

# PORT COOGEE

PROPOSED 6 STOREY + ROOFTOP TERRACE  
RESIDENTIAL PROJECT FOR  
OPEN CORPORATION



Drawing List	
NUMBER	DRAWING NAME
DA000	TITLE PAGE + LOCALITY MAP
DA001	SITE ANALYSIS + CONTEXT PLAN
DA002	Site plan
DA100	Basement Floor Plan
DA101	Level 1 Plan
DA102	Typical Floor Plan Levels 2-5
DA103	Roof Plan
DA200	West Elevation - Brunswick Street
DA201	South Elevation
DA202	East Elevation - Comet Lane
DA203	North Elevation
DA300	A-A Building Section
DA301	B-B Building Section
DA302	C-C Building Section
DA900	Perspective - Brunswick St
DA901	Perspective - Brunswick St Context
DA902	Perspective - Comet Lane
DA903	Comet Lane & Rooftop Perspectives
DA907	Shadow Diagram - 9am/12pm/3pm

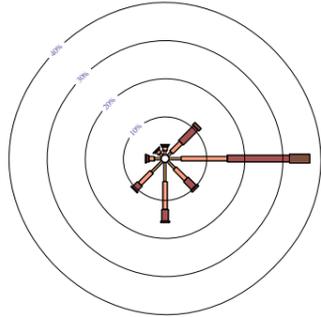


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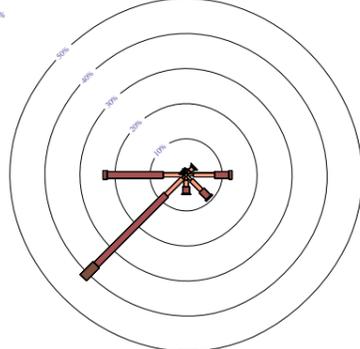


Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue	
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	TITLE PAGE + LOCALITY MAP	DA000	A	
A	DRAFT DA SUBMISSION	30/09/2014									
									Scale @A1	Drawing Size A1	
									Project No. 14052	Date 09/14	Drawn By DD & RW
									<small>Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©</small>		
									<small>CAD Reference C:\Users\DDemirew\Documents\140527_DA_PortCoogee_DDemirew.rvt</small>		

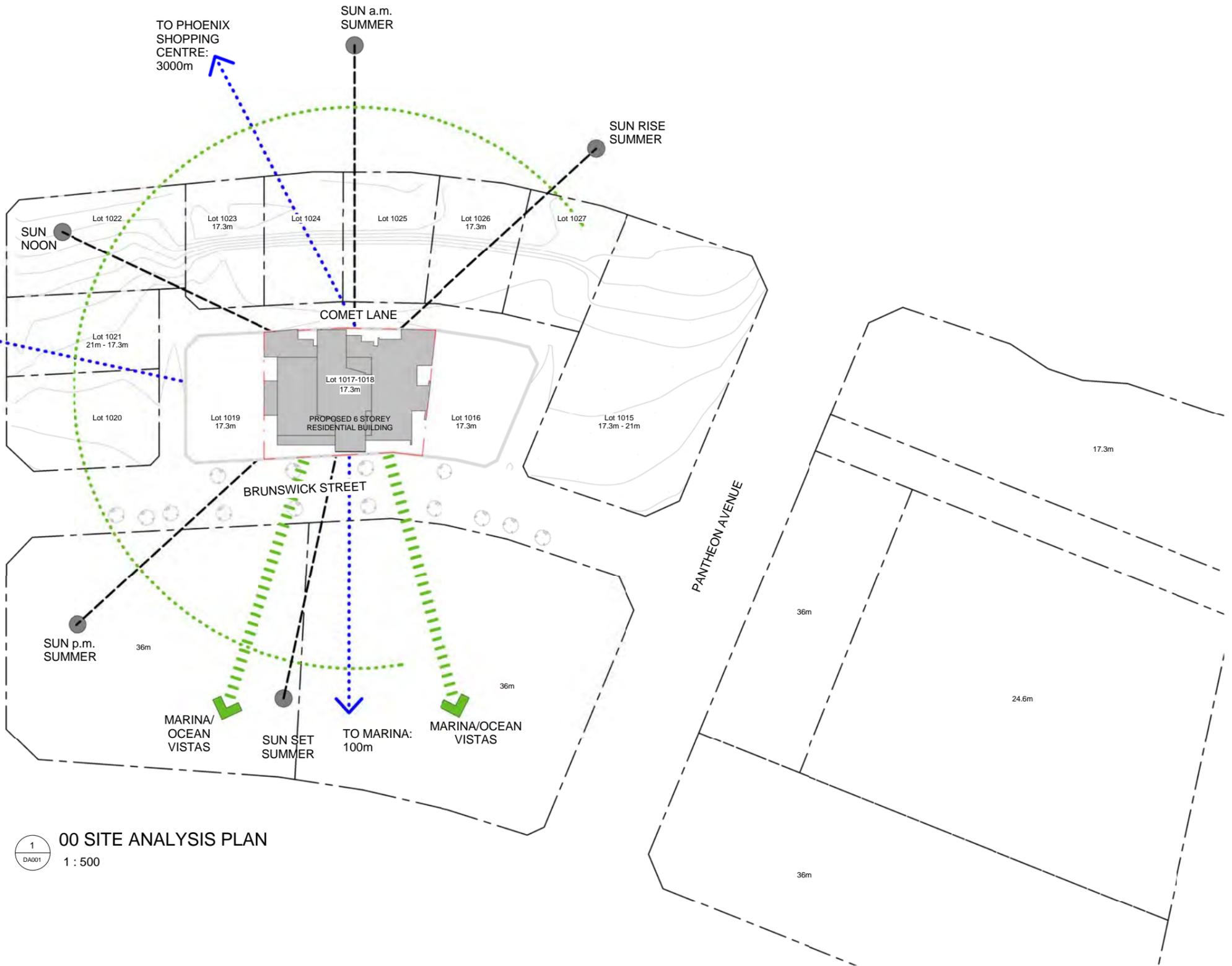
WIND FREQUENCY ANALYSIS (in km/h)  
 PERTH AIRPORT STATION NUMBER 009021  
 Latitude: -31.93 Longitude: 115.98  
 9 am Jan  
 1859 Total Observations (1944 to 2004)  
 Calm 5%



