA)



Figure 1-2 Project Site Plan

2 CRITERIA

2.1 Environmental Noise

Environmental noise in Western Australia is governed by the *Environmental Protection Act 1986*, through the *Environmental Protection (Noise) Regulations 1997* (the Regulations).

Regulation 7 defines the prescribed standard for noise emissions as follows:

- "7. (1) Noise emitted from any premises or public place when received at other premises
 - (a) Must not cause or significantly contribute to, a level of noise which exceeds the assigned level in respect of noise received at premises of that kind; and
 - (b) Must be free of
 - i. tonality;
 - ii. impulsiveness; and
 - iii. modulation,

when assessed under regulation 9"

A "...noise emission is taken to significantly contribute to a level of noise if the noise emission ... exceeds a value which is 5 dB below the assigned level..."

Tonality, impulsiveness and modulation are defined in Regulation 9. Noise is to be taken to be free of these characteristics if:

- (a) The characteristics cannot be reasonably and practicably removed by techniques other than attenuating the overall level of noise emission; and
- (b) The noise emission complies with the standard prescribed under regulation 7 after the adjustments of *Table 2-1* are made to the noise emission as measured at the point of reception.

Table 2-1 Adjustments Where Characteristics Cannot Be Removed

Where	Noise Emission is Not	Where Noise Er	nission is Music	
Tonality	Modulation	Impulsiveness	No Impulsiveness	Impulsiveness
+ 5 dB	+ 5 dB	+ 10 dB	+ 10 dB	+ 15 dB

Note: The above are cumulative to a maximum of 15dB.

The baseline assigned levels (prescribed standards) are specified in Regulation 8 and are shown in *Table 2-2*.

Table 2-2 Baseline Assigned Noise Levels

Premises Receiving		Assigned Level (dB)		
Noise	Time Of Day	L _{A10}	L _{A1}	L _{Amax}
	0700 to 1900 hours Monday to Saturday (Day)	45 + influencing factor	55 + influencing factor	65 + influencing factor
Noise sensitive	0900 to 1900 hours Sunday and public holidays (Sunday)	40 + influencing factor	50 + influencing factor	65 + influencing factor
premises: highly sensitive area ¹	1900 to 2200 hours all days (Evening)	40 + influencing factor	50 + influencing factor	55 + influencing factor
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays (Night)	35 + influencing factor	45 + influencing factor	55 + influencing factor
Noise sensitive premises: any area other than highly sensitive area	All hours	60	75	80

^{1.} *highly sensitive area* means that area (if any) of noise sensitive premises comprising —

As the surrounding area has a mix of commercial and residential premises, the total influencing factor applicable at surrounding noise sensitive premises has been calculated as 1 dB. As Butler Boulevard (considered a secondary road) is over 100m from the nearby residents, a transport factor of 0 dB has been used within the assessment.

The combined traffic and land use influencing factors for all surrounding residences, as shown in *Figure 1-1*, has been calculated as shown in *Table 2-3*. It was determined that all residences nearest to the development have the same influencing factor when rounding to the nearest whole number.

Table 2-3 Influencing Factor Calculation – Nearest Residences

Description	Within 100 metre Radius	Within 450 metre Radius	Total
Industrial Land	0 %	0 %	0 dB
Commercial Land	12-18% / 0.6- 0.9 dB	9% / 0.5 dB	1.1-1.4 dB
	0 dB		
	1 dB		

Table 2-4 shows the assigned noise levels including the influencing factor and transport factor at the receiving locations.

⁽a) a building, or a part of a building, on the premises that is used for a noise sensitive purpose; and

⁽b) any other part of the premises within 15 metres of that building or that part of the building.

Table	2-4	Assia	ned	Noise	Level	S
						_

Premises Receiving		Assigned Level (dB)		
Noise	Time Of Day	L _{A10}	L _{A1}	L _{Amax}
	0700 to 1900 hours Monday to Saturday (Day)	46	56	66
	0900 to 1900 hours Sunday and public holidays (Sunday)	41	51	66
All nearest highly sensitive areas ¹	1900 to 2200 hours all days (Evening)	41	51	56
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays (Night)	36	46	56
Noise sensitive premises: any area other than highly sensitive area	All hours	60	75	80

^{1.} highly sensitive area means that area (if any) of noise sensitive premises comprising —

It is noted the assigned noise levels are statistical levels and therefore the period over which they are determined is important. The Regulations define the Representative Assessment Period (RAP) as a period of time of not less than 15 minutes, and not exceeding 4 hours, which is determined by an inspector or authorised person to be appropriate for the assessment of a noise emission, having regard to the type and nature of the noise emission. An inspector or authorised person is a person appointed under Sections 87 & 88 of the Environmental Protection Act 1986 and include Local Government Environmental Health Officers and Officers from the Department of Environment Regulation. Acoustic consultants or other environmental consultants are not appointed as an inspector or authorised person. Therefore, whilst this assessment is based on a 4 hour RAP, which is assumed to be appropriate given the nature of the operations, this is to be used for guidance only.

Regulation 14A provides requirements for the collection of waste stating that this activity can also be exempt from having to comply with regulation 7 prescribed standards provided it is undertaken between 7am and 7pm Mondays to Saturdays and undertaken in the quietest reasonable manner.

2.2 Transportation Noise

The criteria relevant to this assessment is provided in *State Planning Policy No. 5.4 Road and Rail Noise* (hereafter referred to as SPP 5.4) produced by the Western Australian Planning Commission (WAPC). The objectives of SPP 5.4 are to:

- Protect the community from unreasonable levels of transport noise;
- Protect strategic and other significant freight transport corridors from incompatible urban encroachment;
- Ensure transport infrastructure and land-use can mutually exist within urban corridors;
- Ensure that noise impacts are addressed as early as possible in the planning process; and

⁽a) a building, or a part of a building, on the premises that is used for a noise sensitive purpose; and

⁽b) any other part of the premises within 15 metres of that building or that part of the building.

• Encourage best practice noise mitigation design and construction standards

Table 2-5 sets out noise targets that are to be achieved by proposals under which SPP 5.4 applies. Where the targets are exceeded, an assessment is required to determine the likely level of transport noise and management/mitigation required.

Table 2-5 Noise Targets for Noise-Sensitive Land-Use

Outdoor Noise Target		Indoor Noise Target	
55 dB L _{Aeq(Day)}	50 dB L _{Aeq(Night)}	40 dB L _{Aeq(Day)} (Living and Work Areas)	35 dB L _{Aeq(Night)} (Bedrooms)

Notes:

- Day period is from 6am to 10pm and night period from 10pm to 6am.
- The outdoor noise target is to be measured at 1-metre from the most exposed, habitable facade of the noise sensitive building.
- For all noise-sensitive land-use and/or development, indoor noise targets for other room usages may be reasonable drawn from Table 1 of Australian Standard/New Zealand Standard AS/NZS 2107:2016 Acoustics Recommended design sound levels and reverberation times for building interiors (as amended) for each relevant time period.
- Outdoor targets are to be met at all outdoor areas as far as is reasonable and practicable to do so using the various noise mitigation measures outlined in the Guidelines.

The application of SPP 5.4 is to consider anticipated traffic volumes for the next 20 years from when the noise assessment is undertaken.

In the application of the noise targets, the objective is to achieve:

- indoor noise levels specified in *Table 2-5* in noise-sensitive areas (e.g. bedrooms and living rooms of houses and school classrooms); and
- a reasonable degree of acoustic amenity for outdoor living areas on each residential lot. For non-residential noise-sensitive developments, for example schools and childcare centres, the design of outdoor areas should take into consideration the noise target.

Reference: 22057268-01B Page 6

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¹ A habitable room is defined in State Planning Policy 3.1 as a room used for normal domestic activities that includes a bedroom, living room, lounge room, music room, sitting room, television room, kitchen, dining room, sewing room, study, playroom, sunroom, gymnasium, fully enclosed swimming pool or patio.

3 METHODOLOGY

3.1 Environmental Noise

Computer modelling has been used to predict the noise emissions from the development at all nearby receivers. The software used was *SoundPLAN 8.2* with the ISO 9613 algorithms (ISO 171534-3 improved method) selected, as they include the influence of wind and are considered appropriate given the relatively short source to receiver distances.

Input data required in the model are:

- Meteorological Information;
- Topographical data;
- Ground Absorption; and
- Source sound power levels.

3.1.1 Meteorological Information

Meteorological information utilised is provided in *Table 3-1* and is considered to represent worst-case conditions for noise propagation. At wind speeds greater than those shown, sound propagation may be further enhanced, however background noise from the wind itself and from local vegetation is likely to be elevated and dominate the ambient noise levels.

Table 3-1 Modelling Meteorological Conditions

Parameter	Day (0700-1900)	Night (1900-0700)		
Temperature (°C)	20	15		
Humidity (%)	50	50		
Wind Speed (m/s)	Up to 5	Up to 5		
Wind Direction*	All	All		

^{*} Note that the modelling package used allows for all wind directions to be modelled simultaneously.

It is generally considered that compliance with the assigned noise levels needs to be demonstrated for 98% of the time, during the day and night periods, for the month of the year in which the worst-case weather conditions prevail. In most cases, the above conditions occur for more than 2% of the time and therefore must be satisfied.

3.1.2 Topographical Data

Topographical information was based on data publicly available (e.g. *Google*) in the form of spot heights and combined with finished floor levels where provided on the development drawings. Lloyd George Acoustics also provided a transportation noise assessment for PTA and topographical data from this model have been incorporated in the design model.

3.1.3 Buildings and Receivers

Surrounding existing buildings were included in the noise model, as these can provide noise shielding as well as reflection paths.

Nearby existing houses to the north are single storey based on *Streetview* images and were modelled as 3.5 metres high buildings, with receivers located 1.4 metres above ground level. The residential buildings to the east of the CCC are double storey and were modelled at 6m, with receivers located at 1.4m and 4.2m above ground level. The childcare centre building incorporates a car park and play area as shown in the design drawings of *Appendix A* and this was reproduced within the noise model.

Figure 3-1 shows a 2D overview of the noise model with the location of all relevant receivers identified.

3.1.4 Walls and Fences

A solid timber fence design spaced with visually permeable vertical fins is proposed alongside the play area, with a height ranging from 1.8m to 2.45m.

Figure 3-2 shows a view of the 3D model based on the information above in relation to topography and building and fence heights. Also shown are the outdoor play areas (pink polygon) and point sources (e.g. mechanical plant, car doors) as pink dots.

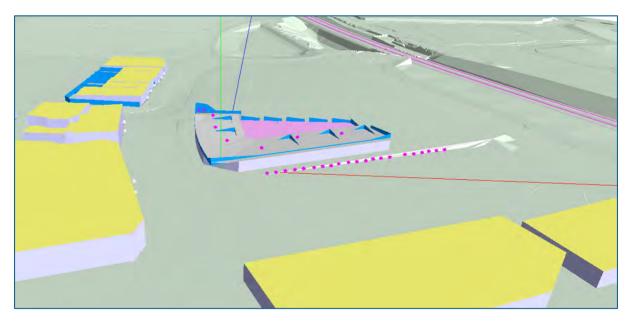


Figure 3-1 South Elevation View of 3D Noise Model

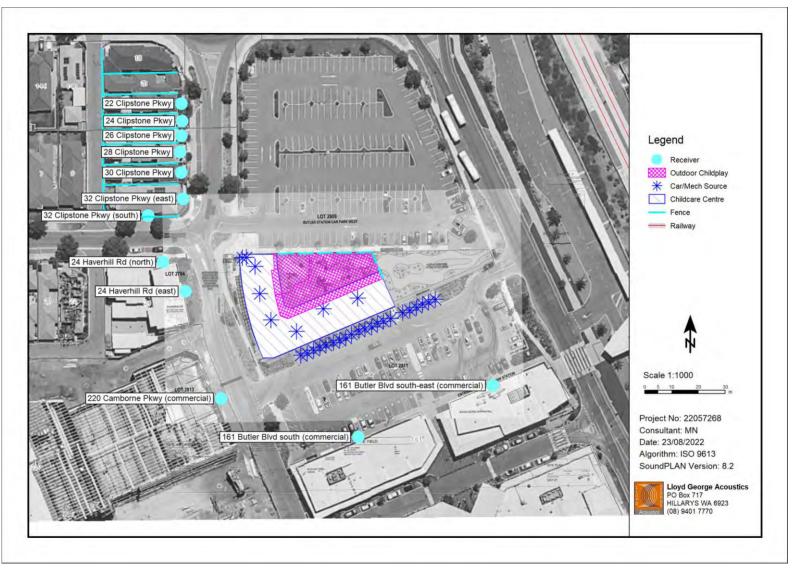


Figure 3-2 2D Overview of Noise Model

3.1.5 Source Sound Levels

The sound power levels used in the modelling are provided in *Table 3-2*.

Table 3-2 Source Sound Power Levels, dB

Description	Octave Band Centre Frequency (Hz)						Overall		
	63	125	250	500	1k	2k	4k	8k	dB(A)
Babies Play Aged 0-2 Years (10 kids), L ₁₀	48	54	60	66	72	74	71	67	78
Toddler Play Aged 2-3 Years (10 kids), L ₁₀	61	67	73	79	81	78	74	70	85
Kindy Play Aged 3+ Years (10 kids), L ₁₀	64	70	75	81	83	80	76	72	87
AC plant, double fan unit (4 off), each, L ₁₀	-	77	77	76	73	68	67	62	78
Toilet/Laundry Exhausts, each, L ₁₀	49	54	51	52	49	50	45	42	56
Kitchen Exhaust, L ₁₀	50	64	61	70	69	66	62	50	73
Closing Car Door, L _{max}	71	74	77	81	80	78	72	61	84

The following is noted in relation to the source levels above:

- Child play source levels are based on Guideline 3.0 provided by the Association of Australasian Acoustical Consultants (AAAC) published September 2020. Where the number of children for individual play areas is specified in the plans, these have been adjusted from the reference source levels using appropriate acoustical calculations. Outdoor child play was modelled as area sources at 1-metre heights above ground level. The sound power levels used in the model were scaled as follows:
 - 12 children aged 0-2 years = 80 dB(A)
 - 35 Toddlers aged 2-3 years = 89 dB(A)
 - \circ 45 Kindy aged 3+ years = 93 dB(A)
- Based on the AAAC Guideline 3.0, source sound power levels for AC condensing units were assumed. Medium sized (double fan) outdoor units were deemed appropriate. Each was modelled as a point source located on the ground in the nominated yard as indicated on plans. It is noted that screening will be provided and this has been replicated in the noise model.
- Other mechanical plant includes five exhaust fans (toilets and laundry) and one kitchen exhaust fan/rangehood fan. All were modelled as point sources approximately 0.5 metres above roof level and above the area serviced.
- Car doors closing were modelled as a point source 1.0 metre above ground level. Since noise from a car door closing is a short term event, only the L_{Amax} level is applicable.

3.1.6 Ground Absorption

Ground absorption varies from a value of 0 to 1, with 0 being for an acoustically reflective ground (e.g. asphalt, concrete) and 1 for acoustically absorbent ground (e.g. grass/sand). In this instance, a value of 1 has been used for the outdoor play area and parks, and 0.1 for all other areas.

3.2 Transportation Noise

To address the noise impact from rail noise, noise modelling has been undertaken generally in accordance with the requirements of SPP 5.4 and associated Guidelines². Lloyd George Acoustics provided a study for the PTA Butler to Yanchep rail extension and this was adapted to the CCC site.

3.3 Noise Modelling

The computer programme *SoundPLAN 8.2* was utilised incorporating the Nordic Rail Prediction Method (Kilde Rep. 130) algorithm. The algorithm has been modified to reflect local conditions as follows:

• The Nordic Rail Prediction Method (Kilde Rep. 130) algorithm is for generic train types in Europe and requires modification to align with measured noise levels of passenger trains operating in the Perth region. Measured noise levels used are shown in *Table 3-1*.

Description	dB(A) at One-Third Octave Frequencies (Hz)							Overall		
Description	31.5	63	125	250	500	1K	2K	4K	8K	dB(A)
Train speed of	30	51	59	62	73	79	79	77	69	
130 km/hr at a	35	54	61	65	73	79	80	74	64	87
distance of 15m	42	53	61	69	78	80	78	72	58	

Table 3-3 Sound Pressure Levels Used in the Noise Model

3.3.1 Ground Topography, Rail Design & Cadastral Data

Topographical data was based on that provided by PTA with the contours being in 0.01 metre intervals. The railway design and groundworks have been incorporated into the existing ground levels to develop a 3-dimensional model. It should be noted that the railway has been designed to be in a cutting, between 4-7m below ground level, for the majority of the alignment and that the modelling has taken this into account.

3.3.2 Train Data

The train configuration and numbers of movements used in the noise prediction modelling are presented below in *Tables 3-2 and 3-3*.

Description of Variable	Value
Type of noise source	Line source
6 Car Set	150 metres
Height of noise source above railhead	0.8 metres

Table 3-4 Variables Used in the Noise Prediction Model

² Road and Rail Noise Guidelines, September 2019, Western Australia Planning Commission

Turin Description	Train Movements			
Train Description	Day	Night		
6 Car Sets	Northbound			
	75	22		
6 Car Sets	Southboo	und		
	75	22		

Table 3-5 Daily Rail Movements Assumed in the Noise Model

3.3.3 Train Speeds

The calculated train speeds, based on simulation plots determined by Australis Rail Consulting, are shown in *Figure 3-3*. The black dotted line shows the maximum speed limit (km/h), the red line shows the actual calculated train speed attained (km/h), and the brown line shows the vertical track profile (m). Note that the train does not always reach the maximum speed limit due the requirement to stop at stations and the effect of track gradients and alignment.

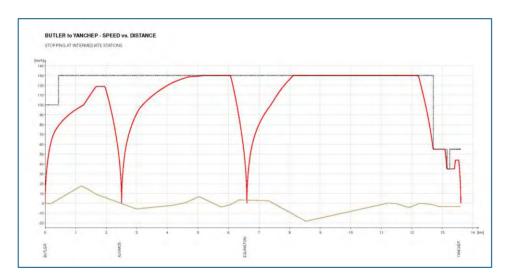


Figure 3-3 Train Speeds vs Distance Butler to Yanchep

4 RESULTS

4.1 Outdoor Child Play

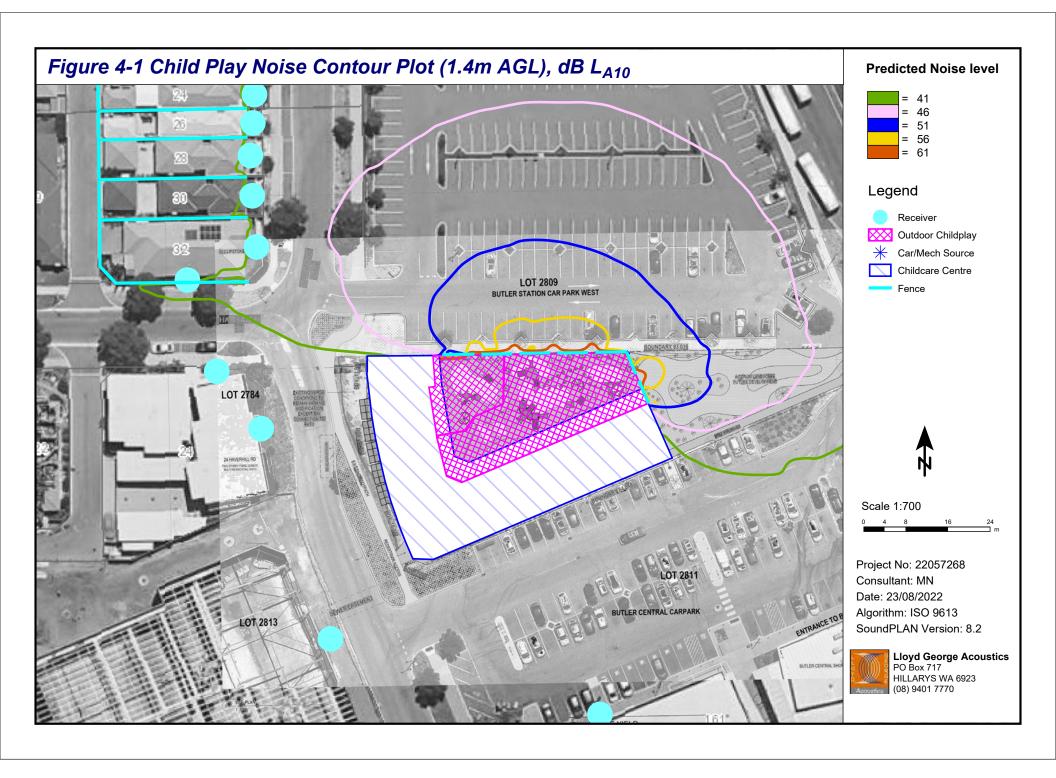
The childcare development will host up to 92 children. It is noted play time is generally staggered and therefore not all children would be playing outside at once for extended periods of time. However, noise levels were conservatively predicted for all children playing, as a worst-case scenario.

Table 4-1 presents the predicted noise levels at each receiver, noting the predicted noise levels are from child play only i.e. mechanical plant noise is not included. *Figure 4-1* also shows the predicted noise levels as noise contour maps at ground level (1.4 metres AGL).

Table 4-1 Predicted Noise Levels of Child Play, dB LA10

Receiver	16 Children (Aged 0-2)	20 Toddlers (2-3)	50 Kindy (3+)	Total Combined
22 Clipstone Pkwy - GF	21	38	35	40
24 Clipstone Pkwy - GF	21	39	35	40
26 Clipstone Pkwy - GF	21	39	36	40
28 Clipstone Pkwy - GF	22	39	36	41
30 Clipstone Pkwy - GF	22	39	36	41
32 Clipstone Pkwy - GF (east)	22	39	36	41
32 Clipstone Pkwy - GF (south)	13	29	26	31
24 Haverhill Rd - GF (east)	18	32	28	33
24 Haverhill Rd - 1F (east)	20	37	34	39
24 Haverhill Rd - GF (north)	13	25	22	27
24 Haverhill Rd - 1F (north)	14	28	25	30
161 Butler Blvd - GF (south-east)	16	31	28	33
161 Butler Blvd - GF (south)	17	28	25	30
220 Camborne Pkwy - GF	17	32	29	34
220 Camborne Pkwy - 1F	21	36	33	38

The highest predicted levels are to the residences to the north. While these levels indicate the conservative potential outcome, in reality noise levels would be lower on average, with reduced child numbers (staggered play times) and with periods of passive play.



4.2 Mechanical Plant

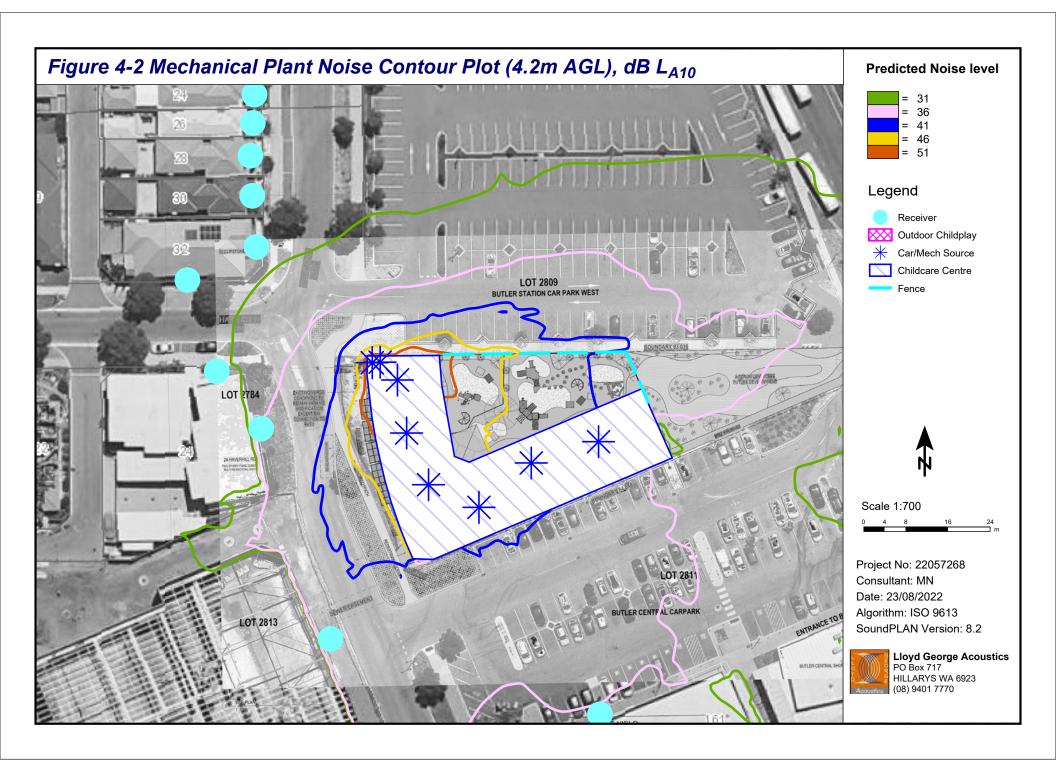
Mechanical plant consists of AC plant and extraction fans for the kitchen, toilets and laundry. The exhaust fans were assumed to be located on the roof and above the room being serviced. The AC plant was modelled as per the designated rooftop areas on the west side of the building.

The predicted mechanical plant noise levels are presented in *Table 4-2*. *Figure 4-2* shows the predicted noise levels as noise contour maps at the first floor level (4.2 metres AGL) as the noise levels are higher at this level.

Table 4-2 Predicted Noise Levels of Mechanical Plant, dB LA10

Receiver	All Plant Combined
22 Clipstone Pkwy - GF	21
24 Clipstone Pkwy - GF	21
26 Clipstone Pkwy - GF	22
28 Clipstone Pkwy - GF	23
30 Clipstone Pkwy - GF	25
32 Clipstone Pkwy - GF (east)	27
32 Clipstone Pkwy - GF (south)	19
24 Haverhill Rd - GF (east)	31
24 Haverhill Rd - 1F (east)	33
24 Haverhill Rd - GF (north)	20
24 Haverhill Rd - 1F (north)	23
161 Butler Blvd - GF (south-east)	28
161 Butler Blvd - GF (south)	31
220 Camborne Pkwy - GF	34
220 Camborne Pkwy - 1F	37

It can be seen that at most of the receivers, the predicted mechanical plant noise is lower than the child play noise levels (*Table 4-1*). Therefore, child play noise would dominate the noise levels during the day at most receivers, except prior to 7.00am, when child play noise is not present. The above results are based on assumed plant source levels and should therefore be recalculated once mechanical plant specifications are known closer to building permit application.

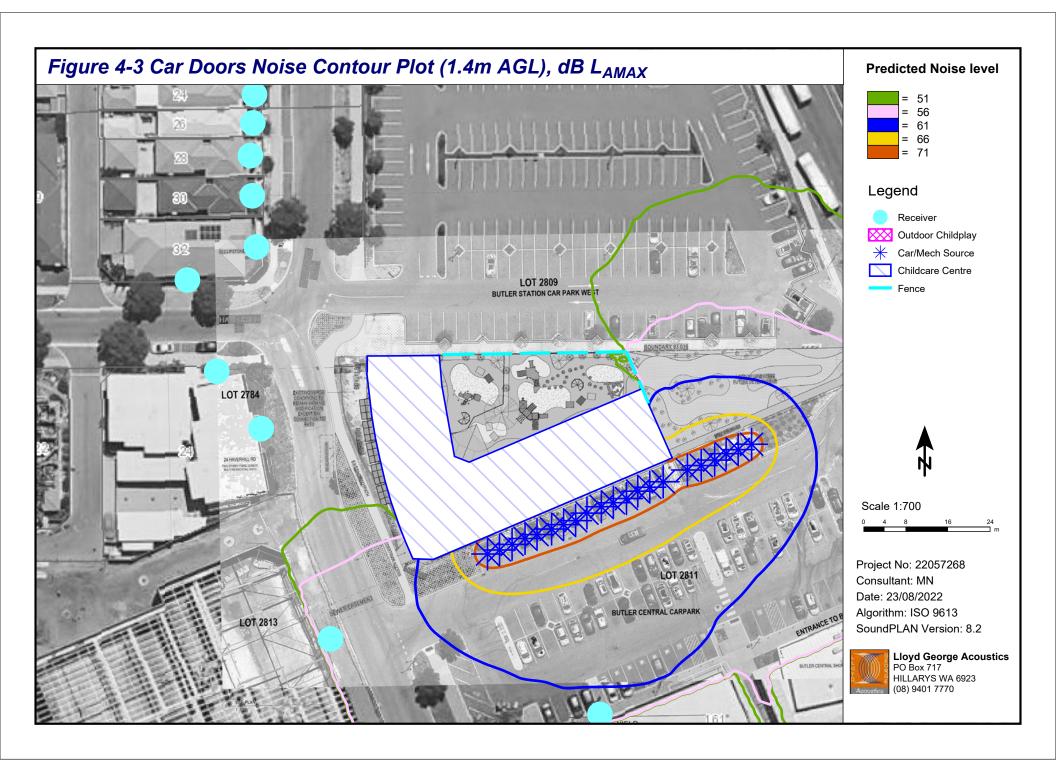


4.3 Car Park

The model includes noise from car doors closing in all parking bays and *Table 4-3* presents the highest predicted noise levels applicable to each receiver. *Figure 4-3* also presents the maximum noise levels at the ground floor (1.4 m AGL) for car doors as a contour map. Note that this contour is not a cumulative level, but a composite contour of each maximum noise event.

Table 4-3 Predicted Car Doors Closing Noise Levels, dB LAmax

Receiver	Car Doors
22 Clipstone Pkwy - GF	34
24 Clipstone Pkwy - GF	33
26 Clipstone Pkwy - GF	32
28 Clipstone Pkwy - GF	34
30 Clipstone Pkwy - GF	30
32 Clipstone Pkwy - GF (east)	28
32 Clipstone Pkwy - GF (south)	25
24 Haverhill Rd - GF (east)	41
24 Haverhill Rd - 1F (east)	40
24 Haverhill Rd - GF (north)	27
24 Haverhill Rd - 1F (north)	28
161 Butler Blvd - GF (south-east)	54
161 Butler Blvd - GF (south)	56
220 Camborne Pkwy - GF	54
220 Camborne Pkwy - 1F	53



4.4 Transportation Noise Modelling

Details of a railway noise assessment between Butler Station to Yanchep Station previously completed for PTA has been incorporated in the CCC transportation noise modelling.

For the childcare centre, it is the daytime noise levels that are critical. The noise model is considered for future daytime conditions which includes movement from the future rail extension.

Results for the future external noise level, predicted at each sensitive façade and the outdoor play space is presented in *Table 4-4*.

Table 4-4 Predicted External Noise Levels to CCC

Room	L _{Aeq(Day)} , dB
Cot Room (North)	49
Cot Room (South)	48
Group 1	48
Group 2	49
Group 3	47
Group 4	47
Group 5	47
Group 6	48
Outdoor play space	48

Based on the above outcomes, the railway noise intrusion at the CCC is below the outdoor noise target of SPP 5.4.

5 ASSESSMENT

5.1 Outdoor Child Play

Although the childcare centre opens from 6.30am, outdoor child play will only occur after 7.00am, when the assigned noise levels increase by 10 dB compared to prior to 7.00am. Noise from child play is not considered to contain annoying characteristics within the definition of the Regulations and therefore, no adjustments are made to the predicted noise levels.

Table 5-1 presents the assessment of the highest predicted noise levels from all 92 children playing outside against the L_{A10} assigned noise level at each receiver.