WIND FREQUENCY ANALYSIS (in km/h)  
 PERTH AIRPORT STATION NUMBER 009021  
 Latitude: -31.93 Longitude: 115.98  
 3 pm Jan  
 1855 Total Observations (1944 to 2004)  
 Calm 2%



2 WIND ROSE\_dwg  
 DA001 1 : 40



1 00 SITE ANALYSIS PLAN  
 DA001 1 : 500

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Issue	Description	Date	Issue	Description	Date	Client
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation
A	DRAFT DA SUBMISSION	30/09/2014				

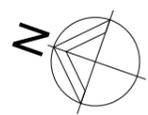
Project  
 PORT COOGEE  
 LOT 1017-1018 BRUNSWICK STREET  
 PORT COOGEE  
 WESTERN AUSTRALIA

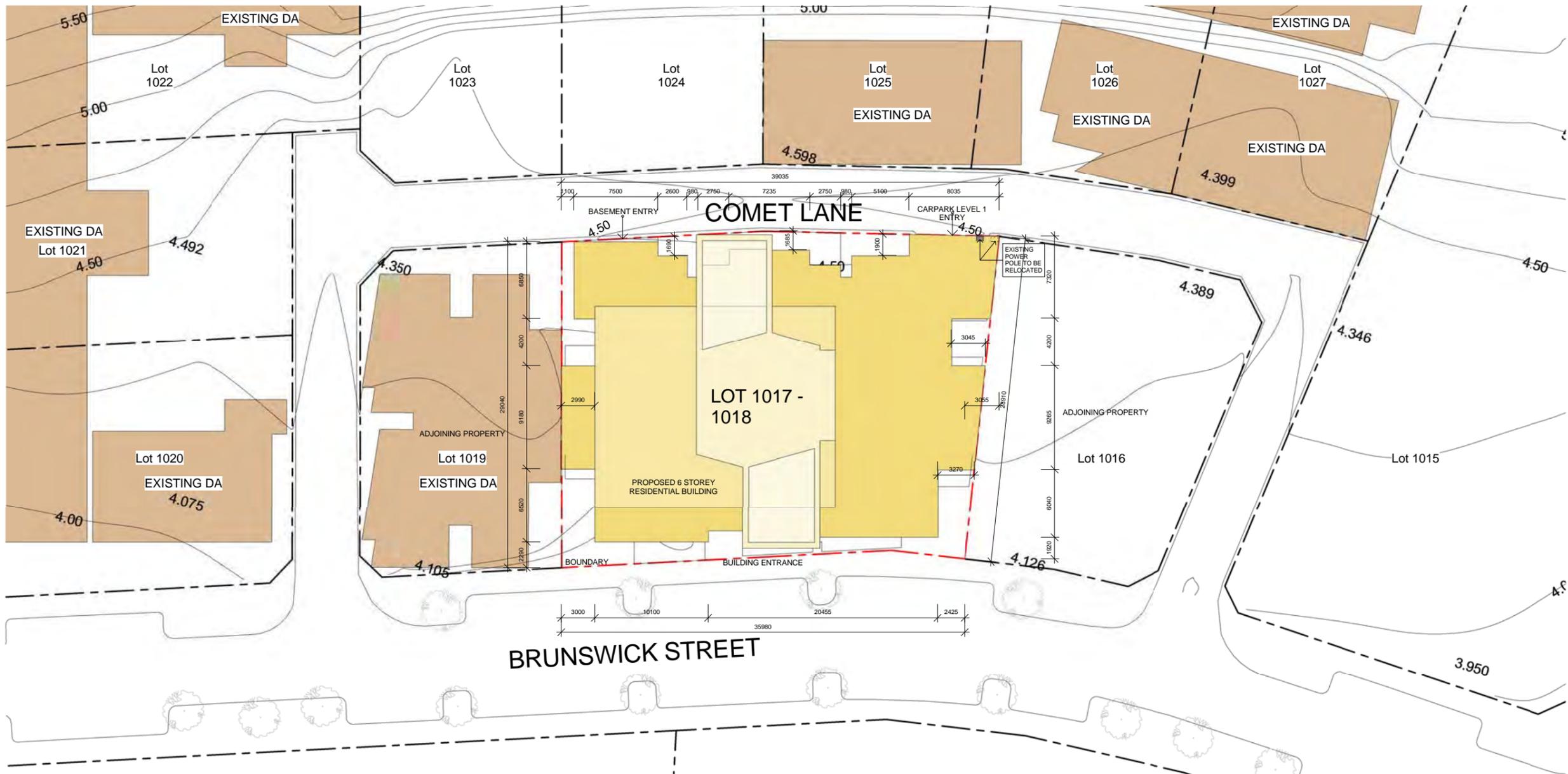
Title  
 SITE ANALYSIS + CONTEXT  
 PLAN

Drawing No.	Title	Issue
DA001	SITE ANALYSIS + CONTEXT PLAN	A
Scale	Drawing Size	
As indicated@A1	A1	
Project No.	Date	Drawn By
14052	09/14	DD & RW

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CAD Reference  
 C:\Users\DDemireou\Documents\140527\_Da\_PortCoogee\_DDemireou.rvt





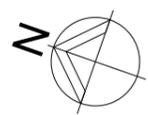
Room Area Schedule		
Number	Gross Internal Area	No. of Bedrooms
<b>LEVEL 01</b>		
1-01	85 m <sup>2</sup>	3
1-02	83 m <sup>2</sup>	3
<b>LEVEL 02</b>		
2-01	65 m <sup>2</sup>	2
2-02	65 m <sup>2</sup>	2
2-03	79 m <sup>2</sup>	2
2-04	79 m <sup>2</sup>	2
2-05	78 m <sup>2</sup>	2
2-06	85 m <sup>2</sup>	2
2-07	56 m <sup>2</sup>	1
2-08	59 m <sup>2</sup>	1
2-09	75 m <sup>2</sup>	2
TOTAL ROOMS 38 Total Room Area: 2726m <sup>2</sup>		
Level 2 Typical to Level 3,4 & 5		

Area Schedule	
SITE AREA	1081m <sup>2</sup>
SITE COVERAGE	88%
SITE PERMEABILITY	3%
<b>BASEMENT</b>	
LEVEL 1 CAR PARK	896m <sup>2</sup>
TOTAL ROOMS AREA	548m <sup>2</sup>
ROOF DECK	2726m <sup>2</sup>
	385m <sup>2</sup>

PRELIMINARY

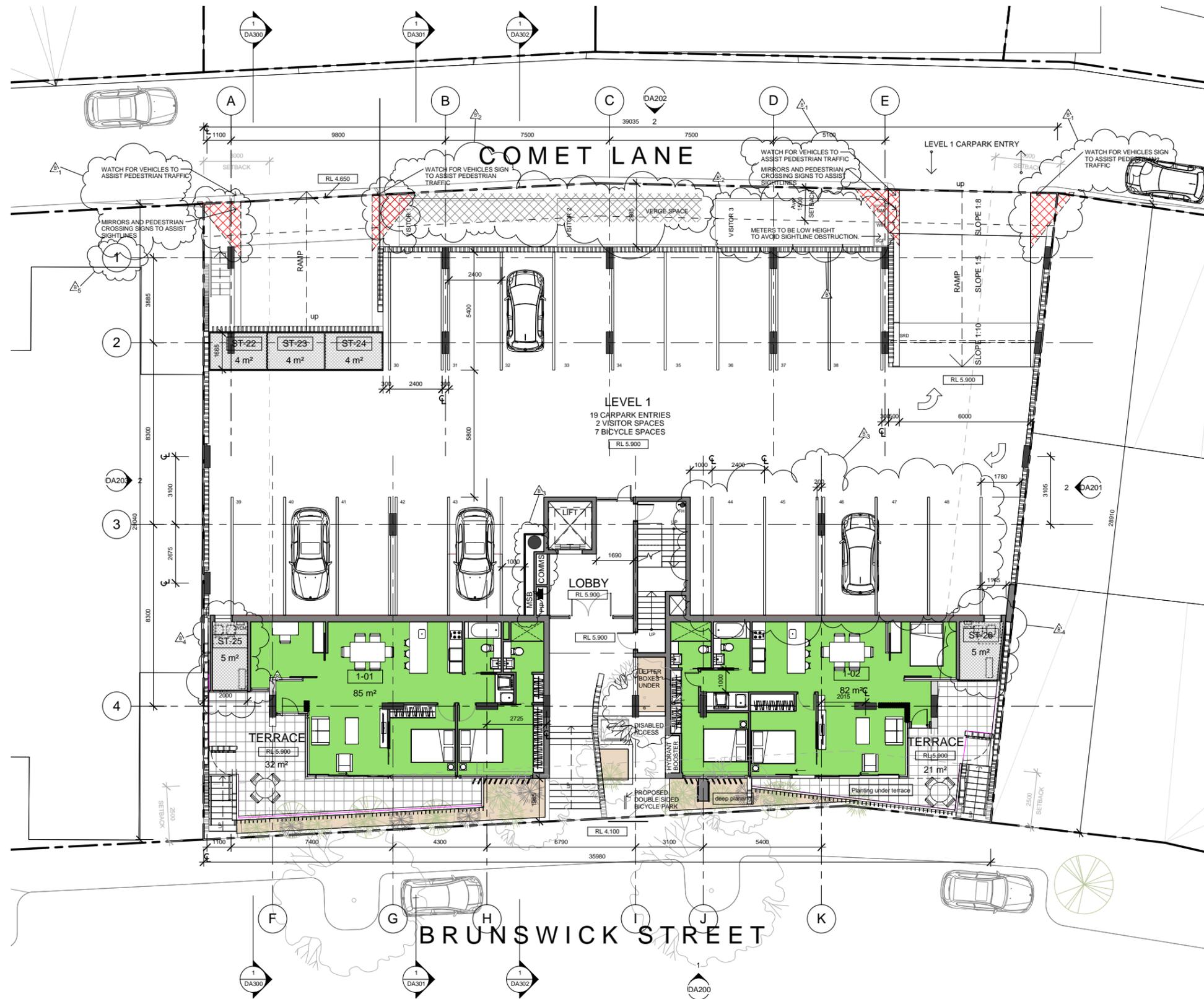


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1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	SITE PLAN	DA002	A
A	DRAFT DA SUBMISSION	30/09/2014							Scale 1:200@A1	Drawing Size A1
									Project No. 14052	Date 09/14
										Drawn By DD & RW



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CAD Reference  
C:\User\DDemirou\Documents\14052\_Draft\PortCoogee\_DDemirou.rvt





Parking Schedule		
Level	Description	Total Count
BASEMENT	5400 x 2400	29
Street Level	Parallel 2100x6600	3
LEVEL 01	5400 x 2400	19
51		

Bicycle Parking Schedule		
Type Comments	Bikes provided	Level
DOUBLE SIDED BICYCLE RACK	2	Street level

STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m <sup>2</sup>	20	STORAGE
BASEMENT	5 m <sup>2</sup>	1	STORAGE
LEVEL 01	4 m <sup>2</sup>	3	STORAGE
LEVEL 01	5 m <sup>2</sup>	2	STORAGE
LEVEL 02	4 m <sup>2</sup>	2	STORAGE
LEVEL 02	6 m <sup>2</sup>	1	STORAGE
LEVEL 03	4 m <sup>2</sup>	2	STORAGE
LEVEL 03	6 m <sup>2</sup>	1	STORAGE
LEVEL 04	4 m <sup>2</sup>	2	STORAGE
LEVEL 04	6 m <sup>2</sup>	1	STORAGE
LEVEL 05	4 m <sup>2</sup>	2	STORAGE
LEVEL 05	6 m <sup>2</sup>	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m <sup>2</sup>	3
1-02	LEVEL 01	82 m <sup>2</sup>	3
2-01	LEVEL 02	65 m <sup>2</sup>	2
2-02	LEVEL 02	65 m <sup>2</sup>	2
2-03	LEVEL 02	79 m <sup>2</sup>	2
2-04	LEVEL 02	79 m <sup>2</sup>	2
2-05	LEVEL 02	78 m <sup>2</sup>	2
2-06	LEVEL 02	85 m <sup>2</sup>	2
2-07	LEVEL 02	56 m <sup>2</sup>	1
2-08	LEVEL 02	59 m <sup>2</sup>	1
2-09	LEVEL 02	75 m <sup>2</sup>	2