Table 5-1 Assessment of Outdoor Child Play Noise Levels, dB LA10

Receiver	Assigned Noise Level	Predicted Level	Exceedance
22 Clipstone Pkwy - GF	46	40	Complies
24 Clipstone Pkwy - GF	46	40	Complies
26 Clipstone Pkwy - GF	46	40	Complies
28 Clipstone Pkwy - GF	46	41	Complies
30 Clipstone Pkwy - GF	46	41	Complies
32 Clipstone Pkwy - GF (east)	46	41	Complies
32 Clipstone Pkwy - GF (south)	46	31	Complies
24 Haverhill Rd - GF (east)	46	33	Complies
24 Haverhill Rd - 1F (east)	46	39	Complies
24 Haverhill Rd - GF (north)	46	27	Complies
24 Haverhill Rd - 1F (north)	46	30	Complies
161 Butler Blvd - GF (south-east)	60	33	Complies
161 Butler Blvd - GF (south)	60	30	Complies
220 Camborne Pkwy - GF	60	34	Complies
220 Camborne Pkwy - 1F	60	38	Complies

Based on a conservative scenario of all 92 children playing simultaneously, the assessment demonstrates compliance at all receivers.

5.2 Mechanical Plant

Given the proposed opening hours of the childcare centre, the night-time period (i.e. before 7.00am) is most critical. The overall noise levels are generally dominated by the A/C condenser noise, which may be considered tonal, and a +5 dB adjustment (refer *Table 5-2*) applies to predictions.

Table 5-2 Assessment of Mechanical Plant Noise Levels, dB LA10

Receiver	Night Assigned Noise Level	Predicted Level	Adjusted Level	Exceedance
22 Clipstone Pkwy - GF	36	21	26	Complies
24 Clipstone Pkwy - GF	36	21	26	Complies
26 Clipstone Pkwy - GF	36	22	27	Complies
28 Clipstone Pkwy - GF	36	23	28	Complies
30 Clipstone Pkwy - GF	36	25	30	Complies
32 Clipstone Pkwy - GF (east)	36	27	32	Complies
32 Clipstone Pkwy - GF (south)	36	19	24	Complies
24 Haverhill Rd - GF (east)	36	31	36	Complies
24 Haverhill Rd - 1F (east)	36	33	38	+2
24 Haverhill Rd - GF (north)	36	20	25	Complies
24 Haverhill Rd - 1F (north)	36	23	28	Complies
161 Butler Blvd - GF (south-east)	60	28	33	Complies
161 Butler Blvd - GF (south)	60	31	36	Complies
220 Camborne Pkwy - GF	60	34	39	Complies
220 Camborne Pkwy - 1F	60	37	42	Complies

The most critical receiver is located to the east of the CCC, with a predicted level of up to 38 dB L_{A10} on the first floor of 24 Haverhill Rd. This exceeds the criteria by to 2 dB. Compliance is demonstrated at all other receivers. Note that this assessment is based on assumptions in relation to the number, location, size and type of AC plant and exhaust fans. Therefore, mechanical plant noise is to be reviewed by a qualified acoustical consultant during detailed design, when plant selections and locations become known. It is recommended that plant be selected with a "quiet mode" capability programmed for night periods. Mechanical plant is deemed to be potentially compliant on this basis.

5.3 Car Doors

Car doors closing noise are short duration events and were therefore assessed against the L_{Amax} assigned noise level. Given the proposed hours of operation, staff and visitors may arrive before 7.00am when the night-time assigned noise level is applicable. Car door noise may be considered impulsive within the definition of the Regulations. Therefore, an adjustment of +10 dB (refer *Table 5-3*) is applied to the predicted noise levels.

Table 5-3 Assessment of Car Doors Closing Noise Levels, dB LAmax

Receiver	Night Assigned Noise Level	Predicted Level	Adjusted Level	Exceedance
22 Clipstone Pkwy - GF	56	34	44	Complies
24 Clipstone Pkwy - GF	56	33	43	Complies
26 Clipstone Pkwy - GF	56	32	42	Complies
28 Clipstone Pkwy - GF	56	34	44	Complies
30 Clipstone Pkwy - GF	56	30	40	Complies
32 Clipstone Pkwy - GF (east)	56	28	38	Complies
32 Clipstone Pkwy - GF (south)	56	25	35	Complies
24 Haverhill Rd - GF (east)	56	41	51	Complies
24 Haverhill Rd - 1F (east)	56	40	50	Complies
24 Haverhill Rd - GF (north)	56	27	37	Complies
24 Haverhill Rd - 1F (north)	56	28	38	Complies
161 Butler Blvd - GF (south-east)	80	54	64	Complies
161 Butler Blvd - GF (south)	80	56	66	Complies
220 Camborne Pkwy - GF	80	54	64	Complies
220 Camborne Pkwy - 1F	80	53	63	Complies

Noise from car doors is demonstrated to comply at all nearest sensitive locations at all time periods.

5.4 Indoor Child Play

An assessment of noise levels from indoor child play was carried out and the resulting noise levels at all locations were predicted to be well below that of outdoor child play considered in *Section 4.1*. This assessment was carried out based on the following considerations:

- Internal noise levels within activity rooms would not exceed those from outdoor play for each age group, regardless of whether windows/doors are open; and,
- Any music played within the internal activity areas would be 'light' music with no significant bass content and played at a relatively low level.

6 RECOMMENDATIONS

Noise from child play is demonstrated to comply during the day at all nearest residences. The timber panels surrounding the child play area must be constructed of a material with minimum surface mass of 8 kg/m².

To minimise noise impact from kitchen exhaust fans, it is recommended that inline type fans, which could be installed with attenuators or diverted ducting, be favoured over externally mounted plant. The noise from AC condensing units may be mitigated with local solid screening and quiet mode programming prior to 7.00am. This should be explored during detailed design and verified by the mechanical services engineer and a qualified acoustical consultant, when plant selections and locations become finalised.

Noise intrusion from the railway extension from Butler to Yanchep is demonstrated to be below the outdoor noise target of State Planning Policy 5.4. The outdoor play area is adequately screened by the surrounding fence and is also considered to meet the State Planning Policy 5.4.

Regulation 14A provides requirements for the collection of waste stating that this activity can also be exempt from having to comply with regulation 7 prescribed standards provided it is undertaken between 7am and 7pm Mondays to Saturdays and undertaken in the quietest reasonable manner.

Separate to the above, the following 'best practice' measures could be incorporated to further reduce acoustic impact (though not specifically required to achieve compliance):

- The behaviour and 'style of play' of children should be monitored to prevent particularly loud activity e.g. loud banging/crashing of objects, 'group' shouts/yelling,
- Favour soft finishes in the outdoor play area to minimise impact noise (e.g. soft grass, sand pit(s), rubber mats) over timber or plastic,
- Favour soft balls and rubber wheeled toys,
- Crying children should be taken inside to be comforted,
- No amplified music to be played outside,
- Any music played within the internal activity areas to be 'light' music with no significant bass content and played at a relatively low level.
- Car park drainage grates to be plastic or metal with rubber gasket and secured to avoid excess banging.

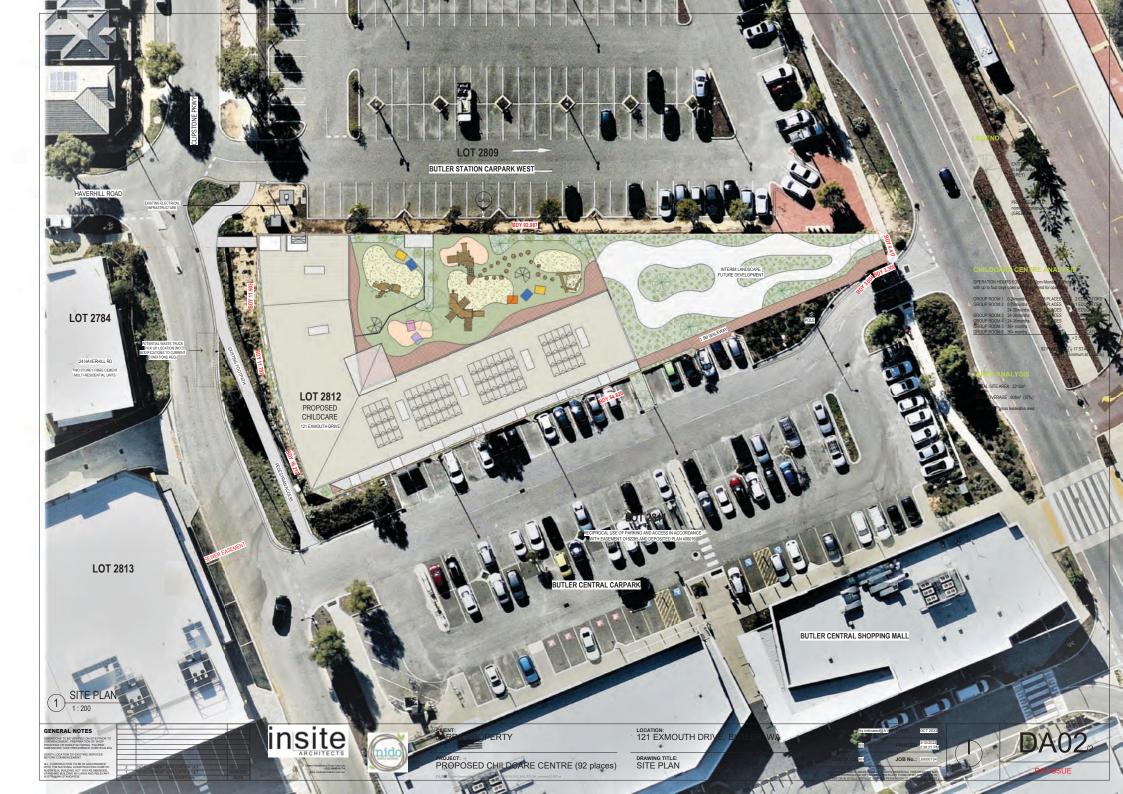
7 CONCLUSIONS

The noise impacts from the proposed childcare centre to be located at Lot 2812 (#121) Exmouth Drive, Butler have been assessed against the relevant criteria of the *Environmental Protection (Noise) Regulations 1997*. Based on the modelling and assessments in relation to the noise emissions from child play, mechanical plant and car doors closing, it is concluded that compliance can be achieved for all nearest noise sensitive premises.

Lloyd George Acoustics

Appendix A

Development Plans





GROUND FLOOR PLAN

1:100

GENERAL NOTES

DIRECTIONS TO BE VERWICE OF DIT FROM TO

COMMISSIONATE PROPERTION OF DISCOVERY

DIRECTIONATE PROPERTION OF DISCOVERY

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DIRECTION

ARCHITECTS
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(10) SHOR STAT.

CLIENT: JARRA PROPERTY

PROPOSED CHILDCARE CENTRE (92 places)

LOCATION: 121 EXMOUTH DRIVE, BUTLER, WA

DRAWING TITLE:
GROUND FLOOR PLAN

SCALE:	1:100@A1	DATE:	OCT 2022
DRAWN:	CW/SS	PRINTED:	3/10/2022 2:30:23 PN
CHECKED	: AH	JOB No.:	J0000734



DA03/2

D/A ISSUE



Lloyd George Acoustics

Appendix B

Terminology

The following is an explanation of the terminology used throughout this report.

Decibel (dB)

The decibel is the unit that describes the sound pressure and sound power levels of a noise source. It is a logarithmic scale referenced to the threshold of hearing.

A-Weighting

An A-weighted noise level has been filtered in such a way as to represent the way in which the human ear perceives sound. This weighting reflects the fact that the human ear is not as sensitive to lower frequencies as it is to higher frequencies. An A-weighted sound level is described as L_A dB.

Sound Power Level (Lw)

Under normal conditions, a given sound source will radiate the same amount of energy, irrespective of its surroundings, being the sound power level. This is similar to a 1kW electric heater always radiating 1kW of heat. The sound power level of a noise source cannot be directly measured using a sound level meter but is calculated based on measured sound pressure levels at known distances. Noise modelling incorporates source sound power levels as part of the input data.

Sound Pressure Level (Lp)

The sound pressure level of a noise source is dependent upon its surroundings, being influenced by distance, ground absorption, topography, meteorological conditions etc and is what the human ear actually hears. Using the electric heater analogy above, the heat will vary depending upon where the heater is located, just as the sound pressure level will vary depending on the surroundings. Noise modelling predicts the sound pressure level from the sound power levels taking into account ground absorption, barrier effects, distance etc.

LASIOW

This is the noise level in decibels, obtained using the A frequency weighting and the S (Slow) time weighting as specified in IEC 61672-1:2002. Unless assessing modulation, all measurements use the slow time weighting characteristic.

LAFast

This is the noise level in decibels, obtained using the A frequency weighting and the F (Fast) time weighting as specified in IEC 61672-1:2002. This is used when assessing the presence of modulation only.

LAPeak

This is the greatest absolute instantaneous sound pressure in decibels using the A frequency weighting as specified in IEC 61672-1:2002.

L_{Amax}

An L_{Amax} level is the maximum A-weighted noise level during a particular measurement.

L_{A1}

An L_{A1} level is the A-weighted noise level which is exceeded for one percent of the measurement period and is considered to represent the average of the maximum noise levels measured.

LA10

An L_{A10} level is the A-weighted noise level which is exceeded for 10 percent of the measurement period and is considered to represent the "intrusive" noise level.

LAeg

The equivalent steady state A-weighted sound level ("equal energy") in decibels which, in a specified time period, contains the same acoustic energy as the time-varying level during the same period. It is considered to represent the "average" noise level.

L_{A90}

An L_{A90} level is the A-weighted noise level which is exceeded for 90 percent of the measurement period and is considered to represent the "background" noise level.

One-Third-Octave Band

Means a band of frequencies spanning one-third of an octave and having a centre frequency between 25 Hz and 20 000 Hz inclusive.

L_{Amax} assigned level

Means an assigned level which, measured as a LA Slow value, is not to be exceeded at any time.

L_{A1} assigned level

Means an assigned level which, measured as a $L_{A \, Slow}$ value, is not to be exceeded for more than 1% of the representative assessment period.

L_{A10} assigned level

Means an assigned level which, measured as a L_{A Slow} value, is not to be exceeded for more than 10% of the representative assessment period.

Tonal Noise

A tonal noise source can be described as a source that has a distinctive noise emission in one or more frequencies. An example would be whining or droning. The quantitative definition of tonality is:

the presence in the noise emission of tonal characteristics where the difference between -

- (a) the A-weighted sound pressure level in any one-third octave band; and
- (b) the arithmetic average of the A-weighted sound pressure levels in the 2 adjacent one-third octave bands,

is greater than 3 dB when the sound pressure levels are determined as $L_{Aeq,T}$ levels where the time period T is greater than 10% of the representative assessment period, or greater than 8 dB at any time when the sound pressure levels are determined as $L_{A Slow}$ levels.

This is relatively common in most noise sources.

Modulating Noise

A modulating source is regular, cyclic and audible and is present for at least 10% of the measurement period. The quantitative definition of modulation is:

a variation in the emission of noise that —

- (a) is more than 3 dB L_{A Fast} or is more than 3 dB L_{A Fast} in any one-third octave band;
- (b) is present for at least 10% of the representative.

Impulsive Noise

An impulsive noise source has a short-term banging, clunking or explosive sound. The quantitative definition of impulsiveness is:

a variation in the emission of a noise where the difference between $L_{A peak}$ and $L_{A Max slow}$ is more than 15 dB when determined for a single representative event;

Major Road

Is a road with an estimated average daily traffic count of more than 15,000 vehicles.

Secondary / Minor Road

Is a road with an estimated average daily traffic count of between 6,000 and 15,000 vehicles.

Influencing Factor (IF)

$$= \frac{1}{10} \left(\% \operatorname{TypeA}_{100} + \% \operatorname{TypeA}_{450} \right) + \frac{1}{20} \left(\% \operatorname{TypeB}_{100} + \% \operatorname{TypeB}_{450} \right)$$
where:
$$\% \operatorname{TypeA}_{100} = \text{the percentageof industrialland within}$$

$$a 100 \text{m radius of the premises receiving the noise}$$

$$\% \operatorname{TypeA}_{450} = \text{the percentageof industrialland within}$$

$$a 450 \text{m radius of the premises receiving the noise}$$

$$\% \operatorname{TypeB}_{100} = \text{the percentageof commercial land within}$$

$$a 100 \text{m radius of the premises receiving the noise}$$

$$\% \operatorname{TypeB}_{450} = \text{the percentageof commercial land within}$$

$$a 450 \text{m radius of the premises receiving the noise}$$

$$+ \operatorname{Traffic Factor}(\text{maximum of 6 dB})$$

$$= 2 \text{ for each secondary road within 100m}$$

$$= 2 \text{ for each major road within 450m}$$

$$= 6 \text{ for each major road within 100m}$$

Representative Assessment Period

Means a period of time not less than 15 minutes, and not exceeding four hours, determined by an inspector or authorised person to be appropriate for the assessment of a noise emission, having regard to the type and nature of the noise emission.

Background Noise

Background noise or residual noise is the noise level from sources other than the source of concern. When measuring environmental noise, residual sound is often a problem. One reason is that regulations often require that the noise from different types of sources be dealt with separately. This separation, e.g. of traffic noise from industrial noise, is often difficult to accomplish in practice. Another reason is that the measurements are normally carried out outdoors. Wind-induced noise, directly on the microphone and indirectly on trees, buildings, etc., may also affect the result. The character of these noise sources can make it difficult or even impossible to carry out any corrections.

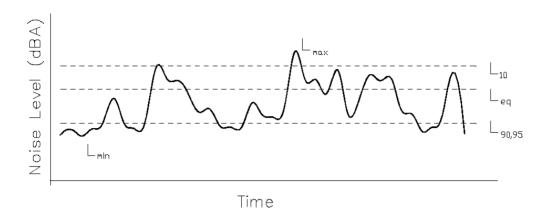
Ambient Noise

Means the level of noise from all sources, including background noise from near and far and the source of interest.

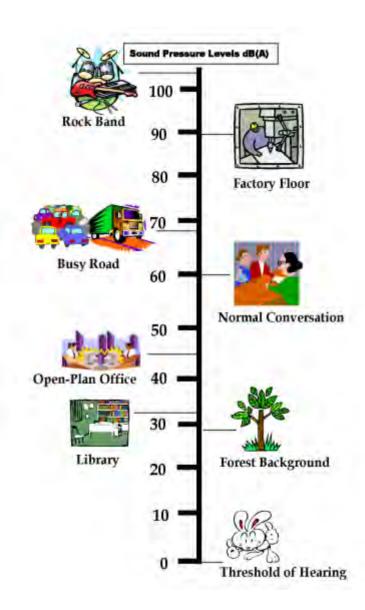
Specific Noise

Relates to the component of the ambient noise that is of interest. This can be referred to as the noise of concern or the noise of interest.

Chart of Noise Level Descriptors



Typical Noise Levels



MARITIME DRIVE, 500 (LOT 9047) JINDALEE – MULTIPLE DWELLINGS (33 UNITS), HOLIDAY ACCOMMODATION (14 UNITS) AND RESTAURANT

Form 1 – Responsible Authority Report (Regulation 12)

DAP Name:	Metro Outer JDAP		
Local Government Area:	City of Wanneroo		
Applicant:	Pinnacle Planning		
Owner:	Lineic Group Jindalee Pty Ltd (under		
	contract)		
Value of Development:	\$13 million		
	□ opt In (Regulation 6)		
Responsible Authority:	City of Wanneroo		
Authorising Officer:	Greg Bowering		
LG Reference:	DA2022/1521		
DAP File No:	DAP/22/02391		
Application Received Date:	2/12/2022		
Report Due Date:	9031 March 2023		
Application Statutory Process	90 Days with an additional 31 days		
Timeframe:			
Attachment(s):	Development Plans		
	2. Location Plan		
	3. Landscape Plan		
	4. Perspectives		
	5. Summary of Submissions		
	6. Design Review Panel Recommendations		
	with Applicant's response and City's		
	Comments		
	7. Traffic Technical Note		
	Revised Acoustic Report		
	9. Revised Waste Management Plan		
	10. Sustainable Design Assessment Report		
Is the Responsible Authority	☐ Yes Complete Responsible Authority		
Recommendation the same as the	⊠ N/A Recommendation section		
Officer Recommendation?			
	☐ No Complete Responsible Authority		
	and Officer Recommendation		
	sections		

Responsible Authority Recommendation

That the Metro Outer JDAP resolves to:

1. **Approve** DAP Application reference DAP/22/02391 and accompanying plans (**Attachment 1**) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes)*

Regulations 2015, and the provisions of the City of Wanneroo's District Planning Scheme No. 2, subject to the following conditions:

Conditions

- This decision constitutes planning approval only and is valid for a period of four years from the date of approval. If the development is not substantially commenced within the specified period, the approval will lapse and be of no further effect.
- 2. Development must not commence on the site until the proposed the lot with an area of 3081m² as shown on the Western Australian Planning Commission's subdivision approval 161714 is registered on a Deposited Plan and title has been issued.
- 3. Development must not commence on site prior to the site being provided gazetted road frontage to a constructed road to the southern boundary of the proposed lot with an area of 3081m² under the Western Australian Planning Commission's subdivision approval 161714.
- 4. Parking areas, driveways and points of ingress and egress must be designed and constructed in accordance with the Australian Standard for Offstreet Carparking (AS 2890) and must be drained, sealed, marked and maintained to the satisfaction of the City prior to occupation of the development.
- 5. Prior to the occupation of the development, bicycle racks for a minimum of five bicycles must be provided on-site at ground level for visitors to the satisfaction of the City.
- 6. The parking areas and associated access indicated on the approved plans must not be used for the purpose of storage or obstructed in any way at any time, without the prior approval of the City.
- 7. A revised Landscaping Plan which depicts the bin location area shown on the approved Waste Management Plan as well as the planting of a minimum of two (2) large trees, must be lodged for approval by the City prior to lodging a building permit. Planting and installation must be in accordance with the approved landscaping and reticulation plans and completed prior to occupation of the development and maintained thereafter, to the satisfaction of the City.
- 8. An onsite stormwater drainage system, which aligns with the bin location area sufficient to contain a 1:100 year storm event (over 24 hours) must be provided. Plans illustrating the system proposed must be submitted for approval prior to an application is being made for a building licence and the system must be installed during the construction of the development.
- 9. The development must comply with all assumptions and recommendations of the Acoustics Report (Ref 28867-3-21498) prepared by Herring Storer Acoustics dated February 2023. Recommended works must be completed prior to the commencement of the use.
- 10. An Environmental Noise Assessment from a qualified acoustic (noise) consultant must be submitted to the City following construction of the development to demonstrate that the development complies with the

- Environmental (Noise) Regulations 1997 prior to occupancy of the development and must also include an assessment of the impact of the lift on the residential apartments.
- 11. A minimum of 7 dwelling units must comply with the Silver Level requirements as defined in the Liveable Housing Design Guidelines and must be demonstrated at the building permit stage.
- 12. Development is to be implemented in accordance with Sustainable Design Assessment Report (Ref 22-14101) prepared by Sustainability WA and dated 16 December 2022.
- 13. Lighting must be installed along all driveways and pedestrian pathways and in all common service areas prior to the development first being occupied and thereafter maintained to the satisfaction of the City.
- 14. All storage areas, external fixtures and building plant, including air conditioning units and water tanks must be located so as to minimise any visual and noise impact on surrounding landowners and screened from view from streets, public places and adjacent properties to the satisfaction of the City.
- 15. Waste management must be undertaken in accordance with the approved Waste Management Plan dated 24 February 2023 prepared by Pinnacle Planning or as agreed by the City in writing.
- 16. A Construction Management Plan must be submitted and approved prior to the commencement of any development. The plan must detail how construction of the development will be managed to minimise disruption to adjoining landowners. The plan must address the following:
 - a) The delivery times for materials and equipment to the site;
 - b) Storage of materials and the location and type of equipment on site;
 - c) Adequate measures be implemented during construction to minimise any
 - d) adverse impacts caused by sand drift and dust from the site;
 - e) Parking arrangements for contractors and sub-contractors;
 - f) Construction times;
 - g) Dilapidation reports;
 - h) Measures to minimise noise impacts on surrounding residents; and
 - i) Any other matter required by the City.
- 17. A short stay management plan is to be submitted to and approved by the City prior to lodging a building permit. The management plan shall include the following:
 - a) The maximum number of guests to be accommodated. This should also include information as to whether pets, or non-registered guests are permitted on the premises.
 - b) Details regarding the expected behaviour and obligations of guests, including the control of noise. This should include a complaints management procedure, which provides a contact telephone number for the owners/managing agents for the property. This number should also be made available for adjoining neighbours;
 - c) Details regarding check-in and check-out procedures;

- d) Details regarding waste management. This should detail the expectations of guests with regards to general rubbish and bin collection; and
- e) Any other matter considered relevant by the City.
- 18. A guest register of all persons occupying the short-term accommodation is required to be kept by the applicant/landowner and shall be made available for inspection by the City on request. This guest register shall include:
 - a) Name and address of every occupant staying within the premises; and
 - b) The date of arrival and date of departure of the occupants.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme	
Region Scheme -	Urban	
Zone/Reserve		
Local Planning Scheme	District Planning Scheme No. 2	
Local Planning Scheme -	Urban Development	
Zone		
Structure Plan	ASP 36 - Lot 12 Jindalee Agreed Local Structure	
	Plan	
01 1 01 10	ASP 78 – Jindalee Coastal Village	
Structure Plan/Precinct Plan	Commercial R80	
- Land Use Designation	Multiple Duvelling Demoitted (D)	
Use Class and		
permissibility:	Holiday Accommodation - Discretionary (D) use	
Lot Size:	Restaurant - Discretionary (D) use 3081 m ²	
Existing Land Use:	Vacant	
State Heritage Register	No	
Local Heritage	⊠ N/A	
Local Hemage		
	☐ Heritage List	
	□ Heritage Area	
Design Review	□ N/A	
	□ State Design Review Panel	
	□ Other	
Bushfire Prone Area	Yes	
Swan River Trust Area	No	

Proposal:

The development entails the following:

- A three (3) storey development consisting of thirty-three (33) one and two bed multiple dwellings, fourteen (14) one and two bed holiday accommodation units and one (1) ground floor 147m² (including alfresco) restaurant (café);
- Under croft parking, storage and bin storage area. A total of eighty-one (81) car
 parking bays and 14 motorcycle bays are provided on-site for the use of
 residents. In addition, five (5) bays, including an ACROD bay, are proposed on

- the ground floor level, three (3) of which are proposed as off-site embayment parking; and
- Main vehicle access to the basement level is provided from the south with access to the two (2) on-site restaurant bays provided off Maritime Drive to the east.

Proposed Land Use	Multiple Dwelling	
	Holiday Accommodation	
	Restaurant	
Proposed Net Lettable Area	97m ² (Restaurant)	
Proposed No. Storeys	Three (3)	
Proposed No. Dwellings	33 Multiple Dwellings	
	14 Holiday Accommodation Units	

Background:

The subject site has an area of 5.9157 ha and is located within the Jindalee Coastal Village precinct. The site is separated from the Indian Ocean foreshore to its east by the Regional Parks and Recreation regional reserve and is located just over 900 metres from Marmion Avenue further west.

Application for subdivision has been conditionally approved by the WAPC – Reference 161714 to create a future lot of 3081m^2 which will be the site of the proposed development. This subdivision approval also creates a portion of road reserve directly to its south. The development area was cleared of vegetation as part of subdivisional earthworks following the approval of WAPC 161714 on 25 February 2022 and is currently vacant. The subdivision process has not yet been completed and the future lot and road reserve to the south from which vehicular access will be gained have not yet been created. The land tenure will be addressed later in this report.

Land directly to the north is developed as Public Open Space (POS) with public parking provided in the adjoining Jindalee Boulevard road reserve between the subject site and the POS. Land further to the north is designated on the structure plan for future Commercial and Mixed Use development with a similar residential density of R80. Land further to the north, west and south generally consists of single house residential development.

The subject site has a fairly flat gradient, with a gentle slope of approximately 2 metres down from east to west, which somewhat contrasts with the much steeper gradient of land to its east and west.

A Location plan is included in **Attachment 2**.

Legislation and Policy:

Legislation

Metropolitan Region Scheme (MRS) City of Wanneroo District Planning Scheme No. 2 (DPS 2)

State Government Policies

State Planning Policy 3.7: Planning in Bushfire Prone Areas (SPP 3.7)

State Planning Policy 7.0: Design of the Built Environment (SPP 7.0)
State Planning Policy 7.3: Residential Design Codes Volume 2 – Apartments (SPP 7.3 Vol 2)

Structure Plans/Activity Centre Plans

Lot 12 Jindalee Agreed Local Structure Plan (ASP 36) Lot 12 Jindalee (Jindalee Beachside Estate) Coastal Village (ASP 78)

Local Policies

Draft Local Planning Policy 4.21 Short-Term Accommodation

Consultation:

Public Consultation

The application was advertised for a period of 21 days which included one week additional for the Christmas week, commencing on 19 December 2022 and concluding on 9 January 2023. Advertising was undertaken by way of letters to surrounding landowners within a 200 metre radius, a sign was placed on site, an advert was placed in the local newspaper and a notice published on the City's website. A total of 70 submissions were received during this time, with 51 (73%) objecting to the proposal, 11 (16%) in support and eight (11%) providing comments only.

The key issues raised in the submissions include the following:

- <u>Scale of development</u>: Development is significantly larger than anticipated causing a detrimental impact on the amenity, an eyesore. Development will be in front of existing dwellings and block their view;
- <u>Land Use</u>: The development will not be viable as there are already similar developments taking place in the area. Too much residential, not enough commercial - more café's/restaurants bars were expected, more commercial uses like shops are also expected. Holiday units not suitable, will spoil serenity of area. Apartments should have at least some residential, not all holiday lets;
- Parking: Inadequate provision for visitor parking;
- Orientation: Restaurant does not orientate towards the beach;
- Noise: Concerned about impact of noise from such a large number of apartments:
- Traffic: Increase in traffic and safety impacts;
- <u>Anti-social behaviour</u>: Speeding cars, noisy holiday makers having parties, increased littering, loitering, street fights and crime; and
- <u>Impact on other amenities</u>: Not enough amenities in the area to support a development of this size. Concerned about the increase in potential issues on the dog beach.

A summary of the submissions received and Administration's response is included in **Attachment 5**.

Referrals/consultation with Government/Service Agencies

The subject site abuts land reserved as Parks and Recreation under the Metropolitan Region Scheme. In accordance with the Instrument of Delegation under the Planning

and Development Act 2005 (DEL2022/03), the application was referred to the Department of Planning, Lands and Heritage (DPLH) for comment. No comment was received.

Design Review Panel Advice

The proposal underwent two rounds of pre-lodgement Design Review Panel (DRP) engagement which required amendments to the design, which is now considered to show a high level of congruence with all relevant design principles under State Planning Policy (SPP) 7.0.

Given the above, the proposed development plans submitted for consideration are considered to sufficiently address the design-based recommendations made by the DRP.

A table with a summary of the DRP's recommendations against each design principle and the applicant's responses with the City's comments are included in **Attachment** 6.

Planning Assessment:

The proposal has been assessed against the relevant provisions of ASP 36, ASP 78, SPP 3.7, SPP 7.0, SPP 7.3 Vol 2 and DPS 2. The following matters have been identified as key considerations for the determination of this application:

Zoning and land use permissibility

Under the City of Wanneroo (City) Planning Scheme No. 2 (DPS 2) the subject site is zoned 'Urban Development". The City of Wanneroo Agreed Structure Plan 78 (ASP 78) has been prepared to guide development of the Jindalee Coastal Village.