Level 2 Typical to Level 3,4 & 5

LEGEND	
ST	STORAGE
COMMS-	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
SRD	SECTIONAL ROLLER DOOR
SD	STRIP DRAIN
	CONDENSING UNIT
	BICYCLE RACK (SINGLE SIDED PARKING RAIL)
	RISER

REVISIONS	
1	OBSTRUCTIONS CLEARED FROM SIGHTLINES MIRRORS AND SIGNAGE ADDED TO IMPROVE PEDESTRIAN & VEHICULAR AWARENESS
2	PEDESTRIAN & VEHICULAR AWARENESS
3	CARPAKING LAYOUT ADJUSTED
4	STORAGE SPACE INCREASED TO INCLUDE INDIVIDUAL BIN STORAGE SPACE
5	GRIDLINE A RELOCATED TO WEST

DEVELOPMENT APPLICATION  
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Issue	Description	Date	Issue	Description	Date	Client
1	PRE-LODGING MEETING	12/08/2014				Open Corporation
2	REVISED LAYOUT/ROOF TOP	28/08/2014				
A	DRAFT DA SUBMISSION	30/09/2014				
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014				

Project  
PORT COOGEE  
LOT 1017-1018 BRUNSWICK STREET  
PORT COOGEE  
WESTERN AUSTRALIA

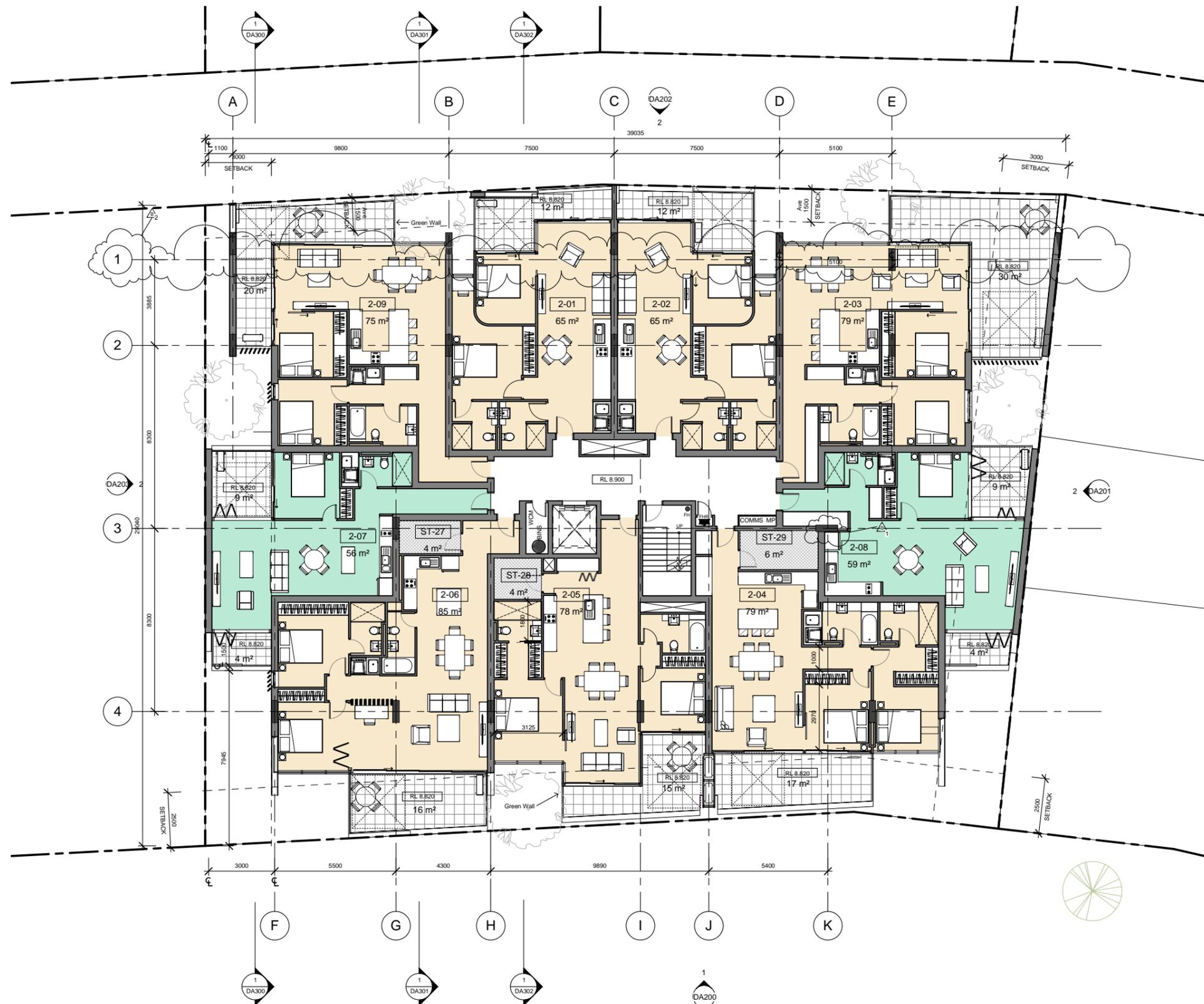
Title  
LEVEL 1 PLAN

Drawing No.	Issue
DA101	B
Scale	Drawing Size
1:100@A1	A1
1:200@A3	A1
Project No.	Drawn By
14052	DD & RW
Date	
11/14	

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CAD Reference  
C:\Users\DDemeterou\Documents\14052\_DA\_PortCoogee\_revised\_traffic & Waste Management\_DDemeterou





STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m²	20	STORAGE
BASEMENT	5 m²	1	STORAGE
LEVEL 01	4 m²	3	STORAGE
LEVEL 01	5 m²	2	STORAGE
LEVEL 02	4 m²	2	STORAGE
LEVEL 02	6 m²	1	STORAGE
LEVEL 03	4 m²	2	STORAGE
LEVEL 03	6 m²	1	STORAGE
LEVEL 04	4 m²	2	STORAGE
LEVEL 04	6 m²	1	STORAGE
LEVEL 05	4 m²	2	STORAGE
LEVEL 05	6 m²	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m²	3
1-02	LEVEL 01	82 m²	3
2-01	LEVEL 02	65 m²	2
2-02	LEVEL 02	65 m²	2
2-03	LEVEL 02	79 m²	2
2-04	LEVEL 02	79 m²	2
2-05	LEVEL 02	78 m²	2
2-06	LEVEL 02	85 m²	2
2-07	LEVEL 02	56 m²	1
2-08	LEVEL 02	59 m²	1
2-09	LEVEL 02	75 m²	2

LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS-	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
SRD	SECTIONAL ROLLER DOOR
SD	STRIP DRAIN
[Symbol]	CONDENSING UNIT
[Symbol]	BICYCLE RACK (SINGLE SIDED PARKING RAIL)
[Symbol]	RISER

REVISIONS	
1	COLUMN MOVED
2	GRIDLINE A RELOCATED TO WEST

DEVELOPMENT APPLICATION  
ONLY NOT FOR CONSTRUCTION



Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGING MEETING	12/08/2014	1	Open Corporation		Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	LEVEL 2 PLAN	DA102	B
2	REVISED LAYOUT/ROOF TOP	28/08/2014								
A	DRAFT DA SUBMISSION	30/09/2014								
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014								

Scale	Date	Drawing Size
1:100@A1	11/14	A1
1:200@A3		

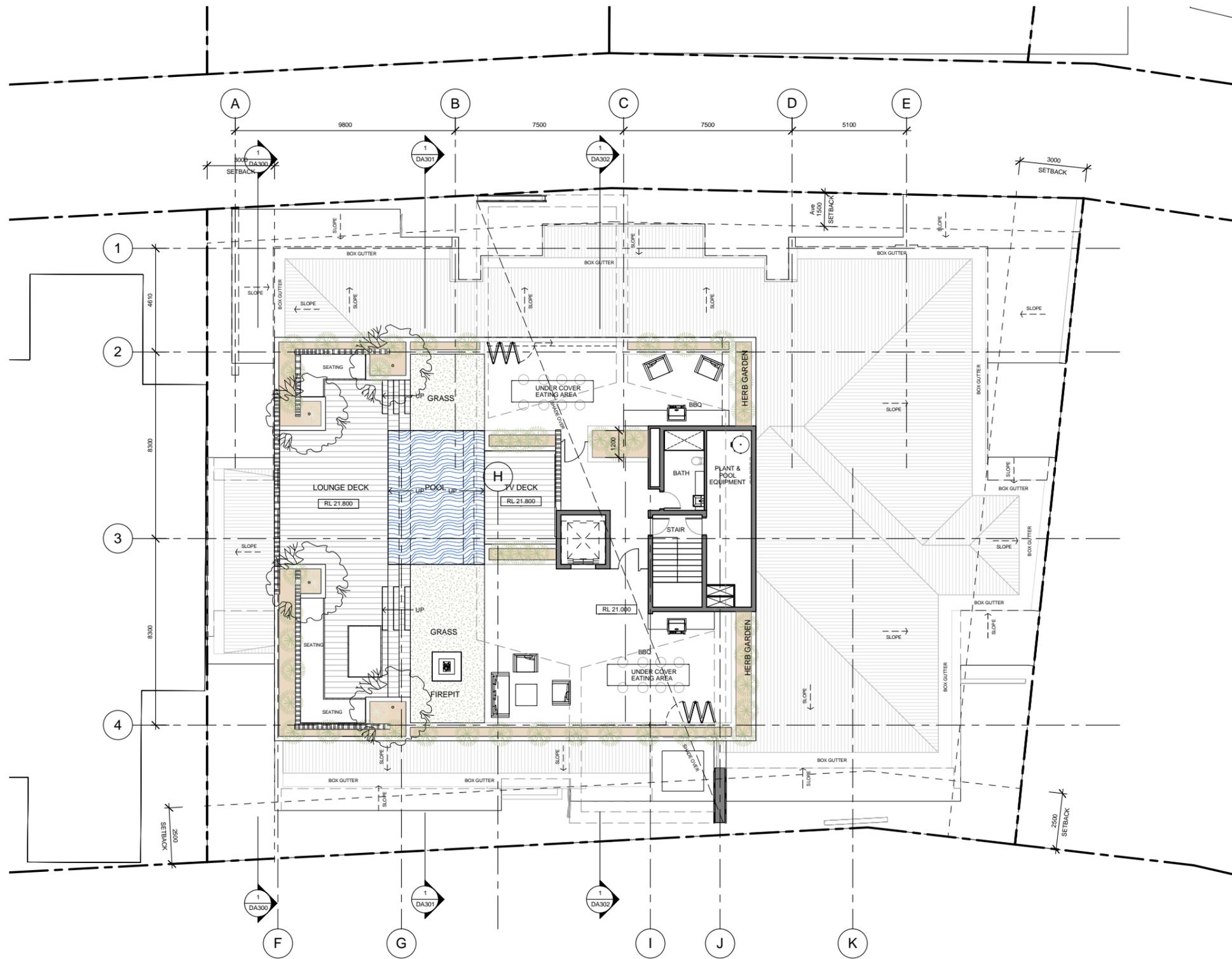
  

Project No.	Date	Drawn By
14052	11/14	DD & RW

CAD Reference
C:\Users\DDemeterov\Documents\14052_Plan_Coogee_revised_traffic_and_waste_management_DDemeterov.rvt

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Issue	Description	Date
1	PRE-LODGEEMENT MEETING	12/08/2014
2	REVISED LAYOUT/ROOF TOP	28/08/2014
A	DRAFT DA SUBMISSION	30/09/2014