ASP 78, under Part 1, cl. 6.1 - Land Use Permissibility states that land use permissibility shall be in accordance with the requirements of the Scheme with certain variations applicable within both the Commercial (within which the subject land is located) and Mixed Use zones. The subject site is zoned "Commercial/Centre" under the ASP and assigned a residential density coding of R80.

The proposed land uses are all capable of approval, as follows:

- Multiple Dwelling Permitted (P) use
- Holiday Accommodation Discretionary (D) use
- Restaurant Discretionary (D) use

ASP 78 requires that the total retail floorspace within the structure plan area shall not exceed 500m² NLA, with no individual tenancy exceeding 200m² NLA. The 97m² restaurant (with an additional 50m² alfresco area) is considered appropriate in this context and is supported.

Height, bulk and scale

The ASP also provides built form guidance and development standards with respect to the development of sites and stipulates that buildings shall not exceed five storeys

(or 18.5m wall height, 23.5m to the top of the roof) above the approved level of the abutting street and/or POS.

Under ASP 78, maximum plot ratio requirements for commercial are 0.5:1 with 2:1 maximum required for residential. The application proposes a plot ratio of 1.26:1 and therefore complies. The predominantly three storey building also complies with the height requirements under ASP 78 and is deemed suitable in terms of bulk and scale in relation to both existing and future development as earmarked by the structure plan.

Setbacks

ASP 78 sets out the required ground and first floor setbacks as follows:

Boundary	Required	Provided
North: Mandatory Active Edge/Nil Setback	0m	0m min
		3.01m max
East - northern section: Mandatory Active	0m	1.5m min
Edge/Nil Setback		2.1m max
East – southern section: Potential Active Edge	0m min	4.255m
	6m max	
South	3m min	0m min
		6.75m max
West	3m min	0m min
		1m max

For all storeys above the first but excluding the top floor, ASP 78 stipulates that the street setback shall be a minimum of that of the storey below. The top floor must be setback a minimum of 3m further than the storey below.

Several variations to the setback requirements are proposed, partly due to the irregular shaped lot. It is also due to the fact that the development largely consists of residential apartments which ideally should be setback further than zero metres to provide a degree of separation from the lot boundaries, particularly where adjoined by roads and dual use paths (to the north, east and west) for purposes of privacy within the apartments.

Given the only commercial use is the small café on the north-eastern corner, it is considered acceptable that greater than nil metre setbacks are provided to the residential apartments as it serves to facilitate visual privacy and a greater degree of separation from pedestrian traffic.

The top storey is not setback a minimum of 3m further than the storey below, but this is considered acceptable given the lower storeys have already been setback further than required and the overall height is lower than the maximum five storeys that is permissible. The design outcome is considered acceptable and suitable in terms of bulk and scale in relation to its location.

The applicant has stated that the nil setback requirement places units too close to the public parking area, and reduces internal amenity considerations, but notwithstanding, direct access staircases are used to provide an active frontage to the short stay unit's front balcony/terrace.

The café tenancy is setback to facilitate a larger alfresco seating area within the site and road reserve. The design of the walk-ups to the short stay units, and the café design, all contribute to provide an engaging frontage and on this basis the setback variations are considered acceptable.

The subject site does not directly abut any other landholdings, with road reserves, footpaths parking areas and open spaces providing adequate separation in proportion to building height.

Access and traffic

The access and egress arrangement of the proposed development is facilitated via two proposed driveway crossovers; one on Maritime Drive and one on the future access road on the southern side of the subject site as per ASP 78 requirements. The proposed crossover on the future access road provides direct access to the under croft car parking area via a two-way ramp system with separate entry and exit ramps. The proposed crossover on Maritime Drive provides access to the visitor parking adjacent to the main entrance of the development at the ground floor level.

Pedestrian access to the subject site is available via the existing external concrete footpath along Maritime Drive and along the western side of the subject site. Pedestrian crossing facilities with refuge islands and drop kerbs are provided at the roundabout intersection of Jindalee Boulevard/Maritime Drive.

Shared paths are provided along both sides of Maritime Drive adjacent to the subject site and provide connectivity to the other shared path that runs along Jindalee Boulevard.

The traffic analysis undertaken in support of the application demonstrates that the estimated traffic generated by the development will have minimal impact on the surrounding road network and concludes that the subject site features good accessibility by the existing road network and pedestrian/cyclist paths.

Parking

The development provides for an overall surplus of 7 car bays, as follows:

Number of car bays required:

Multiple dwellings: 44

Short Stay: 18

Visitors residential: 7

• Restaurant: 5

Total number of car bays required: 74

Number of car bays provided:

Under croft: 76

In front of restaurant (including 1 accessible bay): 2

• Off-site (embayment parking): 3

Total number of car bays provided: 81

The development also provides for an overall surplus of other bays as required for motorcycles and bicycles, as follows:

Number of other bays required:

Motorcycle: 6Bicycle: 24

• Visitor bicycle: 5 bays

Number of other bays provided:

Motorcycle: 14Bicycle: 47Visitor bicycle: 0

In total 74 car bays are required, with 78 provided on-site (of which 76 under croft and 2 in front of the restaurant, including and accessible bay) and an additional 3 bays off-site for an overall number of 81 bays. This results in the surplus of 7 bays.

Similarly, a surplus of parking bays is provided for motorcycles and bicycles. It is suggested that, should the application be approved, a condition be included for the installation of five bicycle bays for visitors.

It is considered that, with the surplus bays provided on-site, the demand for off-site parking will be less as a direct result. The proposed development exceeds minimum parking requirements and is therefore supported.

Landscaping

R-Code requirements require that the proposed development should either provide two large trees with small trees to suit, or five large and five medium trees.

The development does not propose any large trees. It is proposed to plant six medium trees and thirteen small trees. It is suggested that, should the application be approved, a condition be included that requires the landscape plan to be modified to include a minimum of two large trees.

<u>Waste</u>

The applicant has submitted a waste management plan in support of the development. Under the *WALGA Multiple Dwelling Waste Management Plan Guidelines*, for bins up to and including 360L the bin transfer grade should not exceed a 1:14 gradient. The development plans show a 1:5 gradient at the access point. Notwithstanding, Administration considers the optional reduction in bin size to 240L size which is to be collected by private contractors twice weekly is an acceptable outcome.

Bushfire

Although located within a designated bushfire prone area, the BAL contour maps obtained in October 2022 have confirmed that the subject site has now been cleared of vegetation as part of subdivision earthworks and is predominantly BAL 12.5 with a small section being BAL-Low. This means that the level of bush fire resistant construction required for the building will be dealt with as part of the building permit process and no bushfire management plan is therefore required.

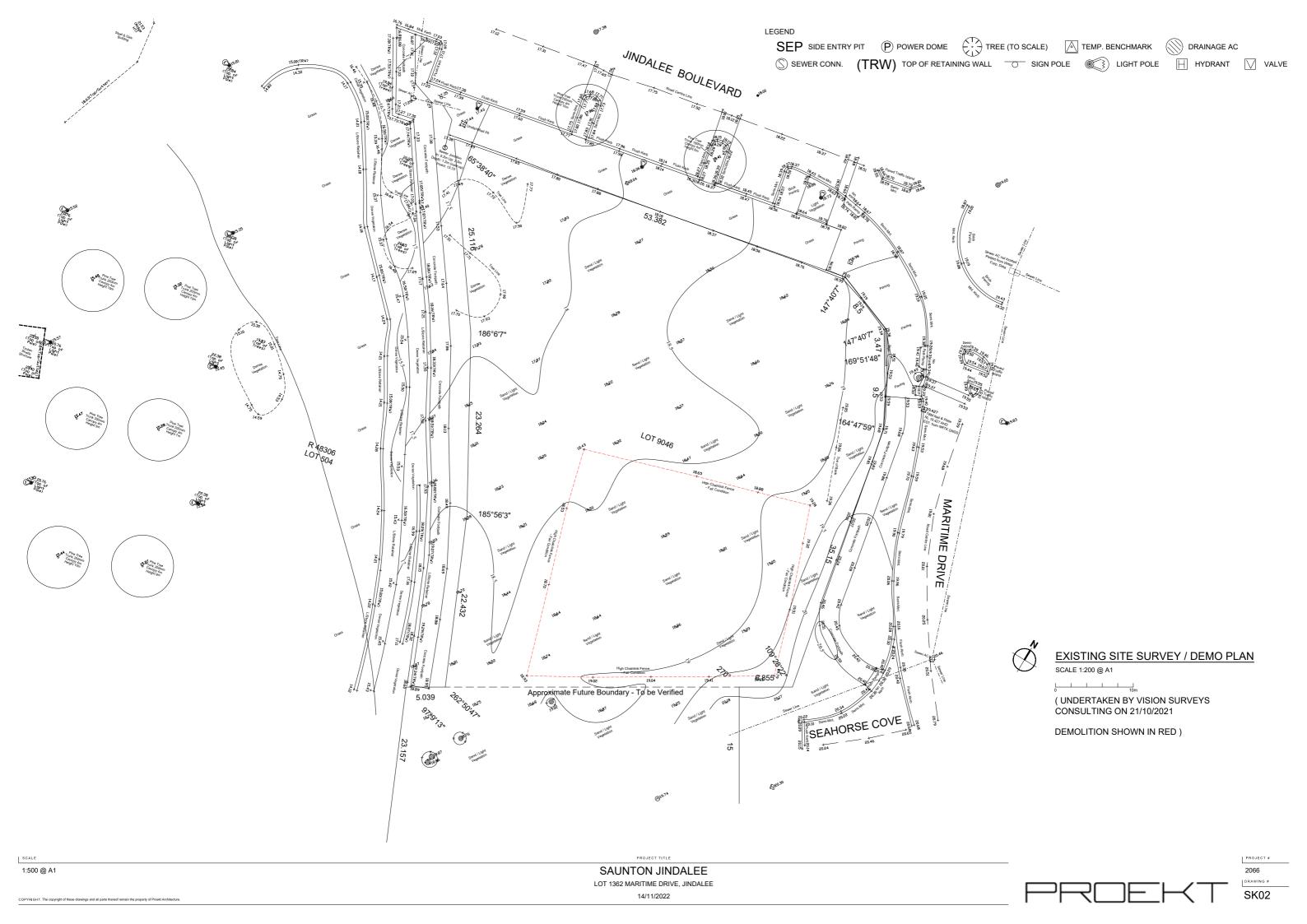
Subdivision

Subdivision was granted approval under WAPC subdivision application No. 161714 for the creation of the proposed lot with an area of $3081m^2$ on which the development is to be constructed. However, the subdivision clearance process has not yet been completed and the proposed lot and future road reserve to the south from which access will be gained have therefore not yet been registered on a Deposited Plan.

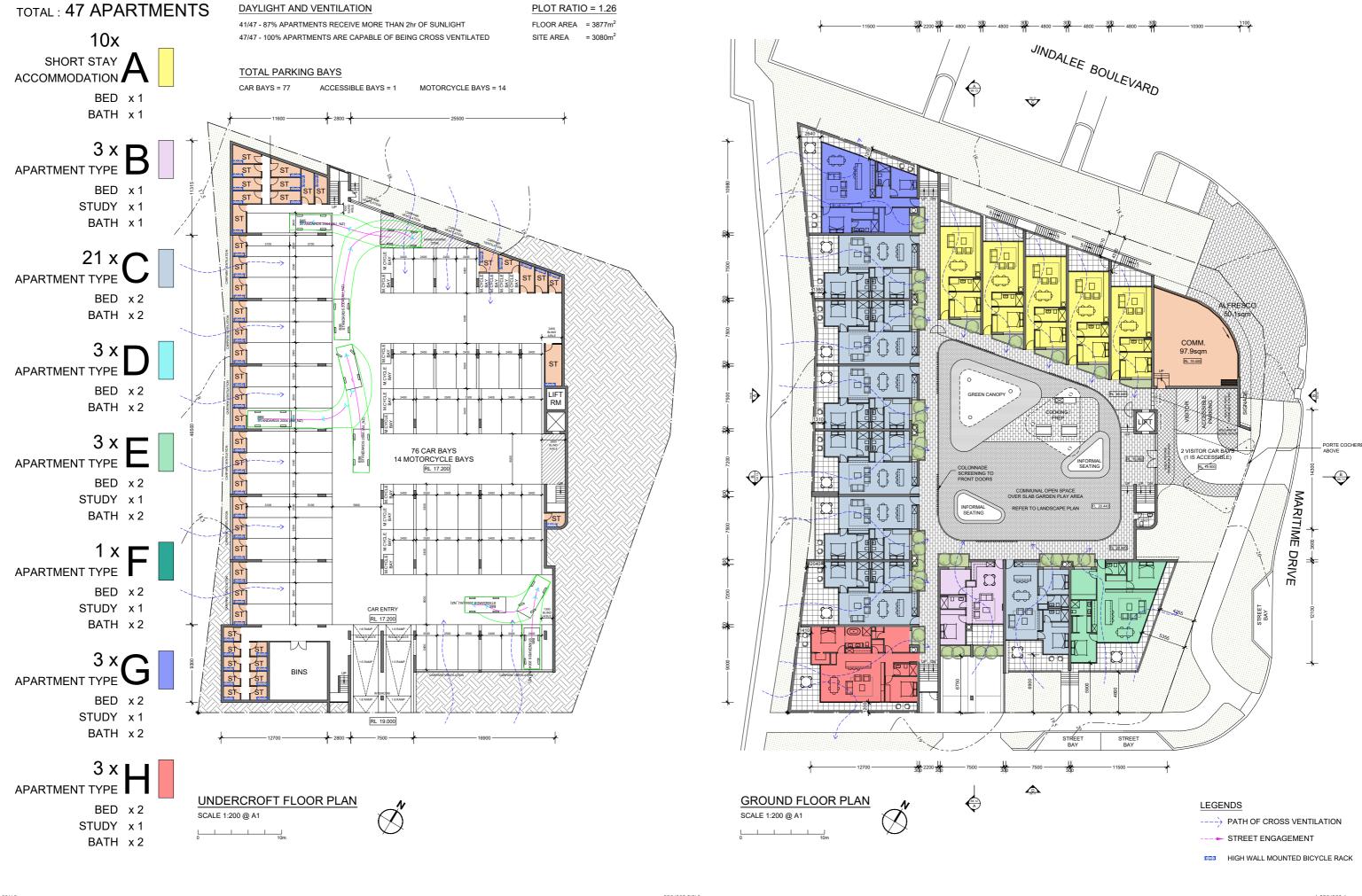
It is therefore required that, should this development application be approved, a condition be imposed that requires the creation of the proposed lot with an area of $3081m^2$ as well as the future road reserve to its south, in accordance with the abovementioned subdivision approval, prior to the commencement of works.

Conclusion

The proposed development is generally compliant with all applicable scheme provisions and other relevant development standards and requirements prescribed by applicable local planning policies and structure plans. The proposed bulk and scale of the development is considered appropriate for the location, being substantially below the maximum permitted height and with minor setback variations. The proposed land uses are suitable and capable of approval in the zone. The overall development is consistent with the intent of the objectives of structure plan ASP 36 and ASP 78 and is supported.







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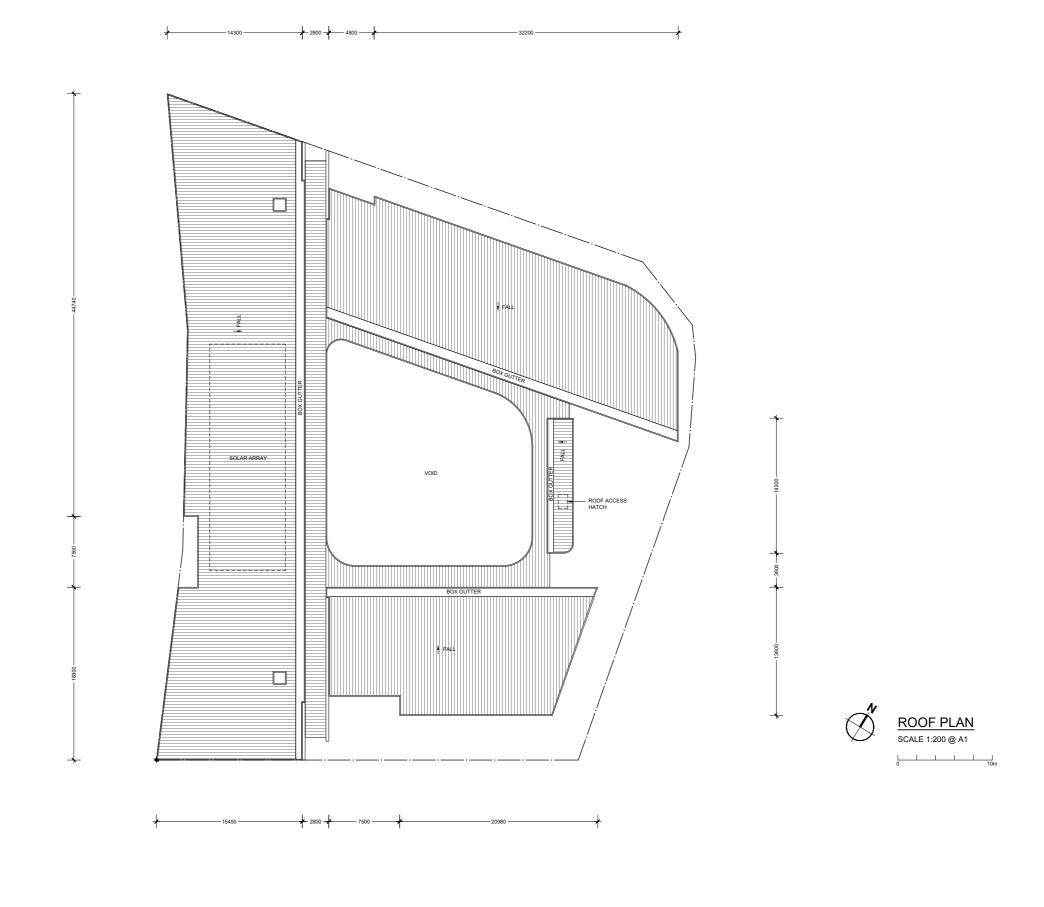
SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 16/02/2023



PROEKT

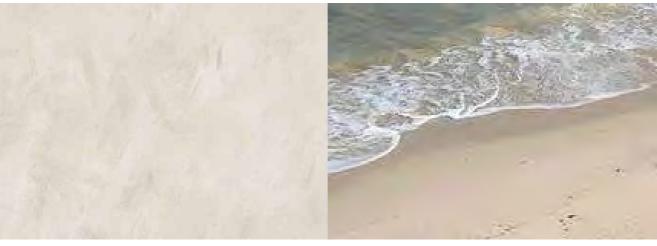
SK05



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SAUNTON JINDALEE

2066



1. EF-01 - Spanish Plaster Finish

Jindalee Beach Reference



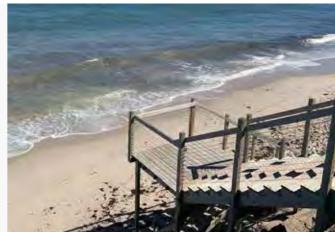
4. EF-04 - Contrasting Paint Finish 5. EF-05 - Contrasting Paint Finish



2. EF-02 - Feature Banding Fascia



Jindalee Beach Reference



6. EF-07 - Base Paint Finish



Jindalee Beach Reference



3. EF-03 - UPVC Cladding

Jindalee Beach Reference



(FACING OCEAN)

WEST ELEVATION A

SCALE 1:100 @ A1







PERSPECTIVE VIEW NOT TO SCALE

1:100 @ A1

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE

14/11/2022









PERSPECTIVE VIEW NOT TO SCALE

PERSPECTIVE VIEW NOT TO SCALE

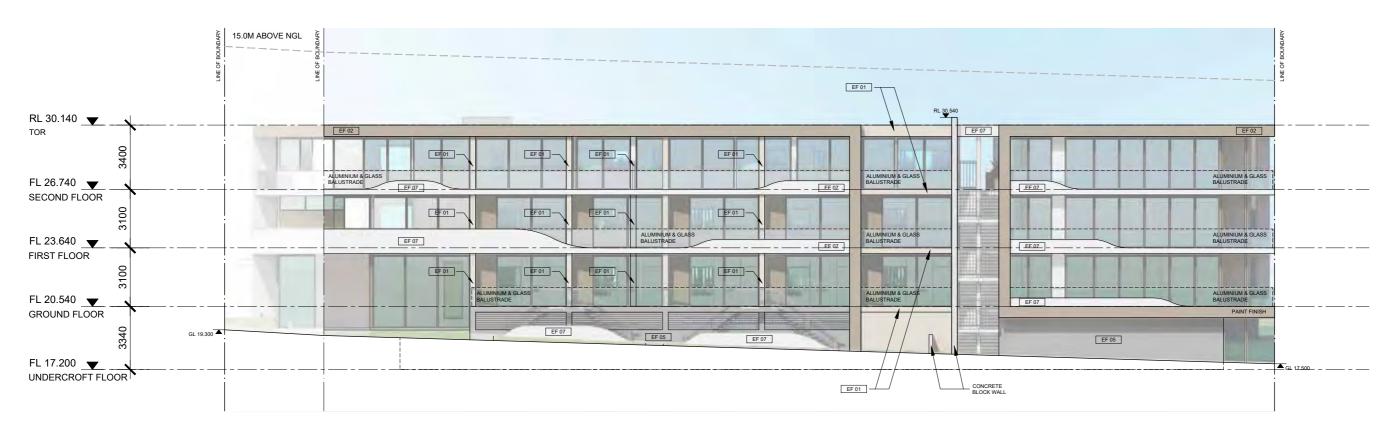
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SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE

14/11/2022

PROEKT



(FACING JINDALEE BOULEVARD)

NORTH ELEVATION C

SCALE 1:100 @ A1







PERSPECTIVE VIEW NOT TO SCALE

1:100 @ A1

PROJECT TITLE

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE

14/11/2022





(FACING SEAHORSE COVE)

SOUTH ELEVATION D SCALE 1:100 @ A1







PERSPECTIVE VIEW NOT TO SCALE

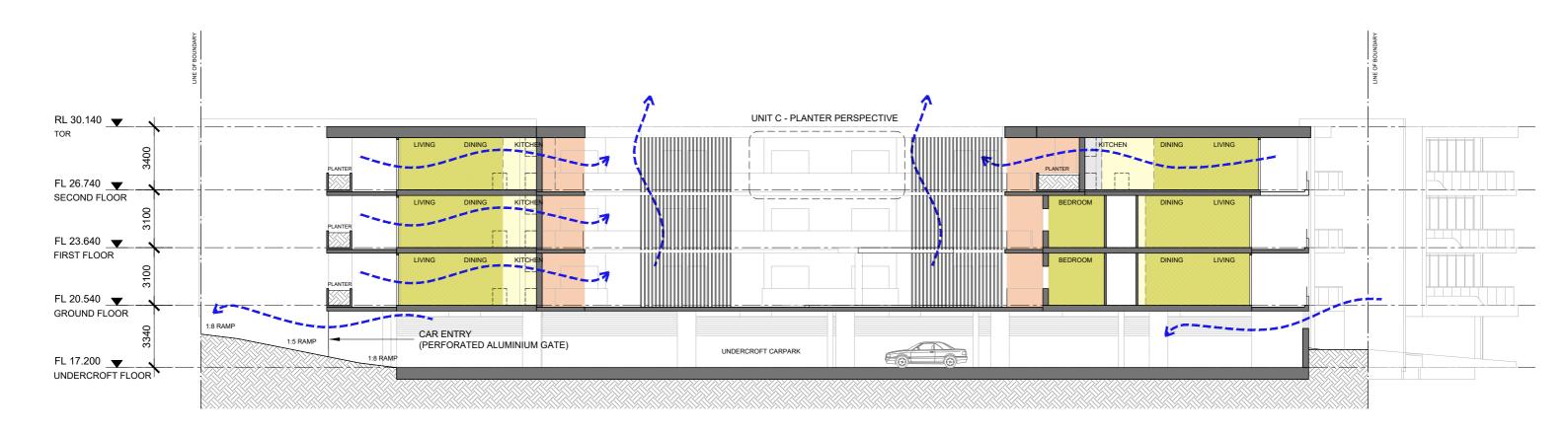
1:100 @ A1

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE

14/11/2022





SECTION A SCALE 1:100 @ A1







UNIT C - KITCHEN WINDOW TO PLANTER NOT TO SCALE

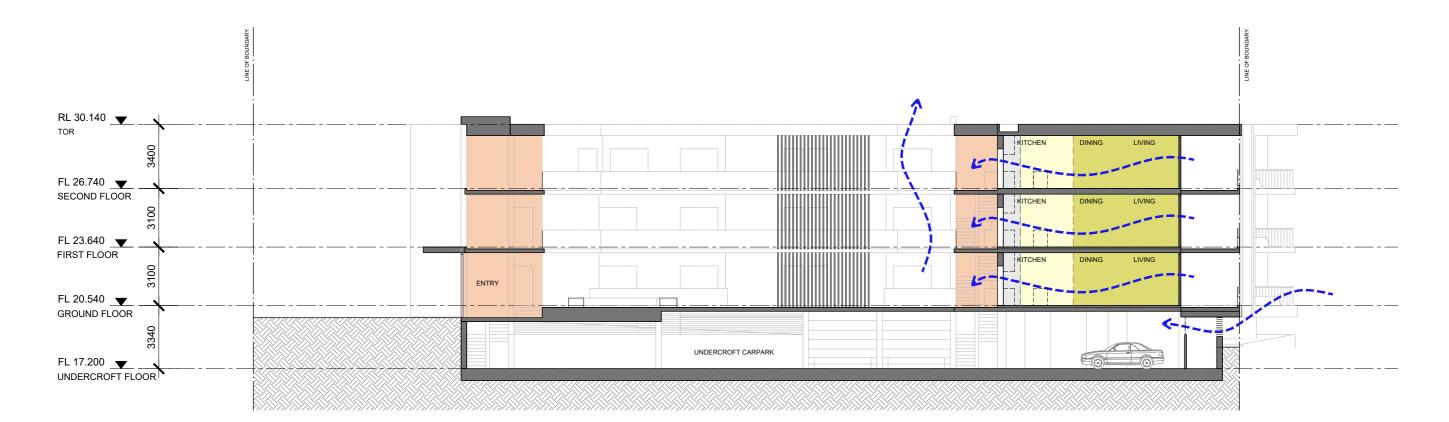
LEGENDS RESIDENTIAL CORRIDOR OPTIMUM DAYLIGHT AND VENTILATION ACCEPTABLE DAYLIGHT AND VENTILATION ADDITIONAL LENGTH MAY BE PROVIDED FOR KITCHEN

2066

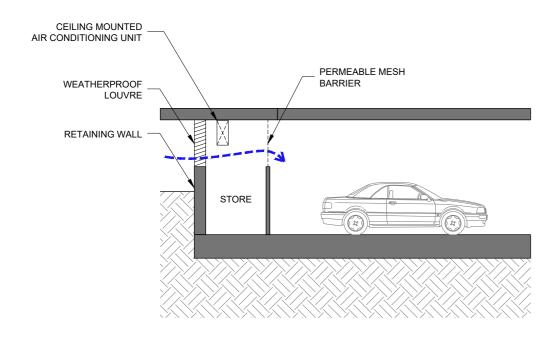
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SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 14/11/2022



SECTION B SCALE 1:100 @ A1



TYPICAL STORE SECTION SCALE 1:50 @ A1

LEGENDS

RESIDENTIAL CORRIDOR

OPTIMUM DAYLIGHT AND VENTILATION

ACCEPTABLE DAYLIGHT AND VENTILATION

ADDITIONAL LENGTH MAY BE PROVIDED FOR KITCHEN

PROJECT #

SK13

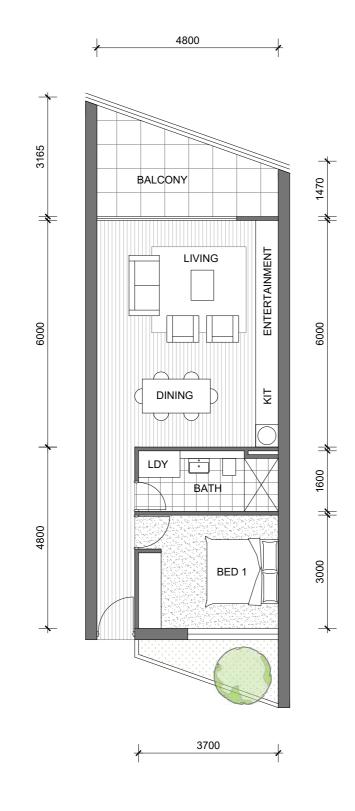
2066

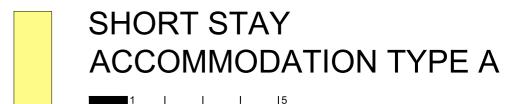
1:100 @ A1

PROJECT TITLE

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 14/11/2022 PROEKT





BED x 1 BATH x 1

STRATA AREA

LIVING -52.3m² BALCONY -10.1m²

STORE - 3.3m² (VARIES)

PROJECT TITLE

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE

14/11/2022

PROEKT



APARTMENT TYPE B

JINDALEE APARTMENTS

BED x 1 STUDY x 1 **BATH** x 1

STRATA AREA

- 60.7m² LIVING - 10m² **BALCONY**

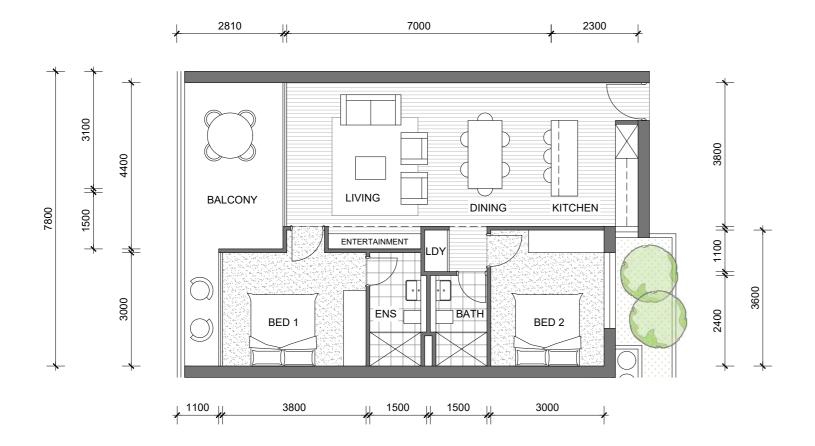
- 3.3m² (VARIES) STORE

1:50 @ A1

SAUNTON JINDALEE

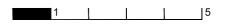
LOT 1362 MARITIME DRIVE, JINDALEE 14/11/2022







JINDALEE APARTMENTS



BED x 2 BATH x 2

STRATA AREA

LIVING - 72.4m²

BALCONY - 13.9m² - 23.9m² (VARIES)

STORE - 4m² (VARIES)

1:50 @ A1

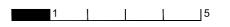
PROJECT TITL

PROFKT





JINDALEE APARTMENTS



BED x 2 BATH x 2

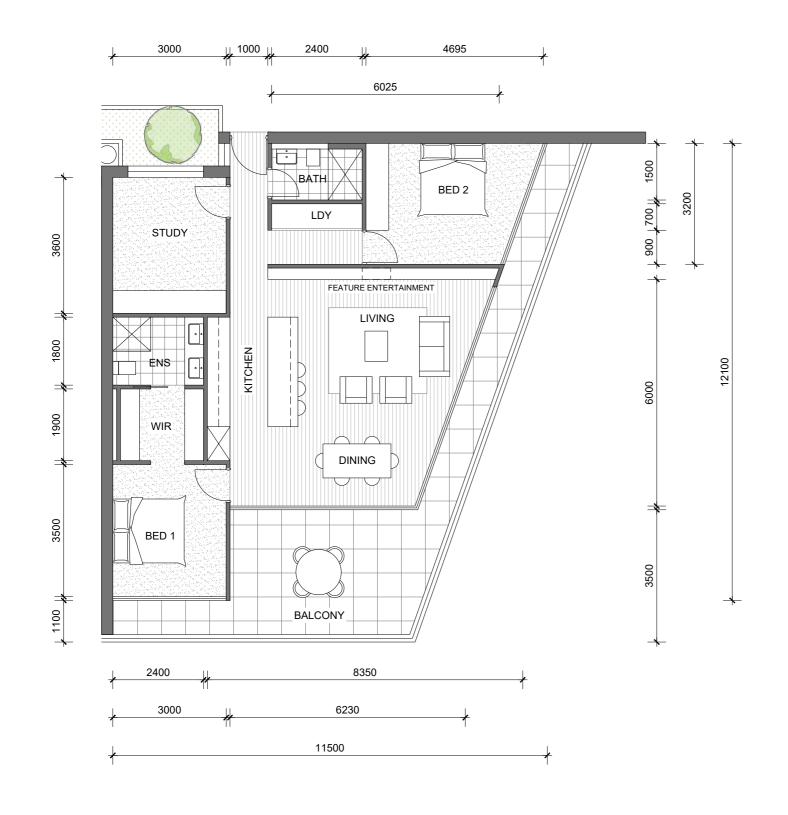
STRATA AREA

LIVING $-80.3m^2$ BALCONY $-16.9m^2$ STORE $-4m^2$ (VARIES)

1:50 @ A1

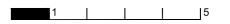
PROJECT TITLE

PROFKT



APARTMENT TYPE E

JINDALEE APARTMENTS



BED x 2 STUDY x 1 **BATH** x 2

STRATA AREA

- 99.6m² LIVING - 29.7m² BALCONY

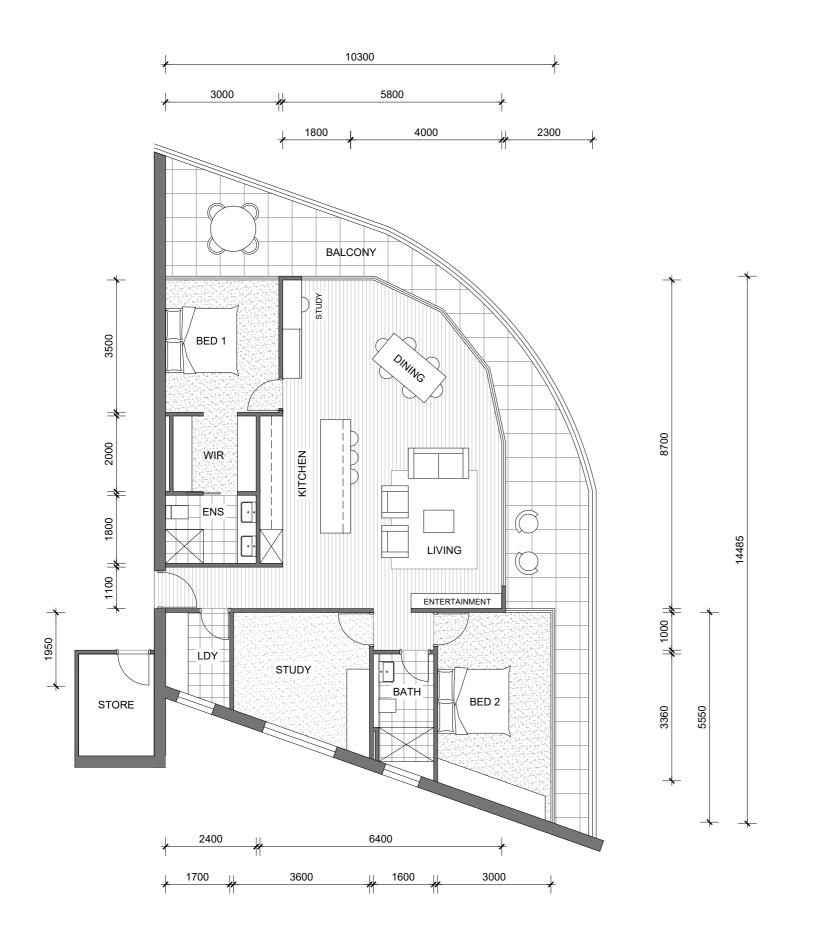
- 5.1m² (VARIES) STORE

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 14/11/2022

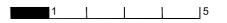


1:50 @ A1



APARTMENT TYPE F

JINDALEE APARTMENTS



BED x 2 STUDY x 1 BATH x 2

STRATA AREA

LIVING $-114.7m^2$ BALCONY $-34.8m^2$ STORE $-5.4m^2$

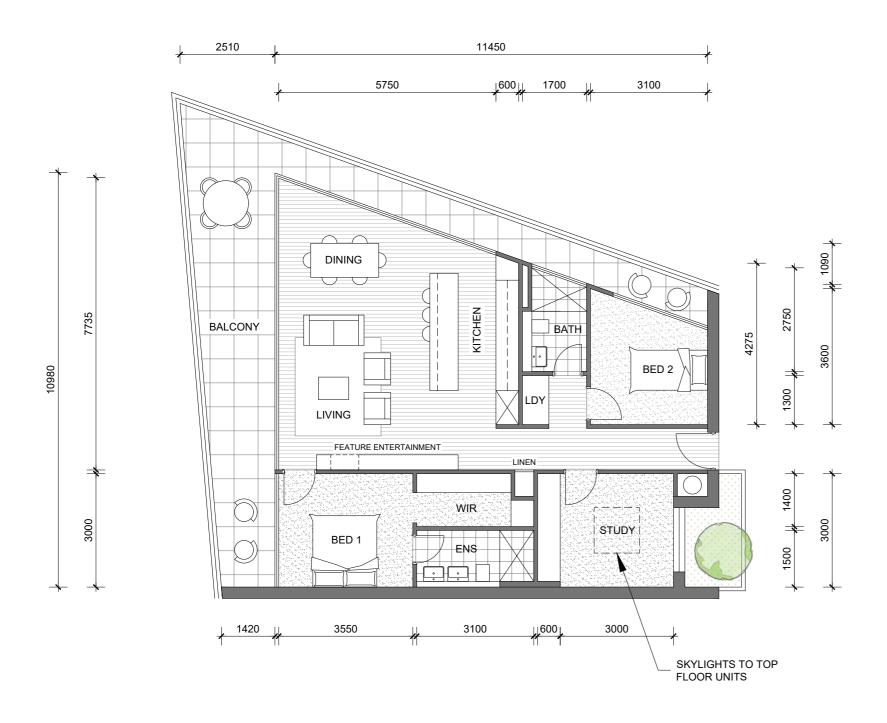
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PROJECT TITLE

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 14/11/2022







JINDALEE APARTMENTS



BED x 2 **STUDY** x 1 **BATH** x 2

STRATA AREA

- 100m² LIVING - 34.4m² **BALCONY**

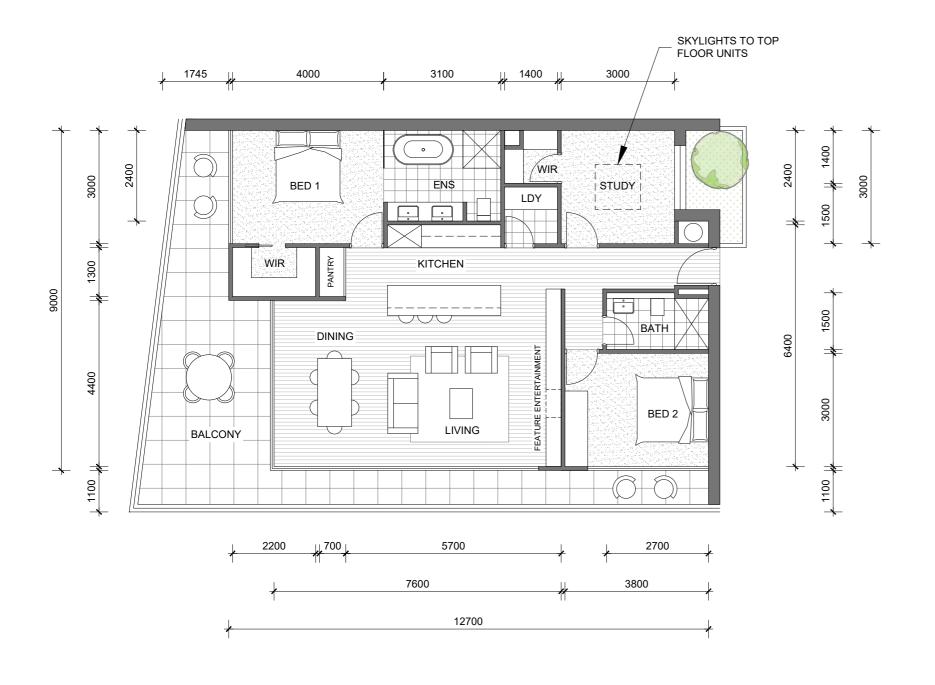
- 5.1m² (VARIES) STORE

1:50 @ A1

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 14/11/2022







JINDALEE APARTMENTS

1 | | 5

BED x 2 STUDY x1 BATH x 2

STRATA AREA

LIVING $-106.6m^2$ BALCONY $-34m^2$

STORE - 5.1m² (VARIES)

1:50 @ A1 SAUNTO

SAUNTON JINDALEE
LOT 1362 MARITIME DRIVE, JINDALEE

LOT 1362 MARITIME DRIVE, JIN 14/11/2022





LOCATION PLAN



SAUNTON JINDALEE LOT 1362 MARITIME DRIVE, JINDALEE

14/11/2022



 THIS IS A CONCEPT PLAN ONLY.
 ALL STRUCTURES SUBJECT TO ENGINEERING AND COUNCIL APPROVAL. 3. ALL MEASUREMENTS TO BE CHECKED PRIOR TO CONSTRUCTION.





SELECTED FEATURE FLATING
CARDBOARD PLANT, AGAVES, ALOES

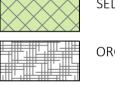
PLANTING



PLANTING TYPE 03 MIXED/STRAPPY PLANTING TYPE 04

RAISED PLANTERS PLANTING TYPE 05 SHADE PLANTING

SELECTED LAWN SPECIES SOFT LEAF BUFFALO SELECTED ARTIFICIAL TURF



ORGANIC MULCH

DEEP SOIL AREAS (DSA)



Total Landscape areas (Ground floor, Level 01 + Level 02) = 689.2m2 (22.4% of site) DEEP SOIL AREAS (DSA) Ground Floor = 303.5m2 On Slab (community area) = 22.6m2 (halved to 11m2 for DSA on structure)

Total = 313.5m2 (10.2% of site) CANOPY COVER

Proposed Medium Trees (6m canopy) x 9 = 254.7m2 Proposed Medium/Small Trees (5m canopy) x 8 = 156.82m2 Proposed Small Trees (4m canopy) x 2 = 25m2 Total Canopy Cover = 436.5m2 (14.2% of site)

REFER PAGE 102 FOR PLANTING NOTES



PLANTING PALETTE

Symbol	Species	Common Name	Quantities	Size
Trees:				
AGOffe	Agonis flexuosa	Weeping Peppermint	As shown	100L
BANint	Banksia integrifolia	Coastal Banksia	As shown	100L
BAUbla	Bauhinia blakeana	Hong King Orchid Tree	As shown	100L
EUCtod	Eucalyptus todtiana	Coastal Blackbutt	As shown	100L
EUCuti	Eucalyptus utilis	Coastal Mort	As shown	100L
HAKlau	Hakea laurina	Pin Cushion Hakea	As shown	100L
HYMfla	Hymenosporum flavum	Native Frangipani	As shown	100L
M ELleu	Melaleuca leucadendra	Weeping Paper Bark	As shown	100L
METtho	Metrosideros thomasii	New Zealand Christmas Tree	As shown	100L
OLEeur	Olea europaea	Bambalina Olive	As shown	100L
Shrubs and Grou	ndcovers:			
CARaus	Carpobrotus 'Aussie Rambler'	Pig Face	+	
CAScou	Casuarina glauca 'Cousin It'	Cousin It	3/m2	140mm
CLImin	Clivia miniata	Bush Lily	3/m2	140mm
CONcan	Conostylis candicans	Grey Cotton Heads	3/m2	140mm
DIAeme	Dianella tasmanica 'Emerald Arch'	Emerald Arch	3/m2	140mm
DIAbla	Dianella tasmanica 'Blaze'	Blaze	3/m2	140mm
DICsil	Dichondra 'Silver Falls'	Silver Falls	3/m2	140mm
GAZtom	Gazania tomentosa	Silver Leaf Gazania	3/m2	140mm
EREblu	Eremophila 'Blue Horizon'	Blue Horizon	3/m2	140mm
FlOnod	Ficinia nodosa	Knotty Club Rush	3/m2	140mm
HARwhi	Hardenbergia violaceae 'White Out'	White Native Wisteria	3/m2	140mm
UNcon UNcon	Junierus conferta	Shore Juniper	3/m2	140mm
LEUbro	Leucophyta brownii	Silver Cushion Bush	3/m2	140mm
LIMper	Limonium perezii	Sea Lavender	3/m2	140mm
LIRjus	Liriope muscari 'Jist Right'	Just Right'	3/m2	140mm
LOM sea	Lomandra 'Seascape'	Seascape	3/m2	140mm
LOMtan	Lomandra 'Tanika'	Tanika	3/m2	140mm
PiTmis	Pittosporum tobira 'Miss Muffet'	Miss Muffet	3/m2	200mm
MELlit	Melaleuca 'Little Nessie'	Dwarf Honey Myrtle	3/m2	200mm
MYOpar	Myoporum parvifolium 'Yareena'	Yareena	3/m2	140mm
OLEaxi	Olearia axillaris 'Little Smokey'	Little Smokey	3/m2	140mm
PHlaxn	Philodendron xanadu	Xanadu	3/m2	200mm
POAlab	Poa labillardierei	Tussock Grass	3/m2	140mm
RHAspi	Rhagodia spinescens	Spiny Saltbush	3/m2	140mm
RHAori	Rhapiolepsis 'Oriental Pearl'	Dwarf Indian Hawthorn	3/m2	200mm
ROScre	Rosmarinus prostrate	Creeping Rosemary	3/m2	140mm
SANtri	Sansevieria trifasciata laurentii	Mother-in-law's Tongue	3/m2 3/m2	200mm
		Thick-leaved Scaevola	3/m2 3/m2	
SCAcra SCAbum	Scaevola crassifolia	Fan Flower		140m m
SCAhum WESaus	Scaevola humilis 'Purple Fusion'		3/m2	140mm
	Westringia 'Aussie Box'	Aussie Box	3/m2	140mm
WESgre	Westringia 'Grey Box'	Grey Box	3/m2	140m m
Feature Plants:				
AGAatt	Agave attenuata	Foxtail	As Shown	12L
ALOtop	Aloe hybrid	Topaz	As Shown	12L
STRreg	Strelitzia reginea	Bird of Paradise	As Shown	12L
ZAMfur	Zamia furfuracea	Cardboard Palm	As Shown	12L
	1	The state of the s		1

PLANTING IMAGES

















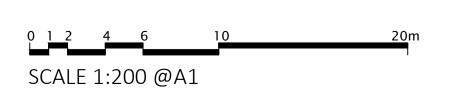


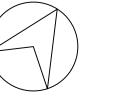
JINDALEE DEVELOPMENT LANDSCAPE CONCEPT PLAN - GROUND FLOOR

DEVELOPMENT APPROVAL

REV D

JOB No. 0182 **PAGE 101**







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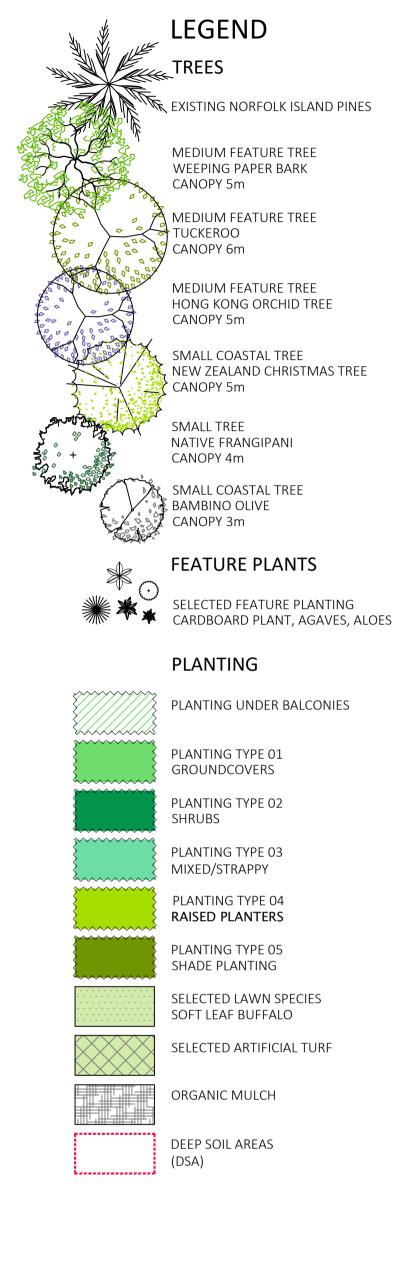
mob: 0450 965 569

email: kelsie@kdla.com.au

kelsie davies

landscape architecture





REFER PAGE 101 FOR PLANTING PALETTE AND PLANTING CALCULATIONS



1. GENERAL

REV DATE

A 21.09.22

- 1.1 DRAINAGE FROM THE RAISED PLANTER AREAS AND POTS TO BE PROVIDED BY BUILDER
- 1.2 ALL SCALES ARE AS NOTED AND TO SUIT A1 PAPER SIZE
- 1.3 FOR ALL FINISHED LEVELS, PLANTER HEIGHTS, DRAINAGE DESIGN AND WATER CONNECTION POINTS REFER TO ASSOCIATED PROJECT DOCUMENTATION (BY OTHERS).

2. SOIL PREPARATION

2.1 ALL AREAS ARE TO BE FINE GRADED EVENLY TO CONFORM TO KERB LEVELS AND SURROUNDING FINISHES. 2.2 SURFACES SHALL BE FREE FROM DEPRESSIONS, IRREGULARITIES AND NOTICEABLE CHANGES IN GRADE. GENERALLY,

APP DESCRIPTION

KD LANDSCAPE CONCEPT PLAN - L01

- GRADES SHALL DEVIATE IN LEVEL NO GREATER THAN 20mm IN ONE LINEAR METRE. 2.3 PLANTED AREAS SHALL BE SPREAD WITH MIN. 50mm OF APPROVED STANDARD SOIL CONDITIONER THAT SHALL BE RIPPED
- INTO EXISTING SOIL TO A MIN. DEPTH OF 200mm.
- 2.4 RAISED PLANTER AREAS AND POTS SHALL BE INSTALLED WITH APPROPRIATE DRAINAGE CELL, AGGREGATE AND GEOTEXTILE MEMBRANE BELOW SOIL.
- 2.5 FILL SOIL TO RAISED PLANTER AREAS AND POTS TO BE APPROVED LIGHTWEIGHT LANDSCAPE MIX.
- 2.6 PLANTING AREA SOIL PROFILES TO BE PREPARED AS SPECIFIED AND REVIEWED BY SUPERINTENDENT BEFORE PLANTING
- 2.7 ALL SITE AND IMPORTED SOILS, POTTING MIX, SOIL CONDITIONERS AND MULCHES TO BE IN ACCORDANCE TO RELEVANT AUSTRALIAN STANDARDS. 3.PLANTING
- 3.1 PLANTED AREAS SHALL BE MULCHED WITH AN ORGANIC MULCH UNLESS OTHERWISE STATED TO A MINIMUM DEPTH OF
- 3.2 ADVANCED TREES SHALL BE STAKED W/ 50x50mm DIA HARDWOOD POSTS. POSTS SHALL BE PAINTED BLACK AND INSTALLED TO A MIN DEPTH OF 500mm. TREES SHALL BE SECURED TO POLES W/ RUBBER TIES IN FIGURE 8.
- 3.3 TREES PLANTED WITH IN 1000mm OF BOUNDARY WALLS AND/OR PARKING AREAS SHALL BE INSTALLED WITHIN 600mm DEPTH NYLEX ROOT BARRIER MEMBRANE. MEMBRANE SHALL BE INSTALLED AS PER MANUFACTURERS RECOMMENDATIONS. 3.4 FINAL PLANTING PLAN SHALL BE SELECTED FROM PLANTING PALETTE - REFER PAGE 101.
- 3.5 PLANTS TO BE SET OUT IN EVEN SPACING TO FILL THE DESIGNATED AREAS. 3.6 IN AREAS OF MIXED PLANTING, SPECIES TO BE SPREAD OUT AT RANDOM, IN GROUPINGS OF 2 OR 3.
- 3.7 PLANTS SHALL BE SUPPLIED FROM AN INDUSTRY ACCREDITED WHOLESALE NURSERY. PLANTS SHALL BE IN APPROPRIATE SIZE FOR THE LISTED POT SIZE AND IN GOOD HEALTH. 3.8 IF SPECIES ARE UNAVAILABLE (OR IN SIZES SPECIFIED), SUBSTITUTES MUST BE APPROVED BY SUPERINTENDENT BEFORE
- 3.9 SUPERINTENDENT TO REVIEW SAMPLES OF ALL TREE SPECIES AND PLANTS AT SOURCE OR BY PHOTOGRAPH PRIOR TO
- DELIVERY AND INSTALLATION. 4. IRRIGATION
- 4.1 PLANTING TO GROUND LEVEL TO BE IRRIGATED VIA A FULLY AUTOMATIC SYSTEM FROM MAINS. 4.2 WATER PRESSURE TO HAVE A MINIMUM FLOW RATE OF 30L/pm AT 300kPA FROM THE WATER CONNECTION POINT (OR
- 4.3 PLANTING ON ALL 'ON SLAB' AND UPPER LEVELS TO BE IRRIGATED VIA BATTERY OPERATED VALVE (CONNECTION POINTS TO BE PROVIDED TO EACH LEVEL).
- 4.4 CONTROLLER TO BE LOCATED IN SERVICE ROOM (OR AS SHOWN ON IRRIGATION DETAILS TO FUTURE DETAIL). 4.5 SLEEVES BENEATH PAVED SURFACES AND TO RAISED PLANTING AREAS TO BE PROVIDED BY OTHERS.
- 4.6 IRRIGATION TO GARDEN BEDS TO BE NETAFIM TECHLINE, SUB SURFACE IRRIGATION. INSTALLED TO MANUFACTURERS SPECIFICATION. IRRIGATION TO TURF TO BE POP UP SPRINKLERS; MP ROTATORS OR SIMILAR. IRRIGATION TO TREES TO BE BE BUBBLERS; TORO FLOOD BUBBLERS OR SIMILAR.
- 4.7 ASCON DRAWINGS, MANUALS AND 12 MONTH WARRANTY SHALL BE SUPPLIED BY THE IRRIGATION CONTRACTOR TO THE CLIENT UPON PRACTICAL COMPLETION.
- 4.8 PLEASE REFER TO IRRIGATION DRAWING SET FOR FINAL LAYOUT AND SCHEDULE TO FUTURE DETAIL.

PLANTING IMAGES

















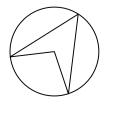
DEVELOPMENT APPROVAL

PAGE 102

Innaloo WA 6018 mob: 0450 965 569 email: kelsie@kdla.com.au JOB No. 0182

REV A

SCALE 1:200 @A1

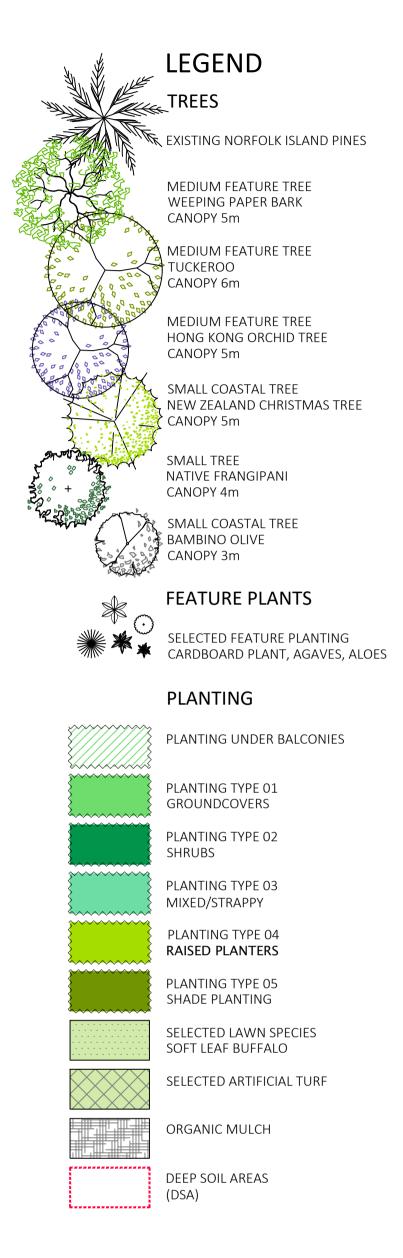




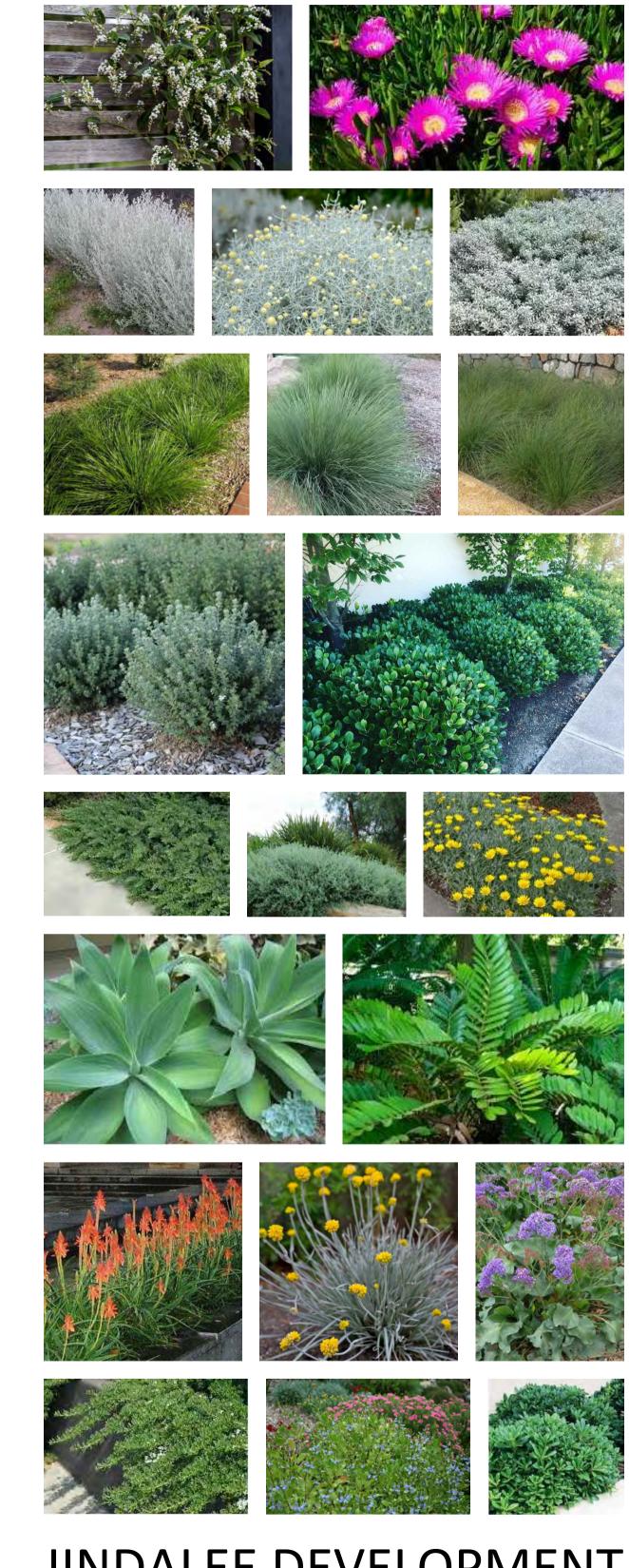
JINDALEE DEVELOPMENT LANDSCAPE CONCEPT PLAN - LEVEL 01

LINIC PROPERTY GROUP LOT 1362 MARITIME DRIVE, JINDALEE





REFER PAGE 101 FOR PLANTING PALETTE AND PLANTING CALCULATIONS REFER PAGE 102 FOR PLANTING NOTES



REV DATE

A 21.09.22

PLANTING IMAGES

DWN APP DESCRIPTION

KD KD LANDSCAPE CONCEPT PLAN - LO2

JINDALEE DEVELOPMENT LANDSCAPE CONCEPT PLAN - LEVEL 02

kelsie davies landscape architecture

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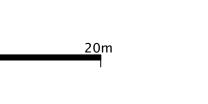
JOB No. 0182