Issue	Description	Date	Client
1	DA300		Open Corporation
1	DA301		
1	DA302		

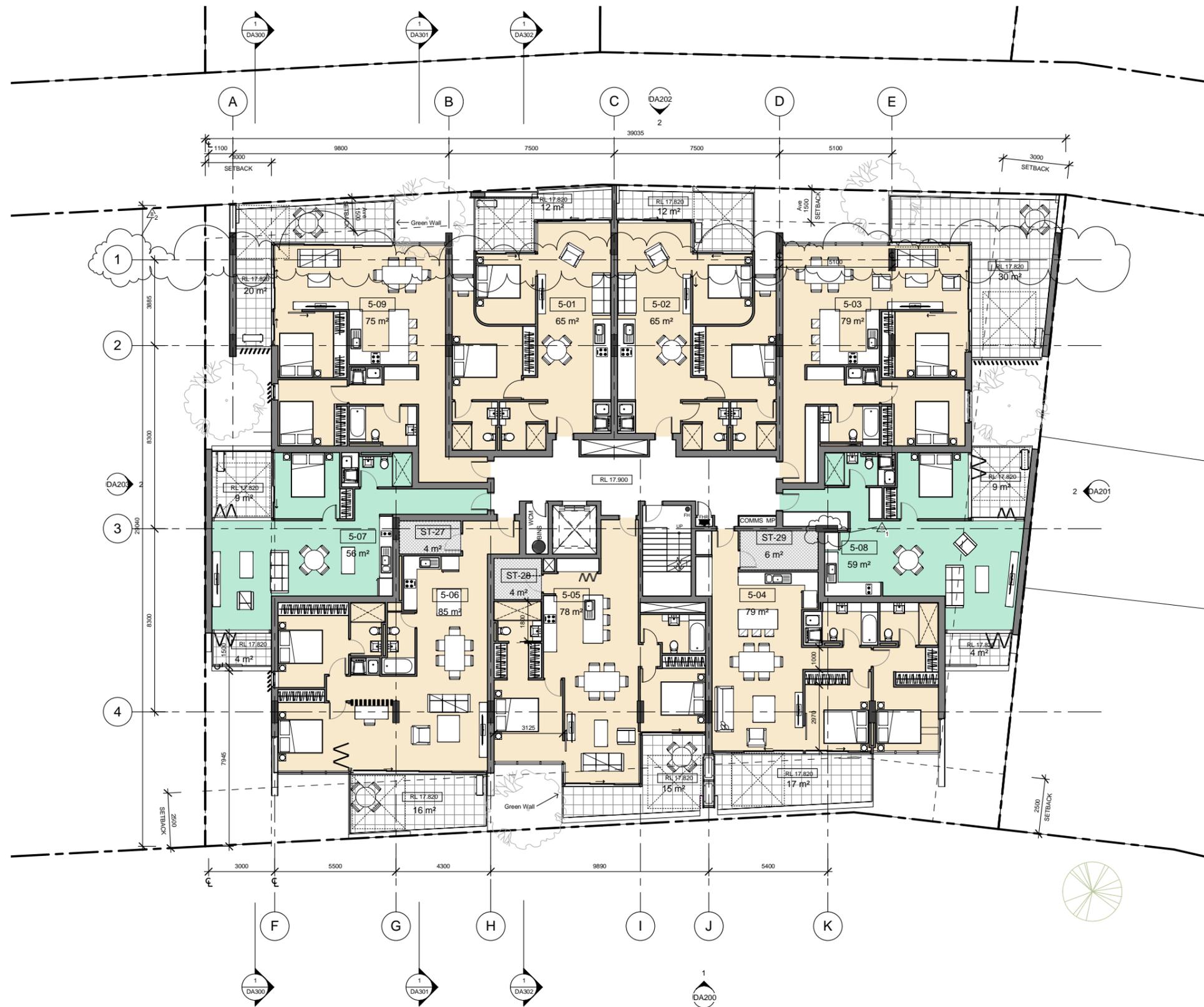
**Project**  
 PORT COOGEE  
 LOT 1017-1018 BRUNSWICK STREET  
 PORT COOGEE  
 WESTERN AUSTRALIA

**Title**  
 ROOF PLAN

Drawing No.	Title	Issue
DA106	ROOF PLAN	A
Scale	Drawing Size	Drawn By
1:100@A1	A1	DD & RW
Project No.	Date	CAD Reference
14052	09/14	C:\User\DD\Documents\14052_Drafting\14052_Drafting.rvt

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STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m <sup>2</sup>	20	STORAGE
BASEMENT	5 m <sup>2</sup>	1	STORAGE
LEVEL 01	4 m <sup>2</sup>	3	STORAGE
LEVEL 01	5 m <sup>2</sup>	2	STORAGE
LEVEL 02	4 m <sup>2</sup>	2	STORAGE
LEVEL 02	6 m <sup>2</sup>	1	STORAGE
LEVEL 03	4 m <sup>2</sup>	2	STORAGE
LEVEL 03	6 m <sup>2</sup>	1	STORAGE
LEVEL 04	4 m <sup>2</sup>	2	STORAGE
LEVEL 04	6 m <sup>2</sup>	1	STORAGE
LEVEL 05	4 m <sup>2</sup>	2	STORAGE
LEVEL 05	6 m <sup>2</sup>	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m <sup>2</sup>	3
1-02	LEVEL 01	82 m <sup>2</sup>	3
2-01	LEVEL 02	65 m <sup>2</sup>	2
2-02	LEVEL 02	65 m <sup>2</sup>	2
2-03	LEVEL 02	79 m <sup>2</sup>	2
2-04	LEVEL 02	79 m <sup>2</sup>	2
2-05	LEVEL 02	78 m <sup>2</sup>	2
2-06	LEVEL 02	85 m <sup>2</sup>	2
2-07	LEVEL 02	56 m <sup>2</sup>	1
2-08	LEVEL 02	59 m <sup>2</sup>	1
2-09	LEVEL 02	75 m <sup>2</sup>	2

LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS-	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
SRD	SECTIONAL ROLLER DOOR
SD	STRIP DRAIN
	CONDENSING UNIT
	BICYCLE RACK (SINGLE SIDED PARKING RAIL)
	RISER

REVISIONS	
1	COLUMN MOVED
2	GRIDLINE A RELOCATED TO WEST

DEVELOPMENT APPLICATION  
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Issue	Description	Date
1	PRE-LODGING MEETING	12/08/2014
2	REVISED LAYOUT/ROOF TOP	28/08/2014
A	DRAFT DA SUBMISSION	30/09/2014
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014

Issue	Description	Date
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Client  
Open Corporation