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DEVELOPMENT APPROVAL

REV A

SCALE 1:200 @A1





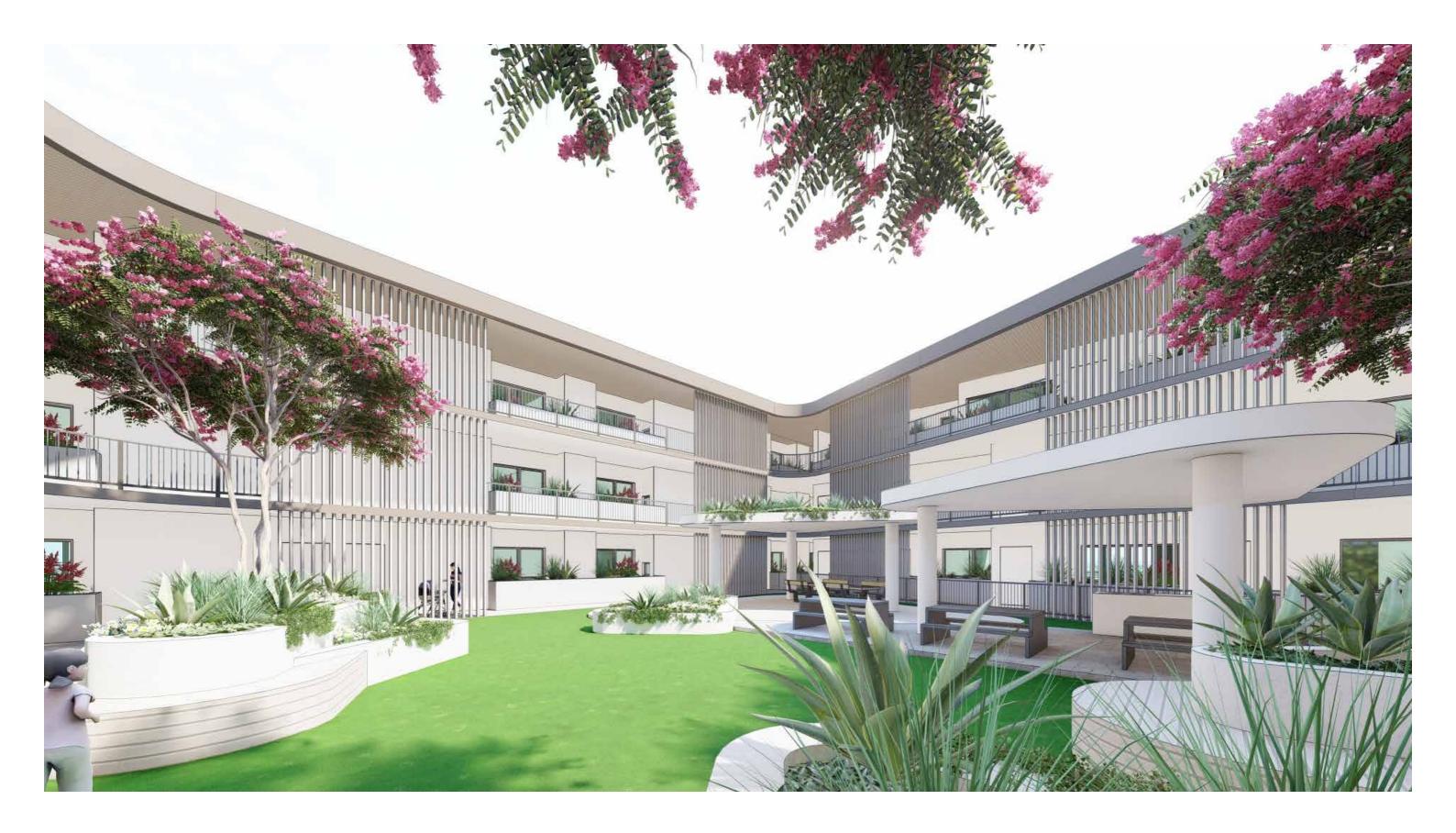














No	Position	Summary of Submission	Administration Comment	Recommendation
1	Ohiect	Submitter 1	Comment	
1	Object	Subject development is significantly larger than originally advertised. There will be a reduction in amenity for local home owners with traffic increase from this significant number of units Note only 6 visitor bays with the advice that the public car park can be utilised, this car park is mostly full during summer and will affect patronage of local business Why should we suddenly have another 370 cars on Jindalee blvd which is barely maintained by council Proposed restaurant appears to be facing maritime drive rather than the ocean, hardly optimal for the surrounding homes and not attractive position for those using the restaurant Badly planned and significant traffic and parking concerns, too large for the residential area	The proposed development is located within a structure plan area that allows up to a five storey development, whilst the proposal will present as three stotey development. Tthe applicant has provided a Traffic Impact Statement (TIS) which confirms that the increase in traffic will have a minimal impact on the surrounding road network. The restaurant location is deemed appropriate as it will assist in providing increased shelter for the restaurant's alfresco area from prevailing sea breezes. The proposal provides for an overall surplus in parking which will serve to an overall reduced demand for utilisation of public parking in the area.	No modifications required
2	Object	Submitter 2		
3	Object	There is already an apart block on the front of maritime drive which the front of is not maintained also behind the beach shack there are more dwellings and a microbrewery/restaurant getting built so to have 3 apartments blocks and 3 food outlets is not viable.	Under the structure plan (ASP 78) the proposed land uses are all allowable: Restaurant (P), multiple Dwelling (D), Holiday Accommodation (D). The retail floor space is limited to 200m2 each for individual tenancies (147m2 provided) and 500m2 overall for the Coastal Village precinct	No modifications required
-'	,			

4	Object	This proposal offers nothing for the community, as we should have more commercial opportunities for potential shops/restaurants to take advantage of a fantastic coastal location, which are now becoming very scarce, we shouldn't be having a 3 storey monstrosity proposed directly in front of all the established dwellings in the area that have paid big dollars to the developer, who I might add have done nothing to the area in the way of maintenance. Also if this is only a proposal, why are the earth works underway, when the community is only getting asked for an opinion now?	Regarding proposed land uses and height, refer Items 1 and 2 above. Earthworks are commenced to fulfill conditions of subdivision.	No modifications required
4	Object	Just letting you know that we	Refer Item 2 above	No modifications
		are very disappointed as when we purchased are land in		required
		Jindalee we were promised Restaurants /Cafes/Bars.		
		Seeing that the block was		
		zoned as commercial we were		
		hoping for more Commercial area other		
		than that was allocated in the		
		plans. I can reassure you that		
		most residence in Jindalee will		
		not be happy with the		
		proposed plans, as for years		
		now we have all been waiting		
5	Object	for what we were promised. Submitter 5		
	Object	We bought in the area with	Refer Item 2 above	No modifications
		the view that more	Neier item 2 above	required
		commercial / restaurants were		- 1- 2-
		to be built. Only space for one		
		comercial space has been put		
		forward. Very disappointing.		
	Obt. :	What a waste opportunity		
6	Object	Submitter 6		A. 1161
		We bought into the area that we were under the	Refer Item 2 above	No modifications
		assumption that there would		required
		be more commercial		
<u> </u>		De more commercial		1

			T	<u> </u>
		properties, restaurants/ bars.		
		This will be a missed		
		opportunity if these flats gets		
		the go-ahead.		
7	Object	Submitter 7		
		Application does not	Refer Item 2 above	No modifications
		incorporate an adequate		required
		amount of commercial vs		
		dwelling / accommodation.		
		One restaurant is not		
		acceptable. Parking overflow		
		for the new building into the		
		current carparking next to the		
		beach shack will impact		
		tourists visiting the beach and		
		the beach shack. Currently		
		overflow parking is on the		
		vacant blocks and dead hills of		
		jindalee, this will become an		
		issue for residents towards the		
		bottom of jindalee near the		
		beach. As groundworks		
		commenced a couple of		
		months ago, it looks like the		
		decision/approval has already		
		been made in the background,		
		so assume all residents		
		feedback will be ignored		
		anyway.		
8	Object	Submitter 8		
		My concern is the amount of	Refer Item 1 above	No modifications
		parking, the current car parks		required
		are not adequate during		
		summer already, parking		
		overflow will become a		
_		problem and an eyesore.		
9	Object	Submitter 9		
		The proposed plans does not	Refer Item 2 above	No modifications
		adequately meet the "retail/		required
		commercial" and		
		"community" objectives.		
		Looking at what I was lead to		
		believe by the original land		
		developer, I believe that I was		
		misrepresented what this land was to be turned into. I was		
		expecting there to be an		
		indoor venue to have food/		
		coffee. There is nothing in the proposal that will enhance the		
		local community. Had I been		
1		iocai community. Had i been		

	T			
		made aware that so many		
		short term let's (Air B&B		
		properties) would be created		
		at the beachfront (and		
		therefore full of residents who		
		have no long term interest in		
		the quality of the local		
		environment) then I wouldn't		
		have brought in Jindalee		
		beach. I can see that works		
		have already started on the		
		land, which says to me that		
		local community opinion is		
		irrelevant in what happens on		
		that land; I therefore have no		
		choice but to seek legal action		
		regarding the		
		misrepresentation which I		
		have been subject to when I		
		purchased my property in		
		Jindalee beach. The proposed		
		development is clearly an		
		obvious example of a		
		developer squeezing as many		
		properties onto the land as		
		they think they can get away		
		with, with no thought on what		
		can also be done to enhance		
		everyone's experience of the		
		area. I don't expect taking		
		legal action will be too difficult		
		as the misrepresentation		
		couldn't be more obvious.		
10	Comment	Submitter 10		
		The first thing I would	Refer Items 1, 2 and 3	No modifications
		comment on is that the	above	required
		preparation of the		'
		development site has already		
		commenced, so clearly it is a		
		fait accompli. Whilst multi		
		storey mixed use properties		
		are a given going forward, one		
		could question the choice of		
		site for such a development		
		on issues such as increased		
		traffic using Jindalee		
		Drive & Maritime Drive; the		
		need for additional car parking		
		if there is too be a restaurant,		
		as there isn't sufficient		
		currently to accomodate the		
	ı	,	1	

		Beach Shack on a good day; Questions about what is planned across the road going east up the side of Jindalee		
		Drive?; Despite the councils dismissing property values out		
		of hand, I am sure some		
		residents around the development who invested a		
		fair amount when choosing to		
		build in the area might feel		
		gipped; Presumably the		
		property will be a strata titled		
		property, what safe guards have the council implemented		
		to ensure it doesn't		
		deteriorate over time. Has the		
		council additional properties		
		in the area that they have ear		
44	Obtain	marked for such development.		
11	Object	Submitter 11	Defeathers 1 and 2	No modifications
		My understanding was that this was to be commercial	Refer Items 1 and 2 above	No modifications required
		shops and a cafe/ restaurant.	above	required
		Jindalee Beach is a small,		
		upmarket part of Jindalee,		
		beautifully laid out and		
		landscaped, this development		
		will be an eyesore, detracting from the beautiful views. I fail		
		to see how this will benefit the		
		residents of Jindalee. Holiday		
		homes and small apartments		
		are not in keeping with the		
		demographic of Jindalee. The		
		residents are proud homeowners who have paid a		
		premium to live in this area		
		and this would change the feel		
		of the suburb, which would		
		be heartbreaking.		
12	Object	Submitter 12	Defendance 2 alance	Nie was differentiews
		I have lived in Jindalee since 2010 and now have started	Refer Item 2 above	No modifications required
		our 2nd build in the area, we		. equil cu
		wanted to stay in the area to		
		enjoy the promised		
		commercial development the		
		land developer advised us would happen we are		
		disappointed that only one		

		commercial space is now		
		planned which is directly the		
		opposite to what we		
		were advised. We would like		
		more commercial space to		
		offer varied choice, create job		
		opportunities and reduce		
		traffic as the whole suburb can		
		reasonably walk to the		
		planned development.		
		Jindalee Beach shack offer		
		great but limited facilities and		
		the popularity of the venue,		
		despite appalling amenities,		
		can only encourage businesses		
		that our suburb want and will		
		use multiple recreational		
		facilities.		
13	Object	Submitter 13		
		I do not approve this project	Refer Item 2 above. An	No modifications
		due to the lack of commercial	acoustic report was	required
		space. We were told by the	submitted with	
		developers that this area	application which	
		would become like 'Cottesloe'	outlines that Noise	
		with Resturants and cafes.	received at	
		Please explain where these	neighbouring premises	
		are located? I can only see one	noise received at	
		space. This is unacceptable	neighbouring premises	
		and I will be ensuring all of my	will mianly be from	
		fellow community members	mechanical services.	
		are aware of this proposal. I'm	Should theapplication	
		also concerned for the impact	be approved, a	
		on the environment with	condition of approval	
		acoustics with that many	will be recommended	
		apartments. I'm not	that once all equipment	
		impressed.	to be used are known,	
			an assessment of noise	
			emissions will be	
			conducted to ensure	
			compliance.	
14	Object	Submitter 14		
		I have high concerns that this	Refer Items 1 and 2	No modifications
		development is not	above	required
		appropriately equiped with		
		enough parking space. It is a		
		shame to see such a		
		magnificent location be		
		consumed by money making		
		units. We need people to visit		
ı				
		our community. Put more		

				I
		Make Jindalee another Eden		
		beach. And Alkimos beach not		
		just another suburb. Take		
		advantage of what we have		
		not waste it on making		
		overseas investors more rich.		
15	Object	Submitter 15		
		We followed all the	Refer Items 1 and 2	No modifications
		regulations when building our	above	required
		house on Maritime Drive to fit		•
		in with the beach		
		surroundings, unlike the		
		propose mixed development.		
		The propose development		
		design looks like a inner-city		
		money grabing investment.		
		Seven years ago there was		
		suppose to be a age care		
		home on Maritime Drive,		
		instead there is 3 separate		
		identical doulbe storey houses		
		on one block of land, where		
		sometimes a family live. Give		
		back the ageing community a		
		cheap place to live on lot 9047		
16	Object	Submitter 16		
	Object	None of this is what the	Refer Items 1 and 2	No modifications
				NO IIIOGIIICALIOIIS
		recidents of iindalee were	ahove Traffic speed	required
		residents of jindalee were	above. Traffic speed	required
		promised when purchasing	control is not a planning	required
		promised when purchasing land. There is already	·	required
		promised when purchasing land. There is already insufficient parking in the area	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout.	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed bumps are not	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed bumps are not the solution and residents on	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed bumps are not the solution and residents on the boulevard need to be	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed bumps are not the solution and residents on the boulevard need to be asked opinion on suitable	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed bumps are not the solution and residents on the boulevard need to be asked opinion on suitable traffic calming chevrons. The	control is not a planning	required
		promised when purchasing land. There is already insufficient parking in the area proposed so six parking bays in addition for visitors will not be sufficient. Traffic down jindalee blvd is also too high with people driving at ridiculous speeds I. The council has done insufficient testing of this making sure you park the speed camera on the wrong side of the roundabout. It's beachside coming up from the beach shack roundabout that is the problem. Speed bumps are not the solution and residents on the boulevard need to be asked opinion on suitable	control is not a planning	required

		holiday let's. This will only		
		bring people to the area that		
		have no investment in the		
		area and won't care how their		
		actions affect the residents of		
		jindalee. We were promised a		
		shop front of commercial		
		premises. This should be the		
		minimum we get. We have		
		paid a lot to build on the		
		promises we have been		
		offered and should at least be		
		on par with		
		iluka or alkimos.		
17	Object	Submitter 17		
		I don't entirely object to a	Refer Items 1 and 2	No modifications
		multi purpose dwelling being	above	required
		build. However looking at the		
		plans this property is far to		
		big. It isn't in keeping with the		
		current neighbourhood and		
		would ruin the beautiful beach		
		front. If they took it down a		
		floor and they could get rid of		
		the restaurant it would look		
		much better. This isn't		
		Scarborough which is where		
		this current plans would be		
		suited.		
18	Object	Submitter 18		
		Can consideration be given for	Refer Items 1 and 2	No modifications
		further amenities such as a	above	required
		cafe, bar or additional		
		restaurant. The existing Beach		
		Shack is seasonal due to being		
		predominantly outdoor and		
		the single restaurant being		
		planned doesn't appear to		
		satisfy current community		
		trends for a beachside eatery.		
		The current planned single		
		restaurant is also facing away		
		from the ocean outlook		
		towards residential housing.		
		As a large proportion of the		
		planned accommodation is for		
		holiday apartment's,		
		consideration should be given		
		for more amenities		
1		I la a l'al account a la company de la compa		
19	Object	holiday makers would utilize Submitter 19		

 			5 6 11 1 1 1	11.61
		We feel that the dwelling	Refer Items 1 and 2	No modifications
		proposed would obstruct our	above. Obstruction of	required
		view from the ocean which	view and devaluation of	
		will effect the value of our	propertyy values are	
		property. Also, I don't think	not valid planning	
		the current infrastructure has	objections.	
		the capacity to cope with the	•	
		additional volume of traffic		
		and people. The serenity of		
		the area may be spoiled with		
		the 14 Holiday units.		
20	Object	Submitter 20		
	0.0,000	I believe with the number of	Refer Items 1 and 2	No modifications
		proposed residential dwellings	above. Anti-social	required
		will have a significant material	behaviour is not a valid	required
		impact to noise levels for the	planning conideration.	
		current residence in this area,		
		particular with the provision		
		of short term holiday lets in		
		the vicinity, these people will		
		have no personal invested		
		reasons to keep the noise		
		down and obviously being in		
		on holiday would want to		
		party in groups. We currently		
		experience noise issues with		
		people using the car park late		
		at night and playing music in		
		their vehicles until 1 am in the		
		morning, the sound		
		significantly travels up to our		
		residence, which means we		
		permanently have to keep our		
		bedroom windows shut at		
		night, instead of enjoying the		
		sea breeze. I believe the		
		number of residential		
		properties should be reduced		
		and no holiday		
		accommodation approved.		
		Furthermore I do not		
		understand why the proposed		
		cafe would face into the		
		direction of the roundabout,		
		completely missing the ocean		
		views that would attract		
		people to the venue during		
		1		
		the day, that roundabout is		
		dangerous at the best of times		
21	Ohiost	with people speeding around. Submitter 21		
21	Object	Submitter 21		

		My husband and I have purchased a retirement house in Jindalee and are against this type of dwelling being built and the effect it will have in our future suburb where we will retire.	Refer Item 2 above. An acoustic report was submitted with application which outlines thatNoise received at neighbouring premises noise received at neighbouring premises will mianly be from mechanical services. Should theapplication be approved, a condition of approval will be recommended that once all equipment to be used are known, an assessment of noise emissions will be conducted to ensure compliance.	No modifications required
22	Comment	Submitter 22	compilance.	
		As a resident of Jindalee Beachside estate for over 5 years, it is very pleasing to finally have some development near the Beach café. The developer at the time, when we purchased told us it was destined to be a busy hub with a mix of commercial and residential including restaurants. I am extremely dissapointed to look over these plans to see there is a tiny (98m) commercial facility in this huge complex. Where are all the people staying in these short stay apartments meant to easily eat without having to travel away from this precinct? I would like to see a significantly larger commercial allocation to this project to allow for a decent restaurant that Jindalee has been needing on the beach for a long time to fill the gap that the Beach Café doesn't fill	Refer Item 2 above. An acoustic report was submitted with application which outlines thatNoise received at neighbouring premises noise received at neighbouring premises will mianly be from mechanical services. Should theapplication be approved, a condition of approval will be recommended that once all equipment to be used are known, an assessment of noise emissions will be conducted to ensure compliance.	No modifications required

		,	<u></u>	,
		with its adhoc and seasonal		
		opening hours.		
23	Object	Submitter 23		
	Object		Defer Home 1 10 and 20	No modifications
		I moved to Jindalee 3 years	Refer Items 1, 19 and 20	
		ago for the quiet, peaceful	above	required
		lifestyle Jindalee offers. I also		
		purchased my home for the		
		views and this complex will		
		affect my views that I pay a lot		
		of money to enjoy. Not to		
		mention if this proposal is		
		appoved, the additional traffic		
		flow on Santa Barbara Pde &		
		Jindalee Blvd. We already		
		have major issue with traffic		
		_ ·		
		noise/noise pollution and to		
		add additional, unnecessary		
		population/noise polution to		
		the area will affect my quality		
		of peace and enjoyment in my		
		home. Allowing holiday		
		makers to come to		
		our suburb, not respecting our		
		private residents and again,		
		creating additional		
		noise/traffic to our local		
		residents. I will be devastated		
		if this is approved.		
24	Commont	Submitter 24		
24	Comment			
		As a resident of Jindalee	Refer Item 2 above	No modifications
		Beachside estate for over 5		required
		years, it is very pleasing to		
		finally have some		
		development near the Beach		
		café. The developer at the		
		time, when we purchased told		
		us it was		
		destined to be a busy hub with		
		a mix of commercial and		
		residential including		
		restaurants.		
		I am extremely dissapointed		
		to look over these plans to see		
		there is a tiny (98m)		
		commercial facility in this		

30	Object	the area Submitter 30		
		I will also bring filore crime to		
		disruption plus blocking view. Will also bring more crime to	above	required
		Way to much noise and traffic	Refer Items 19 and 20	No modifications
29	Object	Submitter 29	D. C. II. 10. 100	A1 1:5:
		the dog beach north.		
		consideration for extending		
		the dog beach. Request		
		increase in potential issues on	this application	
		enough commercial growth in the area. Concerned about the	Issues regarding the dog beach is not related to	required
		Too much residential, not	Refer Item 2 above.	No modifications
28	Comment	Submitter 28		
		children and housing		
		extra traffic to a road with		
		beautiful coastal life. Bring	above	required
	•	Not in keeping with our	Refer Items 1 and 2	No modifications
27	Object	Submitter 27		
		ahead	INOLEU	required
20	Jupport	Yes would love this to go	Noted	No modifications
26	Support	Submitter 26		
		surrounding area and businesses		
		has to be good for the		
		owned with close amenities		
		holiday, rental or home		
		accommodation whether it be		
		brings modern		required
		Any new infrastructure that	Noted	No modifications
25	Support	Submitter 25		
		opening hours.		
		with its adhoc and seasonal		
		long time to fill the gap that the Beach Café doesn't fill		
		needing on the beach for a		
		that Jindalee has been		
		allow for a decent restaurant		
		allocation to this project to		
		significantly larger commercial		
		precinct? I would like to see a		
		travel away from this		
		to easily eat without having to		
		short stay apartments meant		
		the people staying in these		

	1	T-1		1161
		There will be too many people	Refer Item 1 and 20	No modifications
		crammed in. There are also	above	required
		new developments ongoing		
		close by - Jindee etc.		
		Infrastructure, especially the		
		roads won't cope. Noise will		
		be an issue - with holiday		
		rentals & people behaving		
		badly as on holiday drinking &		
		partying etc Traffic is		
		horrendous as it is. The beach		
		car parks		
		can't cope with local traffic as		
		it is. Not enough bays or		
		space. Cars speed down Santa		
		Barbara & Ocean Drive as it is		
		- this will only get worse. No		
31	Object	thank you to this proposal. Submitter 31		
31	Object		Refer Items 2 and 20	No modifications
		It's a residential area - holiday let's etc cause noise unwanted	above	
		behaviour	above	required
32	Support	Submitter 32		
32	Support		Noted	No modifications
		The current sand pit is a bit of	Noted	
33	Support	an eyesore. Submitter 33		required
33	Support		Noted	No modifications
		Great!	Noted	required
34	Object	Submitter 34		required
	•	We live in a affluent /	Refer Item 19 and 20	No modifications
		premium sleepy area with	above	required
		high cost housing and rates		
		this will only cheapen the		
		suburb with holiday units let		
		alone the increased traffic and		
		noise pollution . Alongwith		
		extra litter, loiterers and not		
		enough parking for the extra		
		influx of people.		
35	Object	Submitter 35		
33	Object			
33	Object	The public car park that will be	Refer Item 1 above	No modifications
33	- Caject	used by visitors to the	Refer Item 1 above	No modifications required
		used by visitors to the complex (and by residents	Refer Item 1 above	
	- Carjeet	used by visitors to the complex (and by residents who own more cars than	Refer Item 1 above	
33	- Carjeet	used by visitors to the complex (and by residents who own more cars than available bays for a unit) is	Refer Item 1 above	
	Capeer	used by visitors to the complex (and by residents who own more cars than available bays for a unit) is often full already on	Refer Item 1 above	
	- Carjeet	used by visitors to the complex (and by residents who own more cars than available bays for a unit) is often full already on weekends and people	Refer Item 1 above	
	- Carjeet	used by visitors to the complex (and by residents who own more cars than available bays for a unit) is often full already on weekends and people currently park on the road	Refer Item 1 above	
33	Capeer	used by visitors to the complex (and by residents who own more cars than available bays for a unit) is often full already on weekends and people	Refer Item 1 above	

exacerbate the current lack of parking situation. Additional commercial spaces would be beneficial for the area (bakery, deli/convenience store) 36 Support Submitter 36 Noted No modification required
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deli/convenience store) 36 Support Submitter 36 Noted No modification
36 Support Submitter 36 Noted No modification
Noted No modification
required
37 Object Submitter 37
The focus for development Refer Item 2 above No modification
should be for permanent required
housing, not short stay
accommodation for tourism.
There's clearly a shortage of
housing in Perth and
surrounds, so clear and vacant
land should be utilised for new
housing projects to relieve the
burden on the market, not
multi unit dwellings such as
this proposal. Furthermore,
adding 47 accommodations
(2.5pph) only increases the
saturation of the surrounding
community amenities, which
are utilised not by Jindalee
residents only but outer
reaching suburbs
such as Butler, Merriwa and Quinn's. If additional
accommodation is to be
developed in Jindalee, an
(accurate) assessment of the
available amenities needs to
be undertaken prior.
38 Object Submitter 38
Total over development of the Refer Items 1, 2 and 19 No modification
site in an area of single family above required
occupancy residences. Existing
parking facilities are already
stretched and insufficient
additional parking will only
render the existing facilities
unuseable for existing
residents and rate payers.
Units should have a minimum
of 2 parking spaces plus
visitors spaces. If these cannot
be provided then the site is
being overdeveloped in my
opinion. There are no other 3

		really wasted typing. Money, money, money!!!!		
		really wasted typing Money		
	•	as work has already begun so		
		you are asking residents now		
		monstrosity's. No idea why		
		Byron Bay. NOT triple story		
		like they have in Noosa and		
		restaurants and funky shops		
		costal suburb that needs cosy	above	required
		Jindalee beach front is a quiet	Refer Items 1 and 3	No modifications
42	Object	Submitter 42		
		and usage.		
		population		
		an issue with the current		
		Traffic and parking is already		
		feel of the foreshore area.		
		will affect the ambience and		
		storey and holiday dwellings, and without adequate parking,		required
		Addition of this number of 3	Refer Item 1 above	No modifications
41	Object	Submitter 41		
• •		schemes such as this proposal.		
		populated with dense housing		
		restaurants and bars, not over		
		investment in quality		
		proposal. The area needs		
		outlets which is lacking in this		
		businesses i.e. food type		
		to comprise of more		
		this submission relates to was		
		understanding that the area		required
		We purchased our property in the area of Jindalee on the	Refer Item 2 above	No modifications
40	Object		Pofor Itom 2 shave	No modifications
40	Ohioct	buildings Submitter 40		
		shops rather than high		
		single story restaurants and	above	required
		Would much prefer to see	Refer Items 1 and 2	No modifications
39	Object	Submitter 39	D. C. 11	Al life ii
20	Oh:+	on investment. No thank you		
		totally on maximising return		
		area , the application is based		
		area. No consideration for the		
		totally out of character for the		
		payers with a building that is		
		the views of existing rate		
		is there really a need to block		
		storey buildings in the vicinity,		

No modifications required No modifications required			This is going to lower the	Refer Items 19 and 20	No modifications
have worked hard to get. Also allows for unsavoury people into Jindalee plus destroys the look of our coastline 44 Object Submitter 44 Lan't believe city of Wanneroo even considering this project which will affect the whole area! bought the land to live in a quite peaceful residential place and now you are considering to change it to Commercial place IOBJECT THIS PLAN. Three stories house blocking the view for everyone else in the area. and changing ithe area to industrial place, 2. NO APARTMENTS , NO SHOPPING MALL, NO GYM, NO RESTURANT, Don't change the area to industrial area 45 Support Submitter 45 Another restaurant will be good for the area 46 Comment Submitter 46 1. You should have a restaurant and a cafe facing West for people to enjoy cafe food (like Dome) or good quality restaurant food with ocean views (like Portofino's). It would be a special place. 2. If the above is not acceptable then the cafe you have in your plans is too small in a rea. 3. It would also be advantageous to the people living in the complex and from the surrounding area if you had a small continental deli similar to Basil's in Mindarie. It doesn't have to be as big but unique like Basil's. 47 Object Submitter 47 Development should be limited to two storey. Ground and 1st level. View			This is going to lower the		
allows for unsavoury people into Jindalee plus destroys the look of our coastline look of our coastline and the look of our coastline can't believe city of Wanneroo even considering this project which will affect the whole area. I bought the land to live in a quite peaceful residential place and now you are considering to change it to Commercial place. I OBJECT THIS PLAN. Three stories house blocking the view for everyone else in the area and changing ithe area to industrial place, 2NO APARTMENTS ,NO SHOPPING MALL,NO GYM,NO RESTURANT, Don't change the area to industrial area area area to industrial area area area area area area area ar				above	required
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obstructions More restaurant	47	Object	West for people to enjoy cafe food (like Dome) or good quality restaurant food with ocean views (like Portofino's). It would be a special place. 2. If the above is not acceptable then the cafe you have in your plans is too small in area. 3. It would also be advantageous to the people living in the complex and from the surrounding area if you had a small continental deli similar to Basil's in Mindarie. It doesn't have to be as big but unique like Basil's. Submitter 47 Development should be limited to two storey. Ground	· ·	No modifications
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	Ι	and has autions. The CC		
		and bar options. Traffic		
		management plans to be in		
		place for the increase in		
		traffic.		
48	Object	Submitter 48		
		The area has not got the	Refer Items 1 and 2	No modifications
		amenities to support a	above	required
		development of this size, very		
		little playground facilities		
		nearby, lack of parking		
		available , summer weekends		
		cars are already parking on		
		the verge and grassed		
		area.poor access to the beach		
		and no disabled access to the		
		beach.This development is not		
		in keeping with the local area		
		which is all single houses on		
		blocks and not multi units. This		
		development has no additions		
		for local people. this		
		development has holiday		
		accommodation included and		
		probably a lot of		
		additional Air BnB which will		
		add nothing to the city as a		
		whole.		
49	Object	Submitter 49		
		The proposed development	Refer Items 1 and 2	No modifications
		will significantly increase the	above	required
		traffic flow within the suburb		'
		which is already extensive		
		with the beach access and		
		cafe already present. Traffic		
		already speeds down Jindalee		
		Boulevard - the main access		
		road - and there are no traffic		
		calming measures on this		
		road. There is not enough		
		parking for the current		
		facilities offered and the		
		overflow cars park on the		
		verges, roads		
		and driveways within the		
		suburb causing friction with		
		residents and damage to		
	I	property. This development		
		property. This development		
		1		
		does propose parking facilities for its residents however not		
		does propose parking facilities		
		does propose parking facilities for its residents however not		

	r			
		patrons etc that will then also		
		park on the roads and verges.		
		As a residential area with		
		playgrounds that are not		
		fenced in addition to the		
		beach access and BBQ areas		
		already there is a significant		
		risk of trauma to children and		
		older adults from the		
		increased traffic.		
50	Object	Submitter 50		
	-	Significant increase in traffic	Refer Items 1, 2 and 20	No modifications
		flow and insufficient parking	above	required
		to accomodate all the patrons	above	required
		to the facility. There is already		
		insufficient parking for the		
		beach and cafe currently there		
		•		
		and the overflow traffic parks		
		on the roads and verges		
		blocking accessways and		
		causing damage. As a		
		residential area there is no		
		requirement for a restaurant		
		particularly in such close		
		proximity to houses. There will		
		be significant disturbance		
		from noise and traffic during		
		unsocial hours.		
51	Object	Submitter 51		
		We must ensure	Refer Items 1, 2 and 20	No modifications
		commercial/restaurant/shopfr	above	required
		ont development -		
		residential/to let allocation is		
		not 'developing the area'.		
		Also, why ruin the look of the		
		seafront with with a 3 storey		
		development? There are only		
		two storey buildings in		
		Jindalee and so the proposed		
		development should be		
		aligned within other buildings		
		in the area. Parking will also		
		be an issue with overflow		
		running out onto our streets		
		and a huge increase in the		
		flow of traffic with holiday		
			•	
		units. There are already		
		problems with speeding down		
		problems with speeding down Jindalee		
		problems with speeding down		

		measures in place, even after		
		the request from residents.		
52	Object	Submitter 52		
-	Object	This area needs commercial or	Refer Items 1 and 2	No modifications
		restaurant development , not	above	required
		a three storey residential/to		
		let. Three storey is not in		
		keeping with the beautiful		
		design of the area and will		
		stand out like a tacky sore		
		thumb. Whoever put in this		
		proposal is not thinking of		
		growing the area, only		
		thinking of growing their		
		pocket.We must ensure		
		commercial or restaurant		
		development, not residential		
		three storey properties! It's		
		not in keeping with the area		
		and will not encourage growth		
		of the area!		
53	Object	of the area! Submitter 53		
53	Object	Submitter 53 For years, the developer and	Refer Items 1, 2 and 19	No modifications
53	Object	Submitter 53	Refer Items 1, 2 and 19 above	No modifications required
53	Object	Submitter 53 For years, the developer and	·	
53	Object	Submitter 53 For years, the developer and local agents have advised that	·	
53	Object	For years, the developer and local agents have advised that there were plans for commercial and retail in that	·	
53	Object	For years, the developer and local agents have advised that there were plans for commercial and retail in that area, there has been nothing	·	
53	Object	For years, the developer and local agents have advised that there were plans for commercial and retail in that area, there has been nothing done in that regard, to have	·	
53	Object	Submitter 53 For years, the developer and local agents have advised that there were plans for commercial and retail in that area, there has been nothing done in that regard, to have only one retail space is very	·	
53	Object	For years, the developer and local agents have advised that there were plans for commercial and retail in that area, there has been nothing done in that regard, to have only one retail space is very poor planning. A lot of	·	
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53	Object	For years, the developer and local agents have advised that there were plans for commercial and retail in that area, there has been nothing done in that regard, to have only one retail space is very poor planning. A lot of residents have sold up or planning to sell up and move to areas such as Mindarie or Amberton beach or other areas where they have cafes and restaurants, I don't feel you will have long-term investors that will will Be buying in the area as there's no cafes, restaurants, or a local pub in the area where families can walk to and have	·	

		area with the plans submitted and it will bring down house values There's also only six allocated visitors bays for the holiday rentals. That's not enough parking Space as currently weekend Traffic is overflowing in the car park next to the fish shack into the surrounding streets you will		
		cause more traffic congestion, and		
		parking in side streets where		
	6	already no parking		
54	Support	Submitter 54	Natad	No modifications
		We were informed when we bought our block of land in	Noted	No modifications required
		2006 that there would be		. oquou
		beach side cafe strip type of		
		development. All we have got		
		so far is Chippies Cafe. Look		
		forward to seeing the		
		foreshore finally developed which will improve the suburb		
		and bring it up to Shore Haven		
		and Alkimos standards.		
55	Support	Submitter 55		
		Im all for growth in tbe area	Noted	No modifications
		theres obviously going to be a		required
		few karens who complain but		
		they need to remember there		
		house was once land someone		
		else complained about. The		
		future growth of the quinns		
		jindalee forefront is a bonus		
		and will give us localsone		
56	Comment	more place to go for food etc Submitter 56		
50	Comment		Natad vafavitava 2	No see difications
		I support the proposal however the building height	Noted - refer Item 2	No modifications required
		has a max limit of 5 storeys		required
		yet this plan is for 3 storeys.		
		Seems the opportunity is not		
		being fully utilized and		
		potential lost to get the most		
		out of it. Alternatively if 3		
		storey is the favoured		
		approach has any		
		consideration been made to		
1	ĺ	possibly providing a rooftop		

	ı		Т	<u> </u>
		space for		
		residents, or public to enjoy.		
	6	6.1		
57	Support	Submitter 57		
		There is limited holiday	Noted	No modifications
		accommodation north of		required
		Mindarie at present		
58	Object	Submitter 58		
		Lack of visitor bays- the	Refer Item 1 above	No modifications
		existing public parking is		required
		insufficient for current		
		amount of traffic which		
		utilises the beachfront and		
		cafe with on street and often		
		pavement parking being a		
		frequent concern along		
		Maritime Drive / Sextant Way/		
		Jindalee Boulevard. The		
		proposed addition of		
		apartments and a cafe/		
		restaurant will only increase		
		the number of vehicles		
		needing to park on		
		road/pavement, potentially		
		increasing risks of accidents		
		and vulnerable people being		
		required to walk on the road		
		due to cars being parked on		
		pavements.		
59	Object	Submitter 59		
		The City of Wanneroo has a	Refer Items 1, 2 and 19	No modifications
		duty to existing residents of	above	required
		the area to not be adversely		
		impacted by increased traffic		
		and parking issues as well as		
		drop in property values due to		
		existing views being blocked.		
		A 2 storey development right		
		in the coast is a much better		
		compromise and still allows		
		for creating the "village		
		centre" without devaluing		
		surrounding homes		
60	Object	Submitter 60		
	,	Consideration of existing	Refer Items 1 and 2	No modifications
		residents, the proposal on	above	required
		buying our property was two	above	required
		storey property to be built.		
		Traffic & parking is a problem		

	1			T
		now, not enough visiting		
		parking spaces. More		
		commercial was promised in		
		original sales of area. No		
		holiday accommodation		
		please as can be rented by		
		wrong people. To large		
		building aesthetically for		
		position. More consideration		
		over 55's living should be		
		applied in the area.		
61	Support	Submitter 61		
		Looks amazing what a great	Noted	No modifications
		asset!		required
62	Object	Submitter 62		required
-		The lot is zoned for real retail	Refer Items 1 and 2	No modifications
		space. This proposal is almost	above	required
		purely residential, with 47	above	required
		units of housing (probably		
		another 100 residents). The		
		-		
		proposal includes a cafe only, which is sometimes called a		
		"restaurant" but appears quite		
		small and mainly outdoor - not		
		even clear if there will be real		
		sit-down space or a full		
		kitchen with made-to-order		
		food. Jindalee already has a		
		cafe (Jindalee Beach Shack,		
		adjacent building), and will not		
		benefit at all from another in		
		the same spot. The lot zoned		
		for real retail space, which		
		should include a proper		
		restaurant (and ideally more).		
		This is particularly critical		
		given the additional ~100		
		residents the proposal would		
		bring into the area. Please		
		reject the proposal as		
		submitted, and ask the		
		developer to revise to include		
		more retail space that will		
		provide new/unique benefits		
		to the Jindalee community.		
		This should include a full		
		restaurant (not a cafe) at a		
		minimum, along with parking		
		available for patrons of that		
		retail space.		
63	Object	Submitter 63		
		1	I	I

	1	I		
		A three storey structure will	Refer Items 1, 3 and 19.	No modifications
		block the views that visitors to	Increase in safety risk to	required
		the area have from the park, I	local dog walkers,	
		also expect that it will block	cyclists and children	
		views from certain	that play in the area is	
		residencies. It will also	not a valid planning	
		generate a significant increase	consideration.	
	in traffic putting at risk the			
		local infrastructure, the roads		
		surrounding this development		
		will deteriorate much quicker.		
		It will also increase the risk to		
		local dog walkers, cyclists and		
		children that play in the area.		
		It will increase the need for		
		parking to service the		
		restaurant which will mean		
		the reclaiming of green areas		
		to accomodate additional		
	cars. This will be an eyesore			
on what is a beautiful and				
natural part of the coast line,				
also is this just a tick in the box				
	process by the council			
	because ground works have			
		already commenced, can you		
		please explain how that has		
		happened. I completely object		
		to this development being		
		built in this location.		
64	Support	Submitter 64		
			Noted	No modifications
				required
65	Object	Submitter 65		'
	-	Though I am not opposed to	Refer Items 1, 19 and 63	No modifications
		the village idea around the	,	required
		area, I was aware of it when I		
		brought my block of land. I am		
		concerned for the safety such		
		a large number of dwellings		
		and a restaurant will cause		
especially as there does not				
seem to be enough parking				
		allocated for what is to be		
		built. There is already parking		
		issues when the Beach Shack		
		is busy and cars are parked on		
		Maritime, which is not a very		
		wide		
		road. Children crossing		
		Maritime to play in the play		

	T			
		ground, running between cars		
		that are parked on a road that		
	is not wide enough is very			
		dangerous. On average each		
	living accommodation has 2			
		cars and		
		holiday homes may have 2-4		
		vehicles (depending on how		
		many families stay). Then with		
		the restaurant, there is		
		definitely not enough parking		
		bays. I would prefer if the		
		building was 2 storey (as per		
		the consistency of houses in		
		the suburb) so there isn't a		
		blight on the view of the		
		lovely shoreline (it's not		
		Scarborough). Then there		
		should be enough car bays.		
		Also the City of Wanneroo		
		have advised they are		
		concerned for the footprint of		
		building/driveways, etc to		
	garden space. I would think			
		the size of the footprint of this		
		building would be a		
		environmental concern for the		
		City. Though to me cars		
		parking on the small and not		
		very wide roads in what is		
		meant to be a family friendly		
		area, is of the greatest		
		concern. And the spoiling of		
		what should be lovely views of		
		the ocean and not a big		
		cement building. Thank you		
		for reading my concerns.		
66	Object	Submitter 66		
		I object to holiday dwellings	Refer Items 1, 2, 19 and	No modifications
		being built in what is meant to	20 above	required
		be a family friendly residential		·
		area. I have environmental		
		concerns for a 3 storey		
		building. It will be an eyesore		
		for people who have spent a		
		lot of money to enjoy the view		
		of the ocean and not of a large		
		building. I am also concerned		
		for the parking problems that		
		it will cause in the area.		
		People parking everywhere on		
	•	•		

		the street will not be safe. There is already issues in other coastal areas where large dwellings have been built and I can see the party noises and fights occurring in the street from people who hire the holiday units. I don't mind the cafe and or restaurant idea and of a small, lower dwelling but I don't believe the area is designed for something so large. The streets aren't designed for the amount of traffic and there is not enough carparking bays for what is suggested.		
67	Object	Firstly, it's not very stylish, pleasing to the eye. An eyesore along the coastline. Too large, 47 apartments and too tall at 3 storey's. Secondly, not enough visitor bays considering a cafe is on the ground floor, meaning the current small adjacent carpark will now have to service 2 cafes, beach goers and visitors to apartments as there are only 6 visitor bays, one of which is an acrod bay. Refer to traffic impact statement point 10. Site specific issues. "Due to close proximity of a public parking facility within the immediate vicinity of the subject, visitors can easily park at the public parking facility adjacent to the subject site during their visit. This is VERY untrue. Go on a summer weekend or friday evening and the current carpark is completely full and cars are already parking on verges opposite to the development! Traffic flow 5.2 appears incorrect to me as a resident of an adjoining street. I estimate 90% of traffic flow to	Refer Item 1. Waste bin collection will be as per the Waste Management Plan. A condition will be recommended, should the proposal be approved, that the landscape plan be modified to eflect the waste manegement plan. Poetential impact of windy conditions due to the coastal locality is noted, but cannot be resolved through alternative waste collection methods.	Landscape plan to be modified to reflect the location of the bin collection area as depicted on the waste management plan

the development will come from Jindalee blvd, not maritime drive as much. The waste management plan needs addressing. Having lived on Sextant way for over 10 years I know how windy it gets in winter. Our bins are blown over very often, including when full on stormy nights. So much so we know not to put out overnight due to strong gusty South westerlies. Even putting it out in the morning before work, bins are knocked over by the wind, full and especially when empty. My neighbours has even ended up at forcastle park! Blown up the street. The bins are located on the south side. 12 general, 6 recycling. This IS an issue. And will be wheeled out twice weekly on collection days by strata management or a private waste contractor. This likely will be an issue and impact surrounding residents and the environment when waste from 47 apartments and a cafe is not managed effectively in strong coastal conditions. Will strata management be picking up litter blown over the surrounding area in Jindalee?? Another issue I see is the landscape concept design image doesn't match the waste management plan image of where bins wil be placed for collection. The landscape concept design shows an area of shrubs and plants. So the bins are placed on the road then??? When knocked down they will also block traffic to the proposed future cul de sac. Trust me, these bins will ge knocked down by wind in winter. It is

		not suitable to have these bins	
		left out for pick up	
		unattended. An alternative	
		wast management plan needs	
		to be addressed. The biggest	
		issue I have is the visitor	
		parking affecting flow of traffic	
		and becoming an eyesore	
		parked on verges surrounding	
		the forshore. I do not want	
		visitors to the area to	
		encroach on my street which	
		is free of cars on the road	
		currently. I would like a	
		'residents only' sign errected	
		at the bottom on Sextant way	
		if this large development goes	
		ahead.	
68	Object	Submitter 68	
		Not wanting the extra surge of	 No modifications
		traffic in the area, past my	required
		street and the sea of cars	
		down at the beachfront,	
		spoiling the view (as well as	
		added noise pollution). This	
		proposal was not what was	
		promised when we originally	
		bought our land- it is too	
		commercial looking. A nice	
		cafe and/or restaurant would have been more ideal.	
69	Comment	Submitter 69	
		I SUDMINUEL DE	

		I welcome the addition of the	Refer Item 1 and 20.	No modifications
		development to activate what	Design Review Panel	required
		is currently dead space,	confirmend appropriate	•
		however, given the "prime"	selection of muted	
		nature of the real estate with	colours and textures	
		direct ocean views, I would	responding well to the	
		hope that the external finishes	coastal setting and	
		are of a sufficiently high	aesthetic with AC	
		standard which are	condencers	
		representative of a	appropriately	
		development of this nature.	concealed.	
		Concealment of external		
		building services such as Air		
		Conditioning and Air Handling		
		Units would be preferred as		
		these can be unsightly and		
		affect aesthetics, obtrusive on		
		immediate neighbours and		
		difficult maintain.		
		Consideration should be had		
		to the classification and		
		specification of ALL materials		
		to be used as the impacts of		
		coastal erosion is prevalent - I		
		have first hand experience of		
		this and this affects everything		
		from the nails to the paint		
		systems used. In my opinion,		
		the "development" of		
		apartments immediately to		
		the north of this development		
		on the corner of Maritime		
		Drive / Seagull Vista is of poor		
		external quality in the finishes		
		used and it would be a		
		disappointment if this		
		development delivered a		
		similar aesthetic outcome.		
		Consideration should be given		
		to traffic calming measures on		
		Maritime Drive as this has		
		become somewhat of a "rat		
		race" of late, and anything		
		that can be done to		
		discourage the current band		
70	Compression	of "hoons" would be welcome.		
70	Comment	Submitter 70		

Being a local resident who uses the Cafe on a regular basis, I would wish to ask if a parking management plan has been carried out. We are regularly struggling to find a public parking space at the weekend, when the cafe and its public amenities are in use. How can this new development be placed in such a public place and then have to rely on the current public parking to support it, when there is not enough parking currently for the local community to use. Cars parked on grass verges is very common at the weekend. With a Cafe/Restaurant also being proposed, there would be a conflict of potentially available parking if both the existing cafe and proposed cafe facilities are required to rely on the current lack of public parking bays currently at the busy times of the day during the weekends, breakfast mornings, lunches and for evenings/events.			
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With a Cafe/Restaurant also being proposed, there would be a conflict of potentially available parking if both the existing cafe and proposed cafe facilities are required to rely on the current lack of public parking bays currently at the busy times of the day during the weekends, breakfast mornings, lunches and for	parked on grass verges is very		
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at the busy times of the day during the weekends, breakfast mornings, lunches and for	•		
during the weekends, breakfast mornings, lunches and for			
breakfast mornings, lunches and for			
and for			
	<u> </u>		
evenings/events.	and for		
	evenings/events.		

SPP 7.0: A summary of the DRP's recommendations and the applicant's responses with the City's comments

<u>Principle</u>	DRP Recommendation	Applicant's Response	City's Comments
1 - Context and	Pending Further	The amended design	Modifications are
Caracter	Information:	re-instates direct	deemed to
	Consider	pedestrian access to	adequately address
	incorporating stair	the ground floor short	DRP
	modules to allow for	stay accommodation	recommendations
	the ground floor units at Jindalee	units. Further, the proposal	
	Boulevard to access	has reduced a number	
	the public domain.	of parking bays to the	
	Each unit may	main	
	incorporate a stair,	building entrance to	
	or a stair might be	enable the introduction	
	shared between two	of landscape, and a	
	adjoining units.	canopy	
	Consider design	over to assist with a more walkable and	
	solutions that project	more walkable and engaging design, in	
	the main building entry forwards to	keeping with	
	become a more	the walkable character	
	prominent and	of the locality.	
	visually legible	-	
	element and one		
	which engages with		
	its context more		
2 - Landscape	deliberately. Pending Further	The comments of the	Although it would be
Quality	Information:	DRP have been	Administration's
Quality	Re-consider the use	critically reviewed by	preference if real
	of artificial turf in the	KDLA, and the	turf could be used,
	central courtyard	use of real turf has	the advice provided
	and substitute with	been considered over	by KDLA is noted
	real lawn and/or low	artificial turf.	and there is no
	coastal shrub.	The advice received from KDLA is that real	reason to question or dispute the
	 Consider replacing one planter box 	turf will struggle to	conclusions arrived
	within the central	establish	at by the landscape
	courtyard with a	and be maintained due	architect which
	large tree.	to high traffic, and the	suggest that, due to
	Modifications may	tall deep shadows cast	the reasons
	need to be made to	by	outlined, it will be
	the carpark to	the building.	impractical to use
	accommodate the	The subject site is also surrounded by a	real turf. It is also recognised that
	DSA	number of turfed local	artificial turf, if used,
		open	can at any stage in
		space parklands,	future be replaced
		hence there is no need	by natural turf if
		to double up on this	required. On that
		provision.	basis, the use of
		Finally, the provision of	artificial turf can be
		real turf, we are advised will add to the	supported.
		slab	
		thickness below the	
		turfed area, adding	

3 - Built Form and Scale	Supported: • No further recommendations	further cost to the development, to deliver natural turf that is considered likely to fail. The internal court yard has been developed to reduce the amount of artificial turf, adding further planter beds, and enlarging the existing ones, adding in further timber decking. The design proposes 3 storeys, within a locality that enables 5 storeys, meaning the proposal sits comfortably within the bulk	Administration concurs
4 - Functionality and Build Quality	Supported: Consider how bins will be delivered to the collection point.	afforded to the site. The modified Waste Management Plan (WMP) provides optional specification of 240L bins if bin weight becomes an issue, with collection by private contractors. Bin store area is suitably sized to accommodate this option.	It should be noted that under the WALGA Multiple Dwelling Waste Management Plan Guidelines, for bins up to and including 360L the bin transfer grade should not exceed 1:14. Nonetheless, administration considers the optional reduction in bin size to 240L size to be collected by private contractors as an acceptable outcome
5 - Sustainability	Insufficient Information To Evaluate: • Engage an ESD professional to assist in the design process and develop a coherent and coordinated sustainable design strategy that brings active and passive design opportunities together. • Identify solar PV performance, EV chargers and hot water supply.	The proposal incorporates a solar array to the main roof of the building, and provides co-location of store rooms to 18 tandem car bays, enabling the provision of power connected through the adjacent stores to each unit. An ESD consultant has been engaged, and has reviewed the drawing set, confirming no design changes will be	The ESD report concludes that the project provides a sustainable and energy efficient design that goes beyond the minimum requirements in the National Construction Code. Administration considers the proposal to be compliant.

		required, and a green star equivalency report provided shortly after DA lodgement.	
6 - Amenity	Pending Further Information: Either remove bedrooms where they do not have direct access to natural light, view and vista or rearrange to allow for access to light and view. This might include an adjustment in the roof form or the addition of a generous operable roof light to the top floor units. Consider an additional window to the corner bedroom to the North-west apartment. Consider balcony amenity to the north-west unit.	Following DRP2, the design has been updated to remove bedrooms from units raised as having reduced natural light access, replacing them with study rooms. The use of blinds to balconies will be required for later afternoon sun mitigation to the western façade of the building. This is on the basis that the design of the façade maximises views to the ocean. Further, the use of metal screening to the western façade will cause maintenance and cost issues due to corrosion and rust as a result of sea spray.	Modifications made, as outlined by the applicant, are considered suitable and adequate by Administration to address the main concerns raised
7 - Legibility	Pending Further Information: Consider design solutions that project the main building entry forwards to become a more prominent and visually legible element and one which engages with its context more deliberately.	The main entrance to the building has been improved through the removal of car bays, increased landscaping, and provision of a canopy cover to the pedestrian entrance. These combined changes deliver a stronger legibility and presence to the entrance of the building.`	Administration concurs with the applicant and considers the proposal to be compliant.
8 - Safety	Supported: No further recommendations	The design as presented for lodgement full satisfies all safety considerations.	Administration concurs
9 - Community	Supported: • No further recommendations	The proposal is noted as providing housing choice, accommodation choice, and an active, engaging building.	Deemed compliant

10 - Aesthetics	Pending Further Information: The internal walkway would benefit from further design consideration. The introduction of intermittent vertical column forms will break the linearity of the balustrade and transform the walkway into a	DRP2 to add further materiality and interest into the atrium courtyard area to develop a cloisters style atrium, that delineates the central communal open space from the pedestrian walkaways to the residential and short stay units.	Modified design is considered suitable and deemed compliant.
	'cloister' creating a hierarchy of spaces that mediate between the communal courtyard, resident walkway and private dwelling. Internal elevations should be provided as part of the application to the City	This enhancement of the central space has been reflected on internal renders, demonstrating the level of privacy and amenity added to the design through this change.	



Technical Note: No. 1a Date: 21/02/2023

Project No: t21.342

Project: LOT 1362 MARITIME DRIVE, JINDALEE Subject: PROPOSED RESIDENTIAL DEVELOPMENT

1. Introduction

This Technical Note (TN) has been prepared by Transcore on behalf of Linic Group with regard to the proposed apartment development with a café/restaurant to be located at Lot 1362 Maritime Drive, Jindalee in the City of Wanneroo.

The subject site is currently vacant and is situated at the southwestern corner of the roundabout intersection of Maritime Drive and Jindalee Boulevard as shown in **Figure 1**. The western leg of the roundabout provides direct access to the public car parking facility in the immediate vicinity of the subject site.



Figure 1: Proposed Conversion of Existing Warehouse

The access and egress arrangement of the proposed development is facilitated via two proposed crossovers; one on Maritime Drive and one on the future access road on the southern side of the subject site.

Pedestrian access to the subject site is available via the existing footpath networks along Maritime Drive and the western side of the subject site.

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The proposed crossover on the future access road leads directly to the undercroft car parking area via a two-way ramp system with separate entry and exit ramps and an intercom at the entrance island. The separate entry and exit ramps will avoid potential conflict between inbound and outbound vehicle movements.

Transcore previously prepared a TIS for the proposed development. The aim of this technical note is to address relevant items raised by the city, including revised trip generation, parking assessment, management of delivery operations for the café/ restaurant and undertaking sight line assessment for residential crossover on the future access road.

2. PARKING ASSESSMENT

The parking layout including some structural columns has been redesigned to comply with AS2890.1. Accordingly, the parking bay envelopes have been redesigned so that they are kept clear of all structural obstructions. As shown in **Figure 4**, all the parking bays are compliant with this requirement as columns do not impact the design envelope in any of the bays.

It should be noted that in accordance with AS2890.1, the parking aisle width needs to be 5.8m which is proposed.

The proposed development caters for patronage by bicycles to and from the building. High-mounted bicycle storage racks are proposed within every storage unit within the car park to accommodate bicycle storage for the residents. Further, it is recommended that 5 bicycle parking racks for visitors should be provided at the front of the building along Maritime Drive frontage. The front forecourt/landscape area has adequate space to accommodate the recommended 5 bicycle parking spaces.

The provision of wheel stops within a residential parking area is considered to be a potential hazard to pedestrians and therefore not provided.

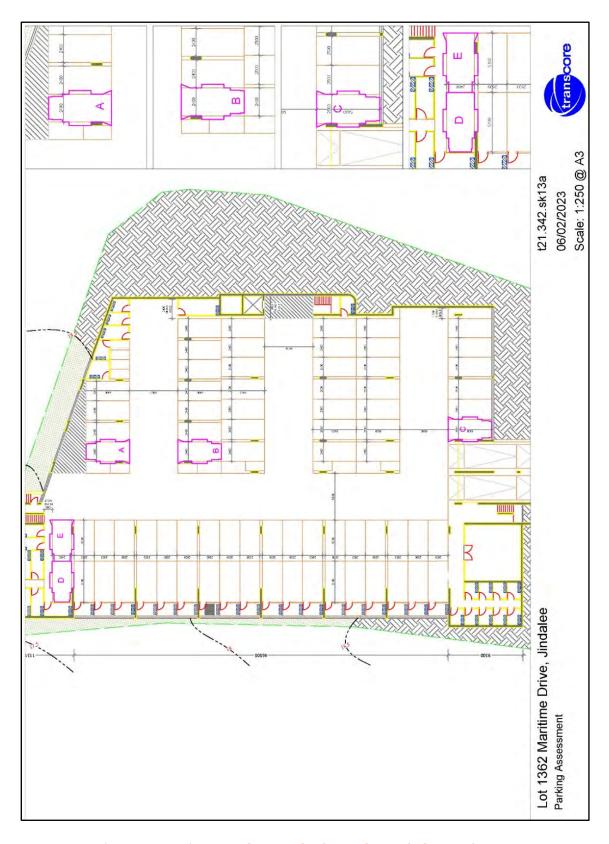


Figure 2: Design envelope to be kept clear of obstructions

3. TRIP GENERATION

The trip generation documented in the TIS has been revised to include the café/restaurant trips and more accurate trip rates for the apartments.

The traffic volumes likely to be generated by the proposed development are estimated in accordance with the *ITE Trip Generation Manual (11th Edition)*.

The traffic generation rates use for the residential part of the proposed development are as follows:

Multifamily Housing (Low-Rise) (220) - 1000 Sq. Ft. GFA

- Daily vehicle trips: 6.74vpd per dwelling;
- Weekday average morning peak hour vehicular trips: 0.4vph per dwelling; and,
- ₩ Weekday average evening peak hour vehicular trips: 0.5vph per dwelling.

The proposed development provides a total of 47 apartments. Accordingly, it is estimated that the traffic generations for the residential component of the development are:

- ♣ Weekday daily: 6.74 x 47 = 317vpd;
- Weekday AM peak hour: 0.4 x 47 = 19vph; and,
- Weekday PM peak hour: 0.51 x 47 = 24vph.

The traffic generation rates use for the commercial part of the proposed development are as follows:

High-Turnover (Sit-Down) Restaurant (932) - 1000 Sq. Ft. GFA

- Weekday daily: 107.2vpd per 1000sqft GFA/ 0.929 = 115.39vpd/ 100m² GFA;
- Weekday AM peak hour of generator: 13.68vph per 1000sqft GFA/ 0.929 = 14.72vph/ 100m² GFA; and,
- Weekday PM peak hour of generator: 16.35vph per 1000sqft GFA/ 0.929 = 17.60vph/ 100m² GFA.

The proposed development provides a 97.9sqm cafe/restaurant facility. Accordingly, it is estimated that the traffic generations for the proposed cafe/restaurant facility are:

- Weekday daily: 115.39 x 97.9/100 = 113vpd;
- \blacksquare Weekday AM peak hour of generator: 14.72 x 97.9/100 = 15vph; and,
- \downarrow Weekday PM peak hour of generator: 17.60 x 97.9/100 = 17vph.

The proposed alfresco is an integral part of the café/restaurant operations, not a stand-alone development and does not generate any additional trips. Accordingly, it is estimated that the proposed development would generate a total of **430** daily

vehicle trips with **34** vehicle trips during AM peak hour and **41** vehicle trips during the PM peak hour periods. These trips include both inbound and outbound vehicle movements. However, it should be noted that a reasonable number of the trips to the Café/ Restaurant are expected to be generated from the residential development itself as well as walk trips from visitors to the area and the beach. Thus, the actual trip generation for the cafe/restaurant is expected to be much lower than reported.

4. PARKING DEMAND FOR RESTAURANT/ CAFÉ

The café/restaurant/ facility is relatively small and is an ancillary part of the development and therefore does not generate the same level of traffic or would have the same level of parking demand if it was a stand-alone development. Further and in addition to the visitations by residents and visitors of the apartments, the balance of the patronage to the café/restaurant is anticipated to be by visitors to the area and the beach who are already parked in the public car park facility and walk to the café/restaurant. Therefore, the parking demand for this facility is expected to be negligible.

5. Provision for Service Vehicle

The bin store area within the development will be used by residents and the café/restaurant facility. The bin storage is proposed at the southern side of the car parking area at the undercroft level. It is anticipated that rubbish bins will be wheeled out from the bin store to the verge for collection on a designated collection day.

The carpark driveway will not be used by the pedestrians. The bin collection will only take place once a week. Thus, the conflict with vehicles and pedestrians will be negligible.

The parking area next to the café/restaurant facility will double up for deliveries of smaller vehicles such as vans. The deliveries will take place outside peak operating periods.

6. CROSSOVER SIGHT LINE ASSESSMENT

Sight line assessment for the subject site crossover on the future road was undertaken as per relevant Australian Standard requirements. The crossover of the subject site is on a Local Road with a default speed limit of 50km/h. Since the crossover is next to the 'T' intersection and due to the curved road alignment, the actual travelling speed on the road is expected to be lower than the speed limit. However, before a conservative and robust assessment, the sight line assessment

was undertaken for the default speed limit of the local road. The stopping sight distance for a 50km/h road is 45m.

Sight line assessments were undertaken in both directions and are shown in **Figure 2**. As evident, the stopping sight distance is achieved and no landscaping and street furniture more than 1.0m tall should be placed along the sight lines.

The sight line assessment for pedestrians is shown in **Figure 3**. As shown, the pedestrian sight line triangle should be kept clear with no landscaping or street furniture more than 1.0m tall. The property boundary of the subject site is secured by a fence with a low-height wall (approx. 750mm AFFL), with an additional openstyle aluminium tubular fence above (90% open format). As per AS2890.1, the driver's eye height is 1.15m.

The driver's eye height (1.15m) will be clearly above the height of the fence (0.75m). and will be above 1.1m {the wall height (.75m) plus the lower level of driveway at 2.5m back from the edge of the pedestrian path(.35m)}.

Thus, there won't be any obstructions in the sight line of the driver towards pedestrians.

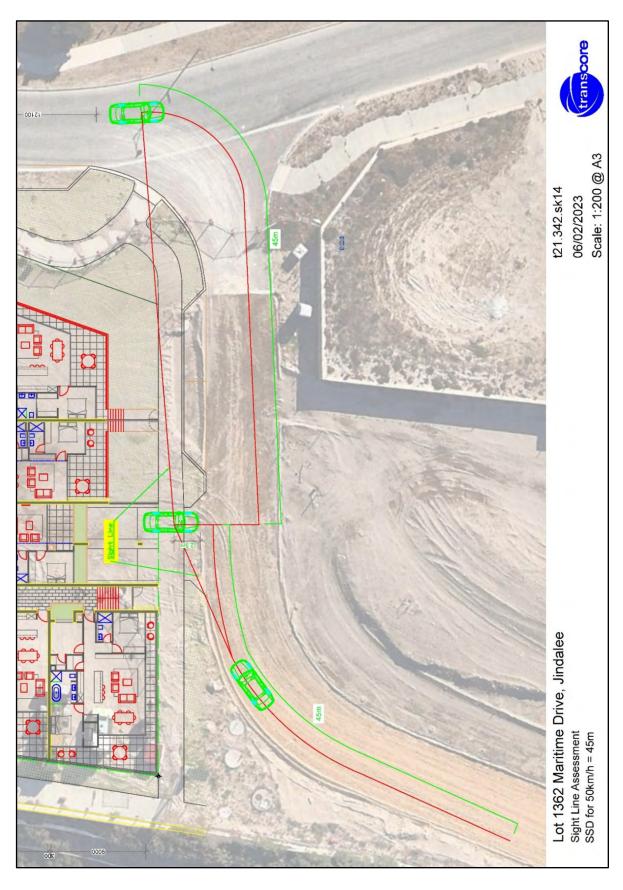


Figure 3: Sight line assessment for vehicles

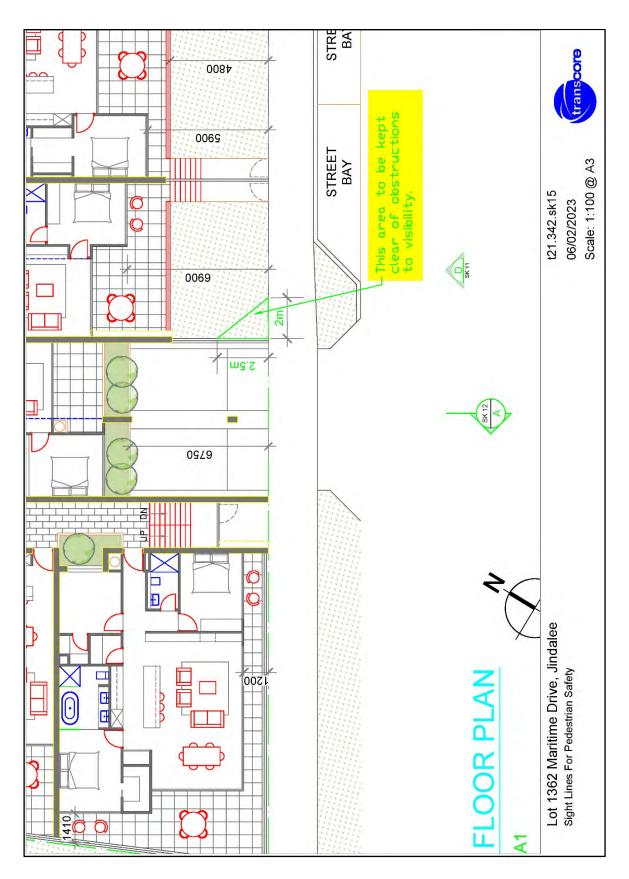


Figure 4: Sight line assessment for pedestrian

7. SWEPT PATH ANALYSIS FOR VISITOR BAYS

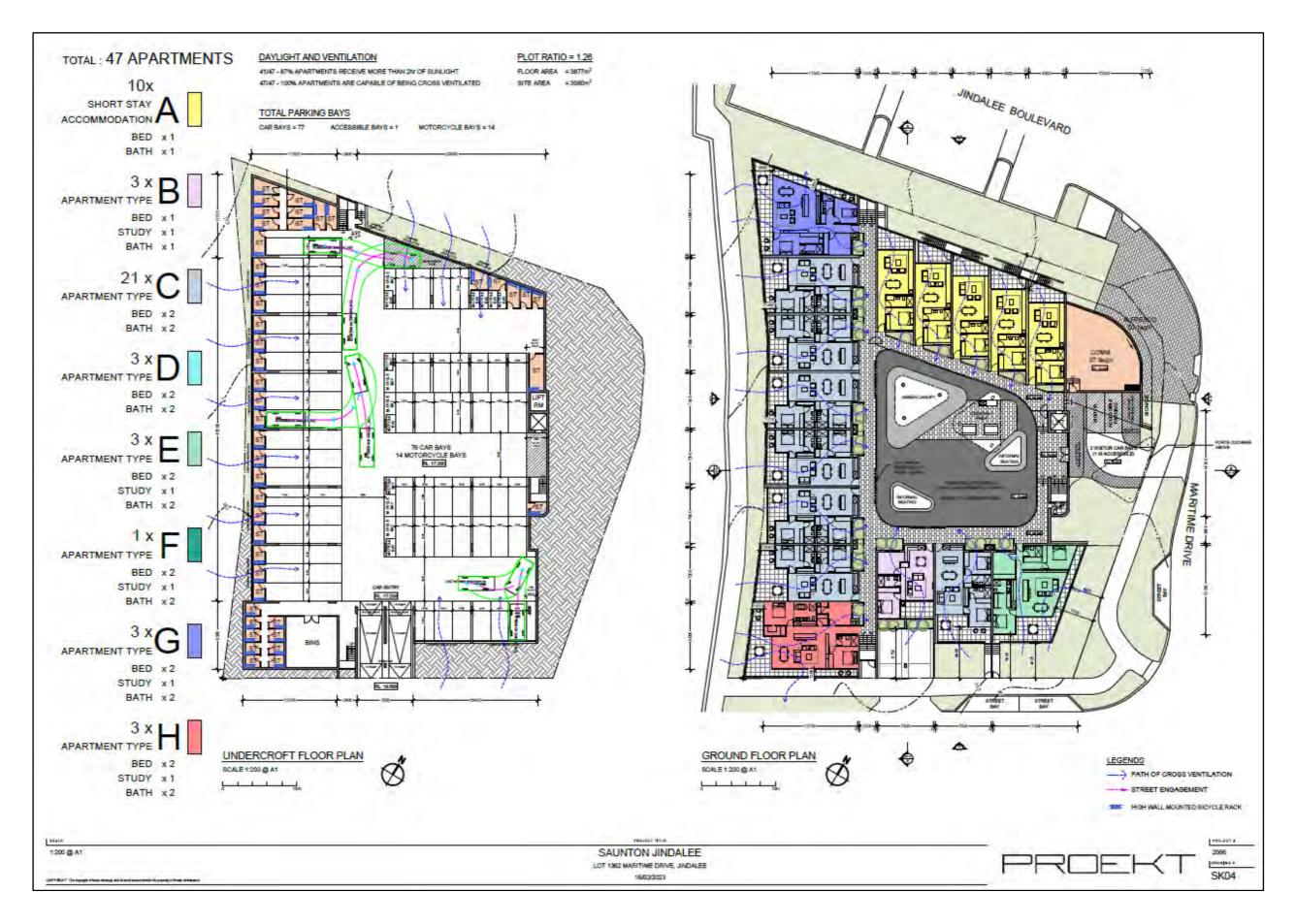
The forecourt area and visitor bays have been redesigned to include an ACROD bay and to present a more functional and compliant design. The development crossover on Maritime Drive has also been widened to facilitate better movements of vehicles in and out of the forecourt area and visitor bays.

The swept path analysis is undertaken for the proposed visitor and ACROD bays for a B99 vehicle. As evident from swept paths in **Appendix B**, the vehicles can enter the forecourt area and visitor parking bays in forward gear. Vehicles will then reverse out of the bays and exit the site also in forward gear.

Appendix A

PROPOSED DEVELOPMENT PLANS



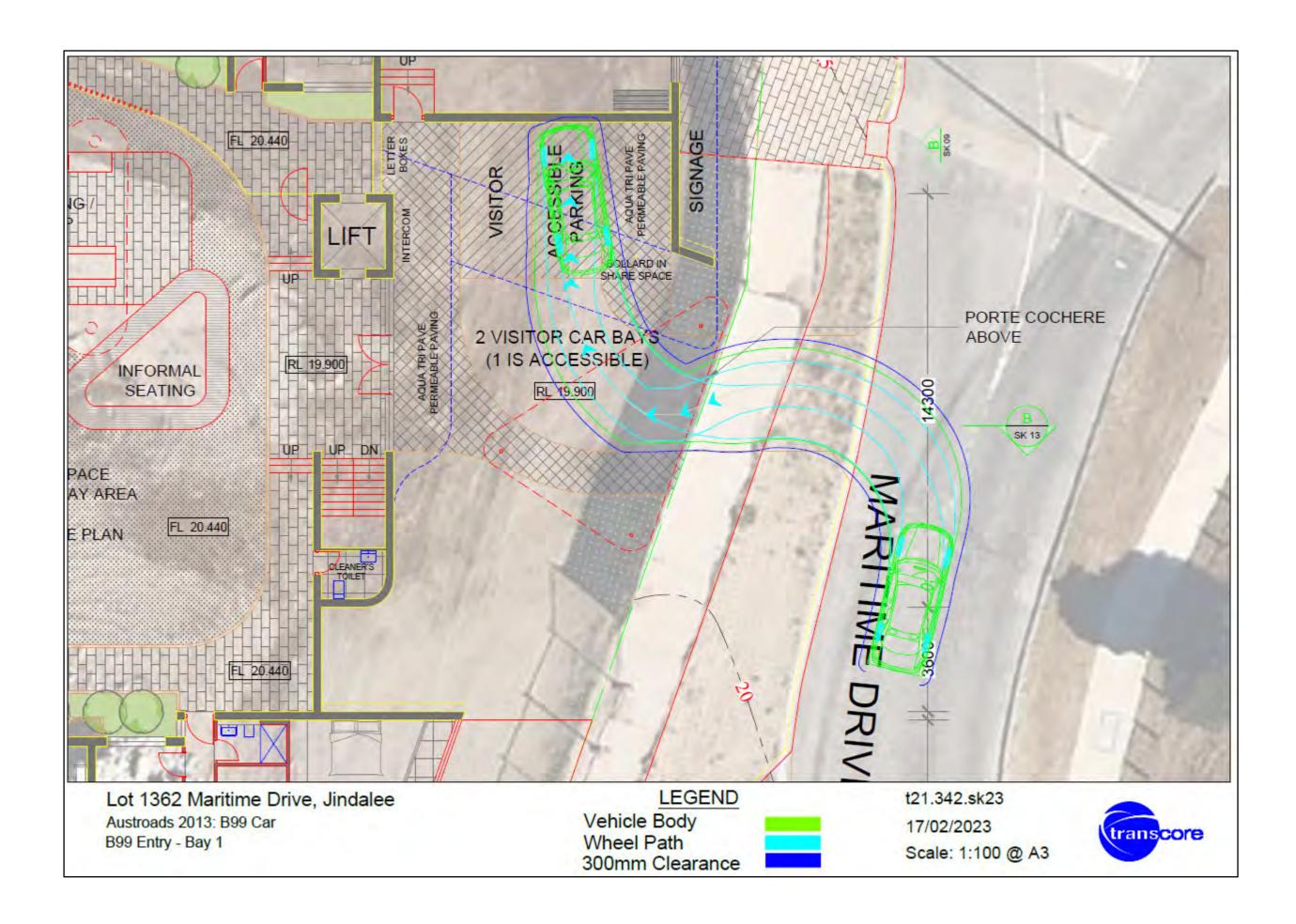


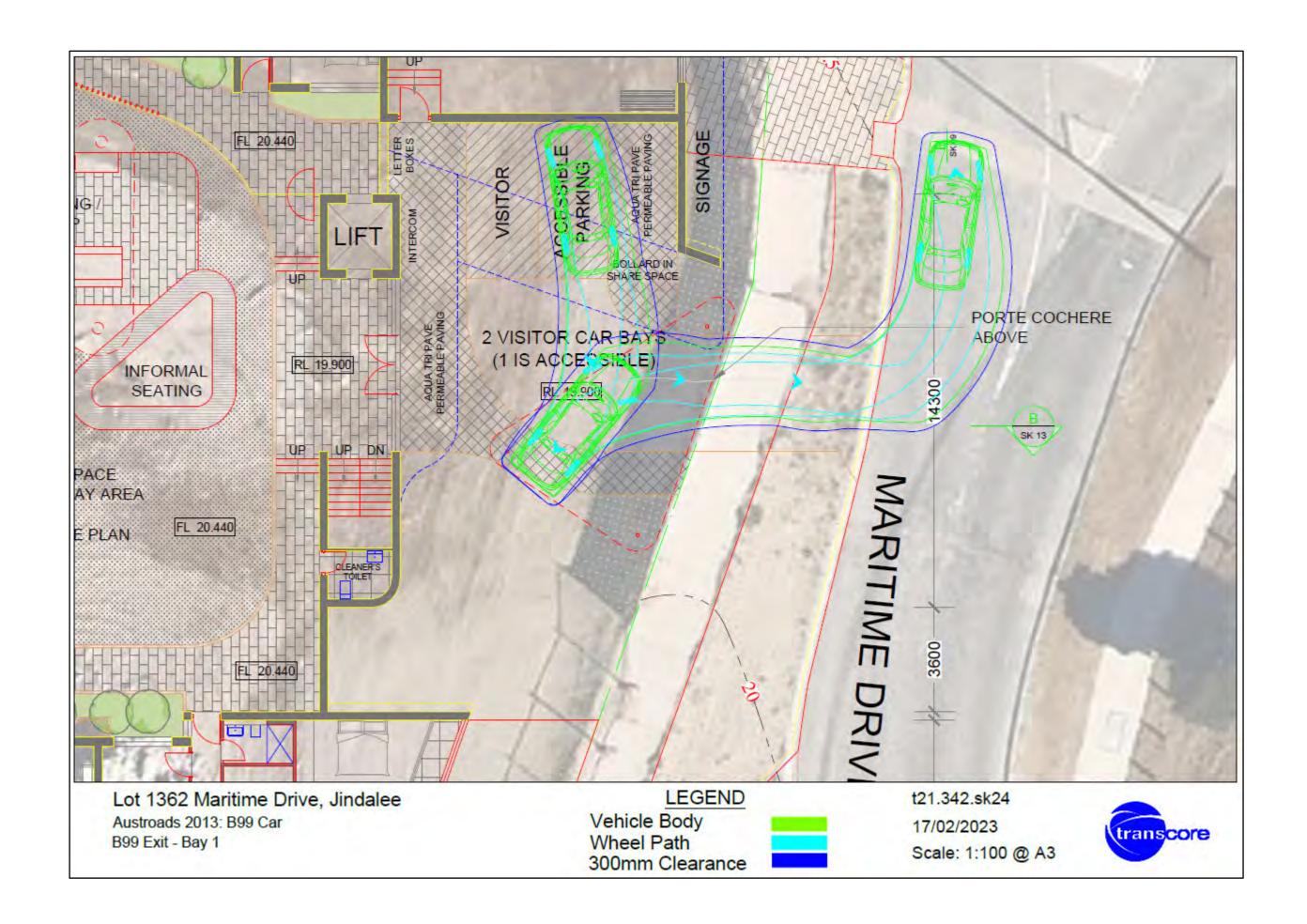


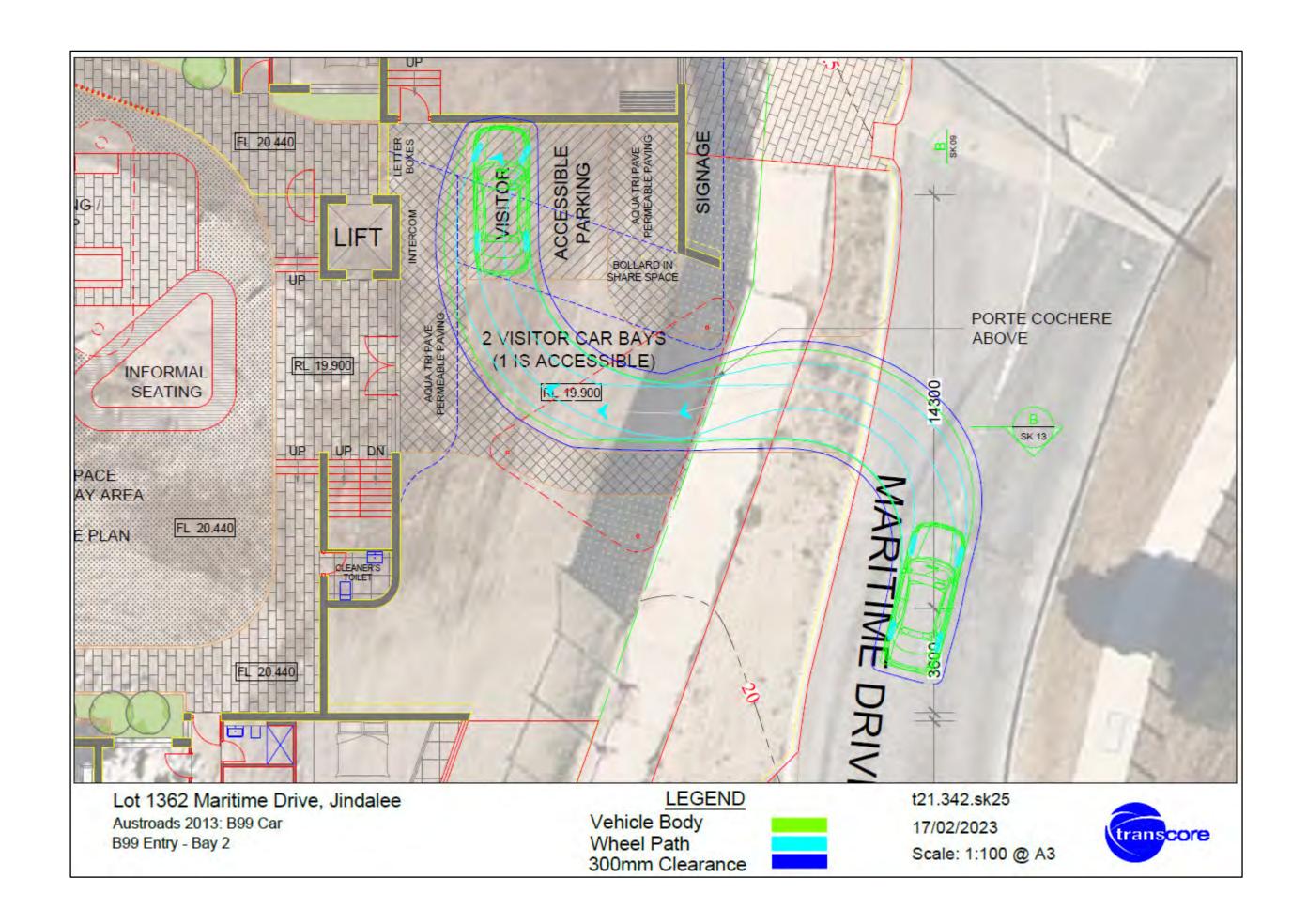
Appendix B

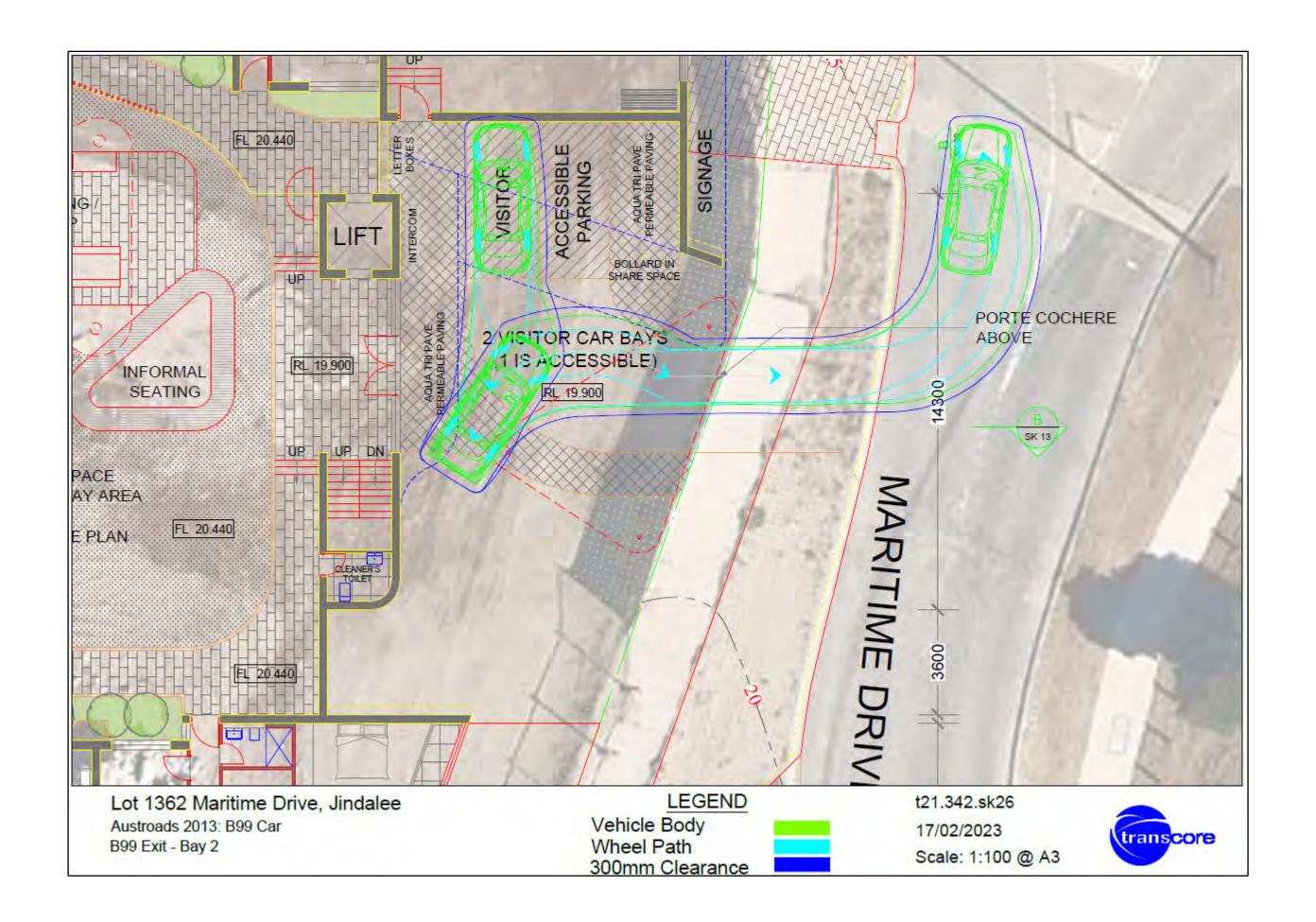
Turn Path Plans













PINNACLE PLANNING

LOT 1362 MARITIME DRIVE JINDALEE

DEVELOPMENT APPLICATION ACOUSTIC REPORT

FEBRUARY 2023

OUR REFERENCE: 28867-3-21498



DOCUMENT CONTROL PAGE

DA ACOUSTIC REPORT

LOT 1362 MARITIME DRIVE JINDALEE

Job No: 21498

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FOR

PINNACLE PLANNING

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APPENDICIES

A Development Application Plans

1.0 INTRODUCTION

Herring Storer Acoustics was commissioned by Linic Pty Ltd, care of Pinnacle Planning, to conduct a preliminary review of the proposed development at Lot 1362 Maritime Drive, Jindalee.

This report has been based on the Development Application drawings provided.

2.0 PROPOSED DEVELOPMENT

The proposed development site is located at Lot 1362 Maritime Drive, Jindalee.

The development consists of an 8 floor building, with basement parking, commercial units on ground floor and 7 floors of apartments.

The following summarises the development:

Undercroft Level

Parking.

Ground Level

Residential Apartments. Commercial Tenancy.

Levels 1 and 2

Residential Apartments.

3.0 CRITERIA

3.1 BCA PROVISIONS

For Class 2 or 3 buildings, Part F5 of the National Construction Code (NCC), outlines the minimum acoustic isolation of apartments. The following summarises the acoustic criteria:

3.1.1 Walls

Wet to wet $R_W + C_{tr}$ not less than 50 dB.

Living to living $R_W + C_{tr}$ not less than 50 dB.

Wet to living R_W + C_{tr} not less than 50 dB plus discontinuous

construction.

Kitchens to living R_W + C_{tr} not less than 50 dB plus discontinuous

construction.

SOU to Lobby R_w not less than 50 dB.

Note: Where kitchens are part of an open living area, we consider the kitchen to be part of the living area and in these cases a discontinuous construction is required. This also includes cases where kitchens are back-to-back, however, discontinuous construction is only required on one side.

3.1.2 Floors

Floors $R_W + C_{tr}$ not less than 50 dB.

Impact Isolation L_{n,w} not more than 55 dB is recommended.

Note: The impact isolation criteria under the BCA is an L_{n,w} of not more than 62

dB. However, as a member firm of the Association of Australasian Acoustic Consultants, (AAAC) we recommend a criteria of an $L_{n,w}$ of not more than

55 dB be adopted for a development of this type.

3.1.3 Service Risers

to Habitable Rooms R_W + C_{tr} not less than 40 dB.

to Non-Habitable Rooms R_W + C_{tr} not less than 25 dB.

3.1.4 Hydraulics

The above requirements also apply to storm water down pipes.

3.1.5 Doors

Door (Connecting to a lobby) R_W not less than 30 dB.

The development will be designed to comply with the requirements of Part F5 of the BCA.

3.2 <u>ENVIRONMENTAL PROTECTION (NOISE) REGULATIONS 1997</u>

The *Environmental Protection (Noise) Regulations 1997* stipulate the allowable noise levels at any noise sensitive premises from other premises. The allowable or assigned noise levels for noise sensitive premises are determined by the calculation of an influencing factor, which is added to the baseline criteria set out in Table 1 of the Regulations. The baseline assigned noise levels are listed in Table 3.1. For commercial premises, the allowable or assigned noise levels are the same for all hours of the day. Table 3.1 also lists the assigned noise levels for commercial premises.

TABLE 3.1 – ASSIGNED NOISE LEVELS

Premises	Receiving	Time of Davi	Assigned Level (dB)			
Noise		Time of Day	L _{A 10}	L _{A 1}	L _{A max}	
		0700 - 1900 hours Monday to Saturday	45 + IF	55 + IF	65 + IF	
Noise	sensitive	0900 - 1900 hours Sunday and Public Holidays	40 + IF	50 + IF	65 + IF	
	within 15 f a dwelling	1900 - 2200 hours all days	40 + IF	50 + IF	55 + IF	
metres or a		2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays	35 + IF	45 + IF	55 + IF	

Note: The L_{A10} noise level is the noise that is exceeded for 10% of the time.

The L_{A1} noise level is the noise that is exceeded for 1% of the time.

The L_{Amax} noise level is the maximum noise level recorded.

It is a requirement that noise from the site be free of annoying characteristics (tonality, modulation and impulsiveness) at other premises, defined below as per Regulation 9.

"impulsiveness"

means a variation in the emission of a noise where the difference between L_{Apeak} and $L_{Amax\;Slow}$ is more than 15dB when determined for a single representative event.

"modulation"

means a variation in the emission of noise that -

- (a) is more than 3dB L_{A Fast} or is more than 3dB L_{A Fast} in any onethird octave band;
- (b) is present for more at least 10% of the representative assessment period; and
- (c) is regular, cyclic and audible.

"tonality"

means the presence in the noise emission of tonal characteristics where the difference between –

- (a) the A-weighted sound pressure level in any one-third octave band; and
- (b) the arithmetic average of the A-weighted sound pressure levels in the 2 adjacent one-third octave bands,

is greater than 3 dB when the sound pressure levels are determined as $L_{Aeq,T}$ levels where the time period T is greater than 10% of the representative assessment period, or greater than 8 dB at any time when the sound pressure levels are determined as $L_{A Slow}$ levels.

Where the above characteristics are present and cannot be practicably removed, the following adjustments are made to the measured or predicted level at other premises.

TABLE 3.2 – ADJUSTMENTS FOR ANNOYING CHARACTERISTICS

Where tonality is present	Where modulation is present	Where impulsiveness is present
+ 5 dB	+ 5 dB	+ 10 dB

From a review of the development, the influencing factor for this development would be 1 dB, based on the following:

Commercial Premises within inner circle.

20% + 1 dB

Total IF + 1 dB

Hence the influencing factor would be + 1 dB and the assigned noise levels would be as listed in Table 3.3.

TARIF 3 3 -	. ASSIGNED	OUTDOOR	NOISE LEVEL

Premises Receiving	Time of Day	Assigned Level (dB)		
Noise		L _{A10}	L _{A1}	L _{Amax}
	0700 - 1900 hours Monday to Saturday	46	56	66
Noise sensitive	0900 - 1900 hours Sunday and Public Holidays	41	51	66
premises within 15	1900 - 2200 hours all days	41	51	56
metres of a dwelling	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays	36	46	56

Note: L_{A10} is the noise level exceeded for 10% of the time.

L_{A1} is the noise level exceeded for 1% of the time.

L_{Amax} is the maximum noise level.

We note that noise emissions from the premises need to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997*. This primarily consists of mechanical services associated with the development.

3.3 NOISE INGRESS

Inbound Noise Levels

Given the location of the development, there is no specific policy that is required to be met.

Notwithstanding the above, it is recommended that the ambient noise level is measured during the design development phase of the project with the internal noise level design criteria adopted from typical noise ingress policies.

The design sound levels would be:

- L_{eq} 35 dB(A) in sleeping areas (bedrooms); and
- L_{eq} 40 dB(A) in living/work areas and other habitable rooms.

4.0 BCA REQUIREMENTS

The proposed development will be constructed to comply with the requirements of Part F5 of the NCC.

5.0 NOISE FROM DEVELOPMENT

The main source of noise from the proposed development will be from mechanical services consisting of air-conditioning plant and perhaps carpark ventilation fans. Noise received at neighbouring premises, and premises within the development, from these items need to comply with the assigned noise levels as determined under the *Environmental Protection (Noise)* Regulations 1997.

5.1 WASTE COLLECTION

It is noted that under the *Environmental Protection (Noise) Regulations 1997*, the noise received at a premise from the collection of waste does not need to comply with the Assigned Noise levels, provided it is carried out in accordance with Regulation 14A of the Regulations.

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In summary, waste collection carried out during the following times do not need to comply with the Assigned noise levels :

- 0700 hours and 1900 hours on any day that is not a Sunday or a public holiday;

or

- 0900 hours and 1900 hours on a Sunday or public holiday.

For information waste collection carried out during the above times is classified as *class* **1** *works*. *Class* **2** works means specified works carried out otherwise than between the hours specified in the definition of class **1** works.

Note:

Class 2 works, also do not need to comply with the assigned noise levels, provided certain requirements, including operating under an approved Noise Management Plan, are followed.

For information it is also noted that, under the Regulations, the above requirements applies to "specified works". Specified works, includes:

- (a) the collection of waste; or
- (b) the cleaning of a road or the drains for a road; or
- (c) the cleaning of public places, including footpaths, cycle paths, car parks and beaches; or
- (d) the maintenance of road verges and public open space (including the collection of rubbish and the planting, trimming, watering or removal of trees); or
- (e) the periodic collection of household items or other things placed on street verges by residents for the purpose of such a collection; or
- (f) activities associated with hazard or emergency management.

Waste means waste from domestic or commercial sources and includes:

- (a) putrescible waste; and
- (b) non putrescible waste; and
- (c) recyclable materials.

Thus, based on the above, noise emissions from waste collections do not need to be assessed and does not form part of the acoustical assessment. This would apply to the commercial and residential sections of the development.

5.2 MECHANICAL SERVICES

The main source of noise from the proposed development will be from mechanical services consisting of car-park ventilation fans and air-conditioning plant. Noise received at residence (neighbours and residence within the development) from these items need to comply with the assigned noise levels as determined under the *Environmental Protection* (Noise) Regulations 1997.

As the mechanical services could operate during the night, noise emissions from the development needs to comply with the assigned $L_{\rm A10}$ night period noise level of 36 dB(A) at residential premises. Potentially, noise emissions from mechanical services could be tonal, in which case an +5 dB(A) penalty for a tonal component could be applied to the resultant noise levels. Therefore, the design level at the neighbouring residential premises would be 31 $L_{\rm A10}$ dB.

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5.2.1 Apartments

The air conditioning for the apartments is not yet known.

Once the design of the system is finalised, an acoustic assessment will be carried out of noise emissions from the mechanical plant and any noise amelioration required will be incorporated into the design to ensure compliance with the *Environmental Protection (Noise) Regulations 1997*.

This would be in response to likely development approval conditions for the project.

5.2.2 Car Park Exhaust Fan

Noise emissions from carpark exhaust fans, or indeed if any mechanical ventilation is required, will also need to comply with the Regulatory requirements. From previous projects, we believe that with careful fan selection and the incorporation of either 1D or 2D unpadded silencers, compliance with the *Environmental Protection (Noise) Regulations 1997* is normally achieved.

An assessment of noise emissions will be carried out once equipment has been selected and submitted for approval. This would be in response to likely development approval conditions for the project.

5.2.3 Commercial Restaurant Tenancy

Noise emissions associated with the restaurant tenancy could include exhaust hood, air conditioning, delivering and rubbish collection.

Mechanical services noise would be assessed in response to likely development approval conditions for the project, as the design for this equipment has not been undertaken at this preliminary stage and varies substantially with the cooking requirements of different restaurants – to the extent that the tenancy may not have need of an exhaust.

Deliveries would be via the visitor parking bays and be no different than normal car parking due to natural restriction on delivery vehicle size that this would impose.

Rubbish collection would be dealt with as per Regulation.

APPENDIX A

DEVELOPMENT APPLICATION PLANS





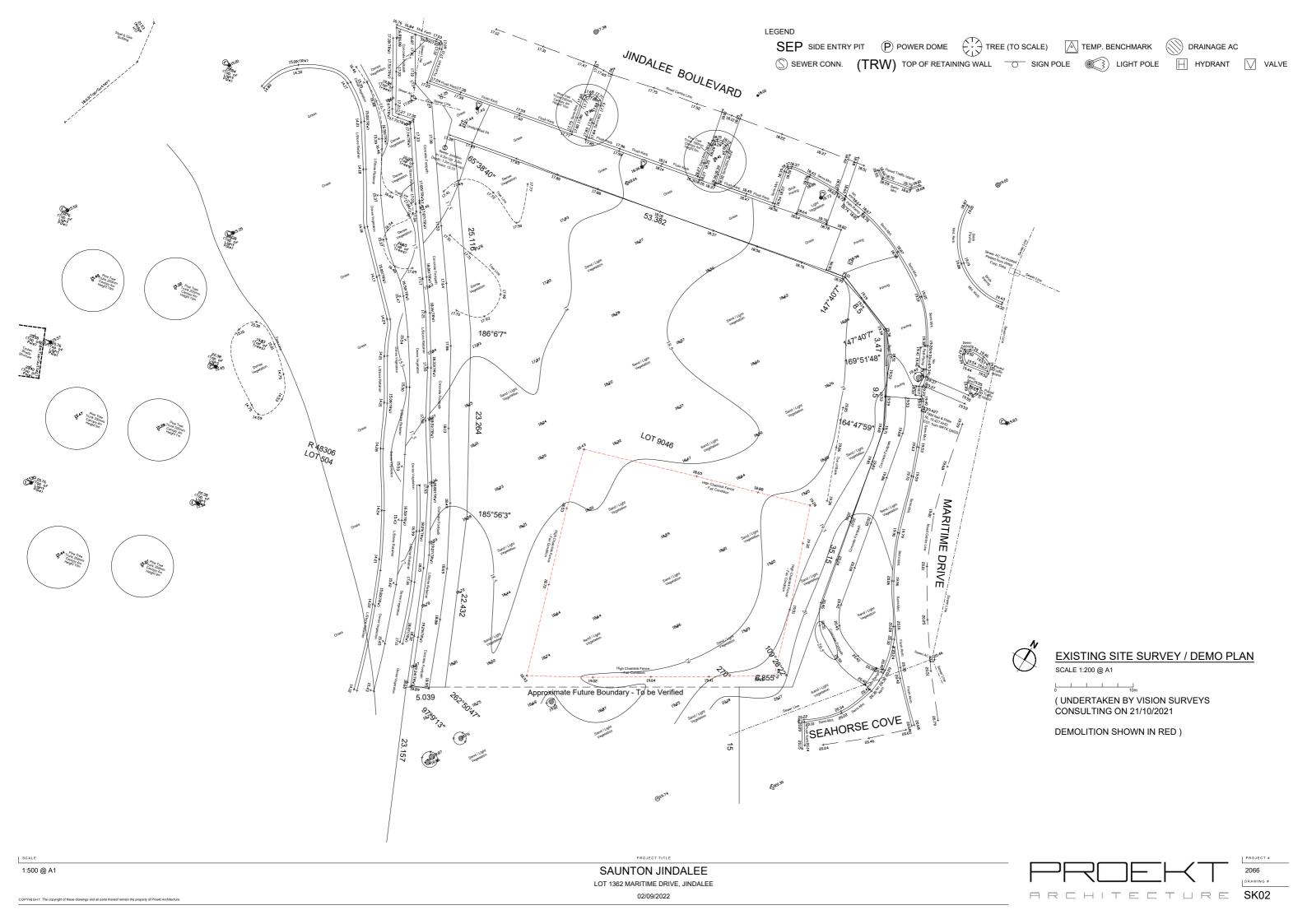
LOCATION PLAN



SAUNTON JINDALEE LOT 1362 MARITIME DRIVE, JINDALEE

02/09/2022









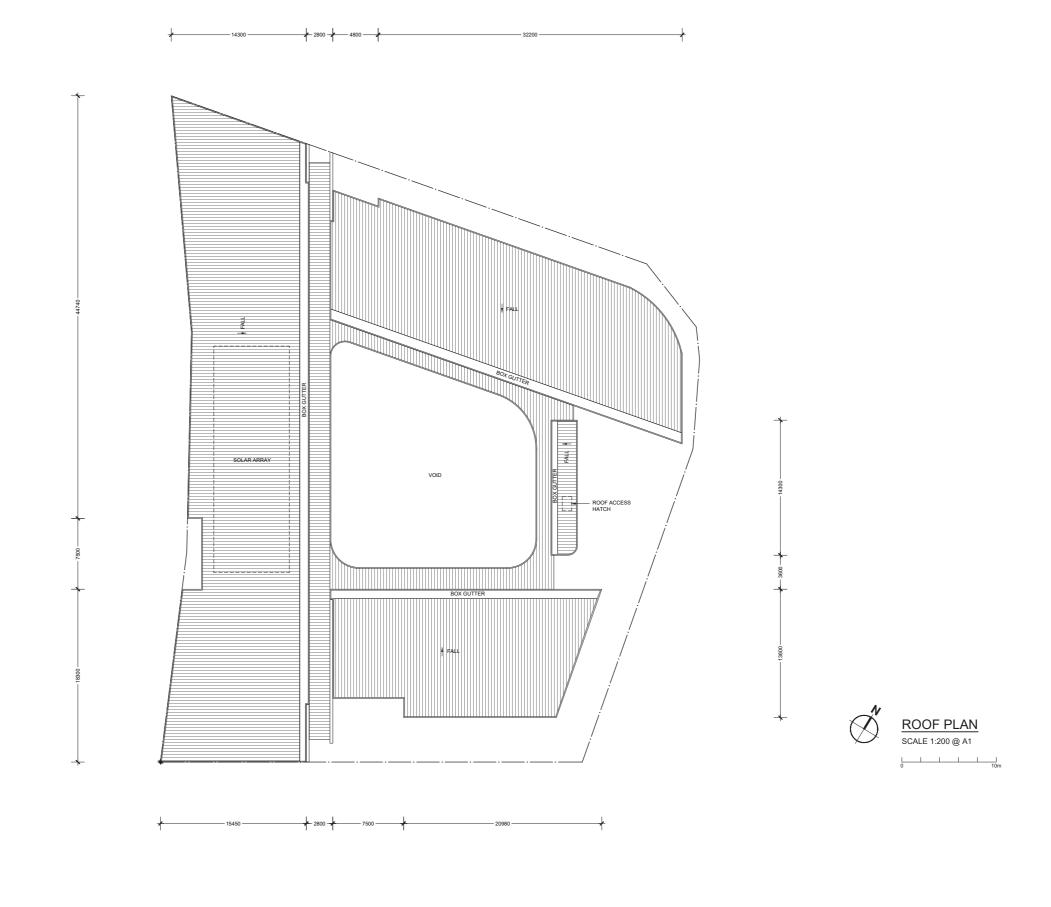
SAUNTON JINDALEE LOT 1362 MARITIME DRIVE, JINDALEE 02/09/2022

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PROJECT TITLE

SAUNTON JINDALEE

LOT 1362 MARITIME DRIVE, JINDALEE 02/09/2022





Waste Management Plan



Lot 9046 Maritime Drive, Jindalee – Mixed Use Development PINNACLE

1.0 Introduction

This waste management plan (WMP) assists and supports a mixed use development to be located at Lot 9046 Maritime Drive, Jindalee, as part of a Development Application to the Metro Outer North Joint Development Assessment Panel.

The WMP has been developed on the basis of preliminary investigations with the project team, and discussions with the City of Wanneroo (City) around suitable waste storage, collection, and provision of waste facilities and infrastructure.

As the proposal has an under croft basement level, and bin store in the basement, the use of 360L bins has been specified, as they are readily moved, as opposed to larger 660L bins. We are advised due to the aesthetic design, underneath clearances to enable a waste vehicle to enter the under croft garage is not possible.

In this regard, and given there are a mix of uses, the waste methodology and approach is to provide for kerbside collection, by a private contractor once or twice weekly.

The methodology and approach of this report ensures the building can cater to the waste demands for the various uses, have a functional and adequate bin store, and have a method of collection that is appropriate to the operation of the building.

The WMP is based on the WALGA Guidelines for Waste Management and ensures the use of appropriate calculation rates, to supply suitable volumes of capacity for waste storage for the amenity of residents of the development.

2.0 SUMMARY OF DEVELOPMENT

The proposal, a high-quality mixed development, consists of a 3 storey development, including apartments, short stay apartments, and a commercial tenancy.

The building includes an under croft basement, that includes the bin store.

2.1 Location of Development

The development is located on the corner of Maritime and Jindalee Drives in Jindalee. The site is located within the City of Wanneroo.

The subject site is located within the Jindalee Coastal Village.

2.2 Number of Floors

The proposal consists of multiple dwellings, short stay apartments and a café tenancy, and is located over 3 floors.

2.3 Number of Dwelling Units by Size

The following table sets out the number of units within the development relative to bedroom size breakdowns, treating the short stay accommodation units the same as single bedroom units:

Residential Dwelling Units	Number
1 Bedroom Unit	3
2 Bedroom Unit	30
Short Stay Unit	14
Total Unit Yield	47

2.4 Size of Each Commercial Unit

The proposed development includes the provision of a 97m² café tenancy.

2.5 Details of the Intended Use of the Development

The development is designed and proposed to be a landmark development at the Jindalee Beach foreshore, with a mix of housing, holiday accommodation and a café use.

3.0 WASTE GENERATION RATES

The City utilises the Western Australian Local Government Association (WALGA) Multiple Dwellings Waste Management Plan Guidelines.

Appendix C, Table 1 of the guidelines sets out the various waste generation rates for residential development projects, and the residential components of mixed-use development projects.

As this proposal relates to residential and commercial rates, both streams are accounted for in the calculations.

3.1 Waste Calculation Rates

The following table details the gross waste generation rates for the development.

Dwelling size	Refuse (L/week)	Recycling (L/fortnight)
2 bed apartment (160/120L	5,440	4080
1 bed apartment (80L/80L)	1,040	1,040
Commercial Tenancy (300L per	2,100	1,400
100sqm per day)		
Gross waste generation	8,580	6,520

3.1.1 Refuse

The proposal has a generation of 8,520 litres of general refuse. This will be stored in the central bin store, described in detail below.

3.1.2 Recycling

The proposal has a generation of 6,520 litres of co-mingled recycling per fortnight, and will be stored in the central bin store, described in detail below.

3.1.3 Other

As the proposal is below the 100 unit threshold for a bulky goods collection room, the proposal does not cater for this requirement.

4.0 BIN STORAGE AREA

The proposal caters to the whole development through a central bin store located in the basement parking area.

Figure 1 contained over leaf sets out the central bin store location, along with the location of the kerbside presentation.

4.1 Waste Disposal

Residents and visitors within the development will be required and advised through strata management and short stay management to present their own waste to the central bin store.

4.2 Bin Storage Area Size

The central bin store is based on the following generation rates and storage capacity:

Dwelling size	Refuse (L/week)	Recycling (L/fortnight)
2 bed apartment (160/120L	5,440	4,080
1 bed apartment (80L/80L)	1,040	1,040
Commercial Tenancy (300L per	2,100	1,400
100sqm per day)		
Gross waste generation	8,580	6,520

The above rates, if adjusted to a twice weekly private collection, means that the following adjusted rates apply, for the purpose of bin provision in the bin store:

Dwelling size	Refuse (L/week)	Recycling (L/fortnight)
2 bed apartment (160/120L	2,720	1,020
1 bed apartment (80L/80L)	520	260
Commercial Tenancy (300L per	1,050	350
100sqm per day)		
Adjusted waste generation for	4,290	1,630
twice weekly collection		

This leads to the central bin store including:

- 12x 360L general refuse bins
- 6 x 360L recycling bins

Figure 1 shows that the bin store adequately caters to the bin sizes for the waste requirements of the development.

Notwithstanding, the bin store has capacity for further bins, should there be a desire to increase waste capacity. Taking this point further, and in this regard, should it be deemed that 240L bins are a more suitable size with respect to presentation to waste collection vehicles using the ramp to the basement, there is suitable capacity for this within the bin store.

Use of size of bin will determine once or twice weekly collection.

4.3 Bin Storage Area Layout

The central bin store is designed to enable access through an access door, facing the under croft car park, with circulation centrally in the bin store.

Consideration needs to be given to the use of 360L, or 240L bins, and the layout of bins to ensure the availability of bins for residents.

Strata management is to ensure suitable management of the rotation of bins to enable suitable use of the bin store by all occupants of the building.

4.4 Alternative Waste

Any alternative waste such as hard, bulk or hazardous waste is to be coordinated for separate removal through residents and tenants alike coordinating with strata management.



4.5 Wash Down Area

The central bin store area is treated with both a water source, and central floor waste, that enables convenient and on site wash down on bins following collection, as per the architectural plan set, and Figure 1 of this report.

4.6 Ventilation

Ventilation grilles are to be provided to the bin store to aid with ventilation.

4.7 Vermin Protection

The bin store is to be treated with vermin proof solutions, with final detailing to occur at building permit stage of the project.

4.8 Noise Reduction

As the bin store is enclosed, save for ventilation, and does not include any mechanical infrastructure by way of compactors, the noise emanating from the bin store is limited to opening and closing of bins, which is a minimal acoustic impact.

4.9 Stormwater Ingress Prevention

The central bin store is sealed at the floor, and suitably protected against stormwater entering the bin store.

5.0 COLLECTION METHOD AND FREQUENCY

5.1 Collection Vehicle to be Utilised

The proposal is to be serviced by private contractors, given the mix of residential and commercial uses on the site.

The use of private collection also enables the use of a smaller bin room, and bins, that can be wheeled out of the parking area to be serviced on the verge, by private contractors.

The use of Council collection can also be contemplated, subject to the confirmation on frequency of collection, and selection of bin sizes.

5.2 Movement of Collection Vehicles/Swept Path Analysis

The development does not cater to internal collection, given the use of private collection, which enables a greater frequency of collection. As such, smaller 360L bins can be used in the bin store, and presented to the street by the strata management, or waste contractor, depending on the level of service engagement, and roles and responsibilities for waste collection.

The use of council collection will mean greater reliance of strata management for presentation of bins to the street.

5.3 Collection Location

The internal bin store area provides bins to be wheeled out via strata management, and accessed via private waste service vehicles.

Bins are to be collected adjacent to the vehicle crossover servicing the development.

5.4 Transfer of Waste to the Collection Vehicle

Bins will be required to be wheeled to the street by the strata management, or private waste contractor, and thereafter transferred to the collection vehicles.

The use of the vehicle ramp for presentation of bins to the street is to occur outside of peak hours, and is to involve the use of witches hats or some other barrier, to assist with blocking off the ramp to negate vehicle conflict.

5.5 Frequency of Collection

The provision of waste receptacles for the development is based on collection of all waste by private contractors, with frequency 2 times a week. There is further capacity to increase the number of bins in the store to facilitate once weekly collection.

This is also dependent on the use of either 360L and 240L bins.

5.6 Chutes

The proposal only caters to at grade waste disposal, and does not include any provisions for a bin chute.

5.7 Compactor

The proposal is shown to adequately cater to the waste generation rates through the various selection of bin types and sizes nominated in this report. Accordingly, there is no requirement for a

compactor in the development, and the avoidance of such infrastructure reduces acoustic impacts on site and to abutting residents.

5.8 Bin Lifter

The units will be serviced by the central bin store. Accordingly, there is no requirement or need to be providing bin lifters, as the residents of the development will not be expected to utilise small bins, and lift into larger bins, given the waste approach for the development does not propose such methods of waste double handling.

6.0 WASTE PROVIDER

Initial engagement and discussions with the City's staff confirm no objection to the proposal for private contractor servicing of waste.

The bin store is appropriately sized to cater to the generation rates and needs of both the residents and commercial tenants alike, with the flexibility to allow private contractors to collect more frequently than the City's services, enabling the size of the bin room to be contained, and not be excessive for the scale of the development.

Similarly, the retention of 360L bins, and a greater number of them presents the ability to consider once weekly Council collection.

7.0 EDUCATION

The education of residents, commercial tenants, and short stay guests alike within the development will be required, to confirm to the obligations with respect to waste management presentation to the bin store.

The central bin store area to include signage, encouraging the residents of the development to report any issues associated with the bin store, to the strata management for resolution of issues.

The use of strata management, and instructing residents and guests as to obligations around good waste management to ensure on-site amenities will be an essential component for incoming residents. This is particularly important with respect to pro-active reporting of residents of bin room capacity, and requirements for the removal of larger bulky waste items, which residents will be required to arrange with strata management, to avoid the bin room being clogged with bulky items and becoming inaccessible for other residents and commercial tenants.

Of importance, the education of residents, visitors and commercial tenants alike to inform around the time of the day, and procedure for blocking the vehicle ramp for ease of movement of bins to the street is vital to ensure occupants of the building can safely plan for and avoid any conflicts with movement of the bins to the street. Further, the education of this process, over time, will ensure all occupants avoid the times and occurrences of bin presentation.

8.0 ONGOING MANAGEMENT

Subject to the approval of the City of Wanneroo, the ongoing management and coordination of the waste management approach for the development will be deferred to the responsibility of the strata management.

The strata body corporate is therefore tasked with the appropriate use of the bin store, which includes upholding the requirements of residents to appropriately dispose of waste, and to alert the grounds keeper or maintenance person tasked with management of any issues.

Similarly, the strata body corporate is required to assign services attributed to the good and regular functioning of waste services at the property, including:

- Regular cleaning and checking of the bin store room;
- Regular inspection of the bin store to ensure functioning of all bins;
- Coordination of presentation of bins for collection by the building's nominated private waste contractor prior to waste collection days;
- Timely returning of bins to the bin store following waste collection;
- Coordination with short stay accommodation around waste generation, and alerting the operator to large generation of waste;
- Coordination of other incidental waste collection, such as bulk or hazardous waste; and
- Regular monitoring of waste levels, and ensuring adequate service for all users, relative to frequency of collection;
- Auditing of waste capacity to ensure the provision as part of the waste management plan remains suitable, or if further bins are required.

Incidental waste collection such as bulk or hard waste, or building materials waste is to be coordinated between residents and the building's strata management.

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<u>Sustainable Design Assessment – DA Stage</u>

Date: 16 December 2022

Our Reference: 22-14101

Project Address: Lot 1362 Maritime Drive, Jindalee

BCA Climate Zone: 5
Building Class: 2/3

Report Commissioned By: Pinnacle Planning

Report Details		
Report Author: Nathan Peart	GBCA Acc. No.: 49264	Signature:
Revision Date: - Reason for Revision		



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1 Project Information

This report has been commissioned to give preliminary feedback on the proposal at Lot 1362 Maritime Drive, Jindalee to guide the sustainability objectives of the project as detailed section 2. The proposal is on a vacant site. The proposed development contains 47 residential apartments with a mixture of long term and short stay accommodation.



Figure 1: Site Location



2 Sustainability overview

The property is within the City of Wanneroo. No specific sustainability criteria have been identified within the planning policies or schemes for this location or project. Therefore, the Green Building Council's Green Star calculator has been used to guide the sustainability objectives of this development with the following criteria considered.

2.1 Solar and Daylight Access

The development has been designed to optimise solar and daylight access for dwellings by:

- Ensuring both 87% of apartments receive more than 2 hours of sunlight.
- Windows being visible from most of a habitable room.
- Minimising the amount of obscure glazing.
- Glazing will have appropriate shading to reduce heat transfer into the dwelling.

2.2 Natural Ventilation

The optimisation of natural ventilation has been addressed in the development by:

- Natural ventilation to facilitate cross-ventilation.
- The use of openings and façade elements to assist the capture and use of prevailing breezes.
- Providing a combination of operable windows for improved breeze control.

2.3 Shading

Effective shading from the summer sun has been achieved in the development through the:

- Provision of balconies and sharing over living areas on the north and west orientation
- Use of fixed external shading devices on the east and west orientation

2.4 Thermal Mass

The use of thermal mass for passive heating and cooling has been included in the development through the use of:

- Concrete floors
- Insulated brickwork
- Insulated roof
- Feature brick walls, slabs etc.

Thermal mass has been designed, where applicable, with the following in mind:

- Located in areas that receive direct sunlight or radiant heat from heaters
- Exposed to cool night breezes and air currents

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2.5 Additional Energy Efficient Initiatives

The following energy efficient initiatives that exceed current minimum practice have been included in the development:

- Ceiling fans to all habitable rooms
- Use of a photovoltaic array for communal services.

2.6 Additional Sustainability Elements

The following energy efficient initiatives that exceed current minimum practice have been included in the development:

- Building Commissioning to be performed as per approved standards and guidelines.
- Building Information Comprehensive operations and maintenance information to be developed and made available to the facilities management team; and Relevant and current building user information to be developed and made available to all relevant stakeholders.
- Paints, Adhesives, Sealants and Carpets At least 95% of all internally applied paints, adhesives, sealants (by volume) or carpets (by area) meet the total VOC limits (See appendix B and C)
- Engineered Wood Products at least 95% (by area) of all engineered wood products meet the formaldehyde emission limits specified by in Appendix D.
- Lighting to be max. 4.5 watts/m²
- AC systems to be min. 5 Star rated.
- Clothes dryer to be within 1 star of maximum star rating available
- Common Area lighting to have automatic lighting control.
- Waste contractor contracted to dispose of construction waste in a manner that complies with Green Star requirements
- Water fixtures as per Appendix E
- Heat Island effect reduction through the use of a Surfmist Roof Colour.

3 Conclusion

It is the view of Sustainability WA that this project provides a sustainable and energy efficient design that goes beyond the minimum requirements in the National Construction Code.

The builder and all consultants involved with the project would need to incorporate the requirements and recommendations of the formal review into their documentation, seek clarification from the author where required, and ensure the building is constructed in accordance with the Green Building Council of Australia requirements.

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Appendices

GBCA Design and as Built Clause	Action Required	Comments on point viability.
2.1	Services and Maintainability Review	Services and Maintainability Review and report by the head contractor during design stage and prior to construction to review commission ability, controllability, maintainability, operability and safety nominated systems.
2.2	Building Commissioning	Building Commissioning to be performed as per approved standards and guidelines.
4.1	Building Information	Building Information – Comprehensive operations and maintenance information to be developed and made available to the facilities management team; and Relevant and current building user information to be developed and made available to all relevant stakeholders.
9.2	Provision of Outdoor Air at a rate 50% to 100% greater than the minimum required by AS 1668.2:2012	Naturally ventilated – openings comply.
9.3	Exhaust or Elimination of Pollutants- Ensure kitchens and photocopy/print rooms are exhausted separately to AS1668.2:2012.	Exhaust fans to be externally flued.
11.0	Minimum Lighting Comfort – All lights to primary and secondary nominated spaces to have light sources must have flicker free lighting and a minimum Colour Rendering Index (CRI) of 80	Electrician/Lighting contractor to ensure compliance
11.1	General Illuminance and Glare Reduction – Lighting to comply with relevant table of AS/NZS1680.2 demonstrating best practice. All bare light sources must be fitted with baffles, louvers, translucent diffusers, or other means that obscures the direct light source from all viewing angles of occupants.	Electrician/Lighting contractor to ensure compliance
11.2	Surface Illuminance - surface reflectance for ceilings of at least 0.75(0.75 = matte white), ceiling area to have an average surface illuminance of at least 30% of the lighting levels on the working plane.	Electrician/Lighting contractor to ensure compliance
11.3	Localised Lighting Control – occupants must have the ability to turn the lights on and off and adjust light levels in their immediate environment.	Electrician/Lighting contractor to ensure compliance
12.0	Glare Reduction – Glazing in all primary spaces to have blinds, screens, fixed devices to reduce glare	Assumed compliant in residential buildings.
12.1	Daylight – Calculator to be completed by Sustainability WA	-
13.1	Views - Calculator to be completed by Sustainability WA Paints, Adhesives, Sealants and Carpets - At least 95% of all internally applied paints, adhesives, sealants (by volume) or carpets (by area) meet the total VOC limits (See appendix B and C)	Specification to be updated to ensure compliance.
13.2	Engineered Wood Products at least 95% (by area) of all engineered wood products meet the formaldehyde emission limits specified by in Appendix D.	Specification to be updated to ensure compliance
15	Calculator by Sustainability WA	Lighting to be max. 4.5 watts/m ² AC systems to be min. 5 Star rated. Clothes dryer to be within 1 star of maximum star rating available Common Area lighting to have automatic lighting control.
16A	Prescriptive Pathway – On-site Energy Generation - on-site renewable energy or on-site generation sources reduces the peak electricity demand by at least 15%. Alternatively,	Included

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	Accredited Green power option can be used. See Table 2	
	15.6A option 1.	
17B.4	Active Transport Facilities	Store provides for active transport.
18B.1	Sanitary Fixture Efficiency – Water efficiency fixtures to be specified as per 18B.1 in Table 4	Fixtures as per Appendix E
18B.4	Landscape Irrigation - either drip irrigation with moisture sensor override specified, or where no potable water is used for irrigation.	Landscape design to include requirements.
20.3	Permanent Formwork, Pipes, Flooring, Blinds and Cables – Specify no PVC to be used or PVC used meets the GBCA Best Practice Guidelines for PVC	PVC products used on project to be registered at: http://www.vinyl.org.au/in-greenstar/best-practice- pvc-product-register
22A	Waste - Construction	Waste contractor contracted to dispose of constructio waste in a manner that complies with Green Star requirements. (NOTE: See 30c)
25.0	Heat Island Effect Reduction	Roof to be Surfmist.
26.2	Stormwater Pollution Targets.	Only rainwater to be released, therefore complies
27.0	Light Pollution to Neighboring Bodies –	Electrical consultant/contractor to ensure that outdoo light of project complies with \$ 4282:1997 Control of the obtrusive effects of outdoor lighting
27.1	Light Pollution to Night Sky	Electrical consultant/contractor to ensure no external luminaire on the project has a ULOR that exceeds 5%, relative to its actual mounted orientation
30C	Improving on Green Star Benchmarks	Ultra Low VOC – 50% of internal paints to ha a maximum TVOC content of 5gL Stormwater meets Pollution reduction targets. Maximum of 5KG of waste per square metre of GFA.



Appendix B: Table 13.1.1: Maximum TVOC Limits for Paints, Adhesives and Sealants

Product Category	Max TVOC content in grams per litre
	(g/L) of ready to use product.
General purpose adhesives and sealants	50
Interior wall and ceiling paint, all sheen levels	16
Trim, varnishes and wood stains	75
Primers, sealers and prep coats	65
One and two pack performance coatings for	140
floors	
Acoustic sealants, architectural sealant,	250
waterproofing	
membranes and sealant, fire retardant sealants	
and	
adhesives	
Structural glazing adhesive, wood flooring and	100
laminate adhesives and sealants	

Appendix C: Carpet Test Standards and TVOC Emissions Limits

Compliance option	Test Protocol	Limit
ASTM D5116	ASTM D5116 - Total VOC	0.5mg/m2 per hour
	limit*	
	ASTM D5116 - 4-PC	0.5mg/m2 per hour
	(4-Phenylcyclohexene)*	
ISO 16000 / EN 13419	ISO 16000 / EN 13419 - TVOC	0.5mg/m2 per hour
	at three days	
ISO 10580 / ISO/TC 219	ISO 10580 / ISO/TC 219	0.5mg/m2 per hour
(Document N238)	(Document N238) - TVOC at	
	24 hours	

^{*}Both limits should be met when testing against ASTM D5116



Appendix D: Table 13.2B: Formaldehyde Emission Limit Values for Engineered Wood Products

Test Protocol	Emmision Limit/Unit of
	Measurement
AS/NZS 2269:2004, testing procedure AS/NZS	≤1mg/ L
2098.11:2005 method 10 for Plywood	
AS/NZS 1859.1:2004 - Particle Board, with use of testing	≤1.5 mg/L
procedure AS/NZS 4266.16:2004 method 16	
AS/NZS 1859.2:2004 - MDF, with use of testing	≤1mg/ L
procedure AS/NZS 4266.16:2004 method 16	
AS/NZS 4357.4 - Laminated Veneer Lumber (LVL)	≤1mg/ L
Japanese Agricultural Standard MAFF Notification	≤1mg/ L
No.701 Appendix	
Clause 3 (11) - LVL	
JIS A 5908:2003- Particle Board and Plywood, with use of	≤1mg/ L
testing procedure JIS A 1460	
JIS A 5905:2003 - MDF, with use of testing procedure JIS	≤1mg/ L
A 1460	
JIS A1901 (not applicable to Plywood, applicable to high	≤0.1 mg/m²hr
pressure	
laminates and compact laminates)	
ASTM D5116	≤0.1 mg/m²hr
(applicable to high pressure laminates and compact	
laminates)	
ISO 16000 part 9, 10 and 11 (also known as EN 13419),	≤0.1 mg/m²hr (at 3 days)
applicable to high pressure laminates and compact	
laminates	
ASTM D6007	≤0.12mg/m³**
ASTM E1333	≤0.12mg/m³***
EN 717-1 (also known as DIN EN 717-1)	≤0.12mg/m³
	≤0.12mg/m³

^{*}mg/m²hr may also be represented as mg/m²/hr.

^{**}The test report must confirm that the conditions of Table 3 comply for the particular wood product type, the final results must be presented in EN 717-1 equivalent (as presented in the table) using the correlation ratio of 0.98.

^{***}The final results must be presented in EN 717-1 equivalent (as presented in the table), using the correlation ratio of 0.98.



Appendix E: Water Fixtures

All fixtures are within one star of the WELS rating stated below:

ted below:
WELS Rating
6 Star
6 Star
5 Star
3 Star (> 4.5 but <= 6.0)**
5 Star
6 Star

^{**} The 3 star (>4.5 but <=6.0) requirement relates to Range F which is specified for both High Pressure and Low Pressure Showers as per Table 3.1 and Table 3.2 respectively of the AS NZS 6400-2016 Water Efficient Products standard. For showers, within one star of this Category F WELS rating means showers must be either 3 star (6.0 but <=7.5), 3 Star (> 4.5 but <= 6.0), 4 Star (>6.0 but <=7.5) or 4 Star (> 4.5 but <= 6.0).

Appendix F: Table 26.2 Pollution Reduction Targets

Pollutant	Reduction Target (% of the typical urban annual load)		
	Α	В	С
Total Suspended Solids (TSS)1	80%	80%	90%
Gross Pollutants	85%	90%	95%
Total Nitrogen (TN)2	30%	45%	60%
Total Phosphorus (TP)2	30%	60%	70%
Total Petroleum Hydrocarbons3	60%	90%	90%
Free Oils3	90%	90%	98%

Notes:

- 1. Load based on the following particulate size distribution (by mass): $20\% < 20 \mu m$; $20\% 60-150 \mu m$; $20\% 150-400 \mu m$; $20\% 400-2000 \mu m$.
- 2. Load includes particulate and dissolved fraction.
- 3. This requirement is not applicable where the site contains less than a total of 200m2 of uncovered areas where vehicles are likely to transit and/or park e.g. roads, loading docks, refuelling bays, car parking etc.