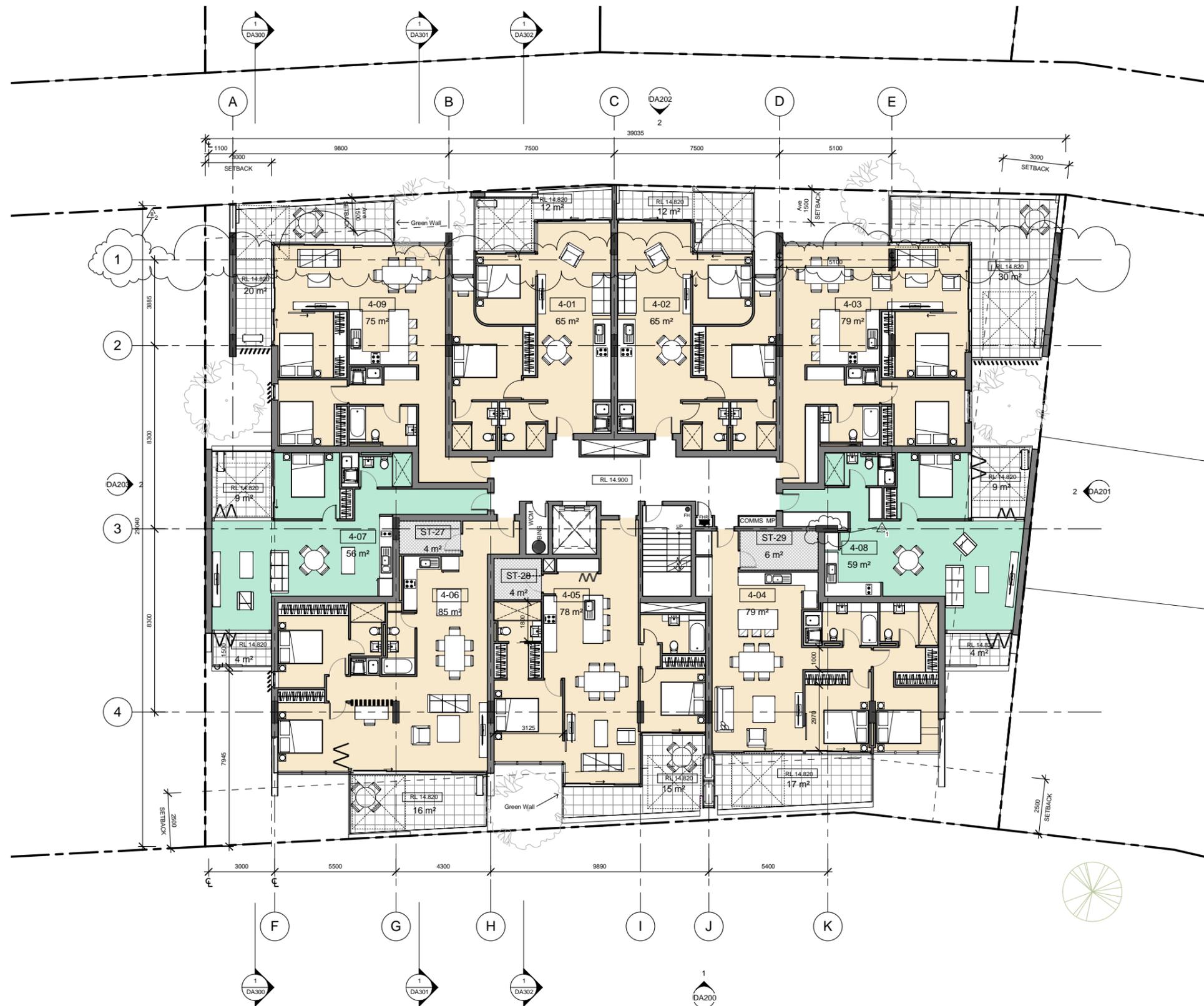
Project  
PORT COOGEE  
LOT 1017-1018 BRUNSWICK STREET  
PORT COOGEE  
WESTERN AUSTRALIA

Title  
LEVEL 5 PLAN

Drawing No.	Title	Issue
DA105	LEVEL 5 PLAN	B
Scale	Drawing Size	Drawn By
1:100@A1	A1	DD & RW
Project No.	Date	Drawn By
14052	11/14	DD & RW

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CAD Reference  
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STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m <sup>2</sup>	20	STORAGE
BASEMENT	5 m <sup>2</sup>	1	STORAGE
LEVEL 01	4 m <sup>2</sup>	3	STORAGE
LEVEL 01	5 m <sup>2</sup>	2	STORAGE
LEVEL 02	4 m <sup>2</sup>	2	STORAGE
LEVEL 02	6 m <sup>2</sup>	1	STORAGE
LEVEL 03	4 m <sup>2</sup>	2	STORAGE
LEVEL 03	6 m <sup>2</sup>	1	STORAGE
LEVEL 04	4 m <sup>2</sup>	2	STORAGE
LEVEL 04	6 m <sup>2</sup>	1	STORAGE
LEVEL 05	4 m <sup>2</sup>	2	STORAGE
LEVEL 05	6 m <sup>2</sup>	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
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1-01	LEVEL 01	85 m <sup>2</sup>	3
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2-03	LEVEL 02	79 m <sup>2</sup>	2
2-04	LEVEL 02	79 m <sup>2</sup>	2
2-05	LEVEL 02	78 m <sup>2</sup>	2
2-06	LEVEL 02	85 m <sup>2</sup>	2
2-07	LEVEL 02	56 m <sup>2</sup>	1
2-08	LEVEL 02	59 m <sup>2</sup>	1
2-09	LEVEL 02	75 m <sup>2</sup>	2

LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS-	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
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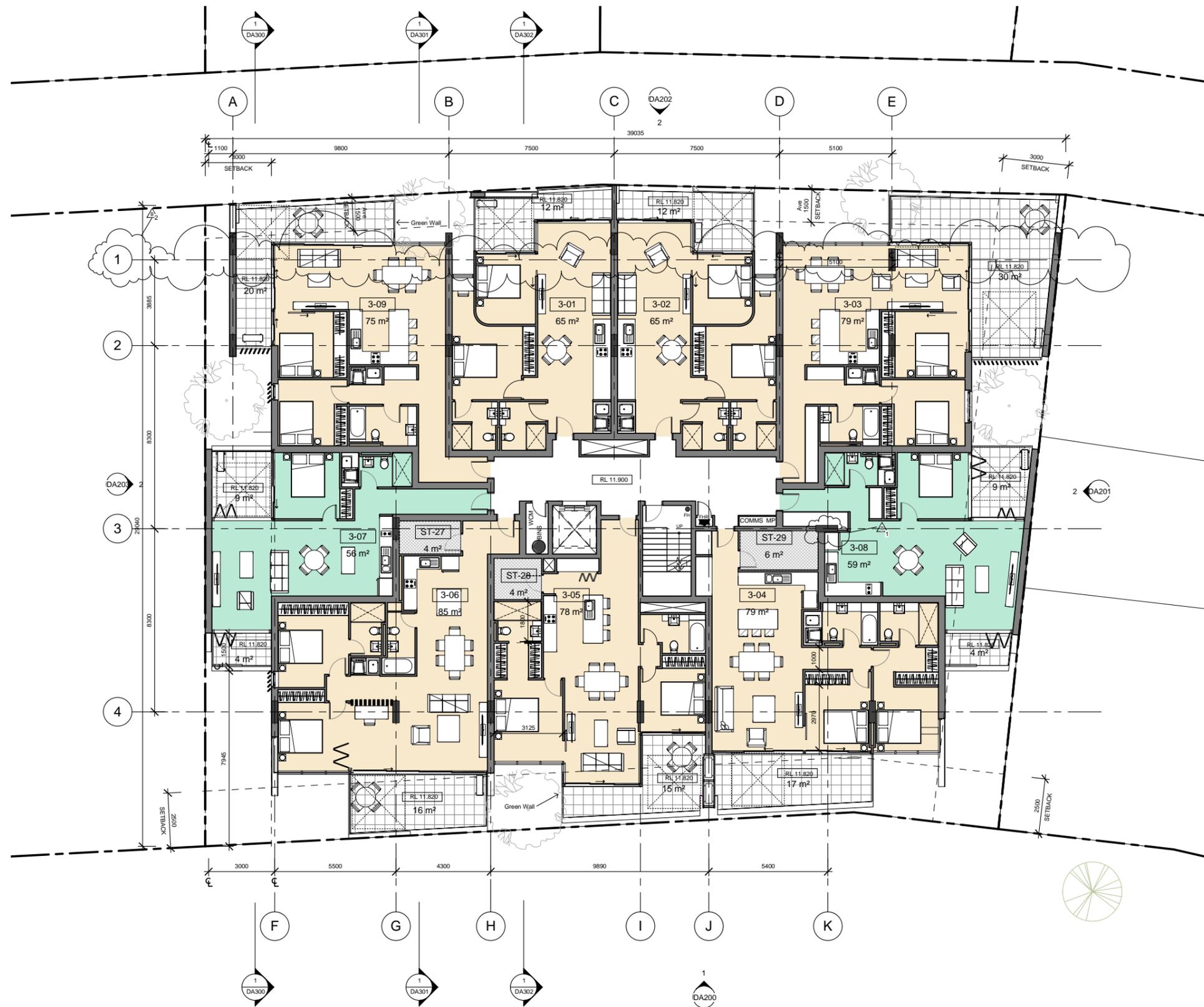
Project  
PORT COOGEE  
LOT 1017-1018 BRUNSWICK STREET  
PORT COOGEE  
WESTERN AUSTRALIA

Title  
LEVEL 4 PLAN

Drawing No.	Title	Issue
DA104	LEVEL 4 PLAN	B
Scale	Date	Drawing Size
1:100@A1	11/14	A1
Project No.	Date	Drawn By
14052	11/14	DD & RW

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STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m²	20	STORAGE
BASEMENT	5 m²	1	STORAGE
LEVEL 01	4 m²	3	STORAGE
LEVEL 01	5 m²	2	STORAGE
LEVEL 02	4 m²	2	STORAGE
LEVEL 02	6 m²	1	STORAGE
LEVEL 03	4 m²	2	STORAGE
LEVEL 03	6 m²	1	STORAGE
LEVEL 04	4 m²	2	STORAGE
LEVEL 04	6 m²	1	STORAGE
LEVEL 05	4 m²	2	STORAGE
LEVEL 05	6 m²	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m²	3
1-02	LEVEL 01	82 m²	3
2-01	LEVEL 02	65 m²	2
2-02	LEVEL 02	65 m²	2
2-03	LEVEL 02	79 m²	2
2-04	LEVEL 02	79 m²	2
2-05	LEVEL 02	78 m²	2
2-06	LEVEL 02	85 m²	2
2-07	LEVEL 02	56 m²	1
2-08	LEVEL 02	59 m²	1
2-09	LEVEL 02	75 m²	2

LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS-	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
SRD	SECTIONAL ROLLER DOOR
SD	STRIP DRAIN
	CONDENSING UNIT
	BICYCLE RACK (SINGLE SIDED PARKING RAIL)
	RISER

REVISIONS	
1	COLUMN MOVED
2	GRIDLINE A RELOCATED TO WEST

DEVELOPMENT APPLICATION  
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Issue	Description	Date	Issue	Description	Date	Client
1	PRE-LODGEING MEETING	12/08/2014				Open Corporation
2	REVISED LAYOUT/ROOF TOP	28/08/2014				
A	DRAFT DA SUBMISSION	30/09/2014				
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014				

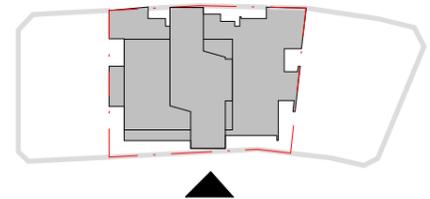
Project  
PORT COOGEE  
LOT 1017-1018 BRUNSWICK STREET  
PORT COOGEE  
WESTERN AUSTRALIA

Title  
LEVEL 3 PLAN

Drawing No.	Title	Issue
DA103	LEVEL 3 PLAN	B
Scale	Date	Drawn By
1:100@A1	11/14	DD & RW
Project No.	Date	Drawn By
14052	11/14	DD & RW

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FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

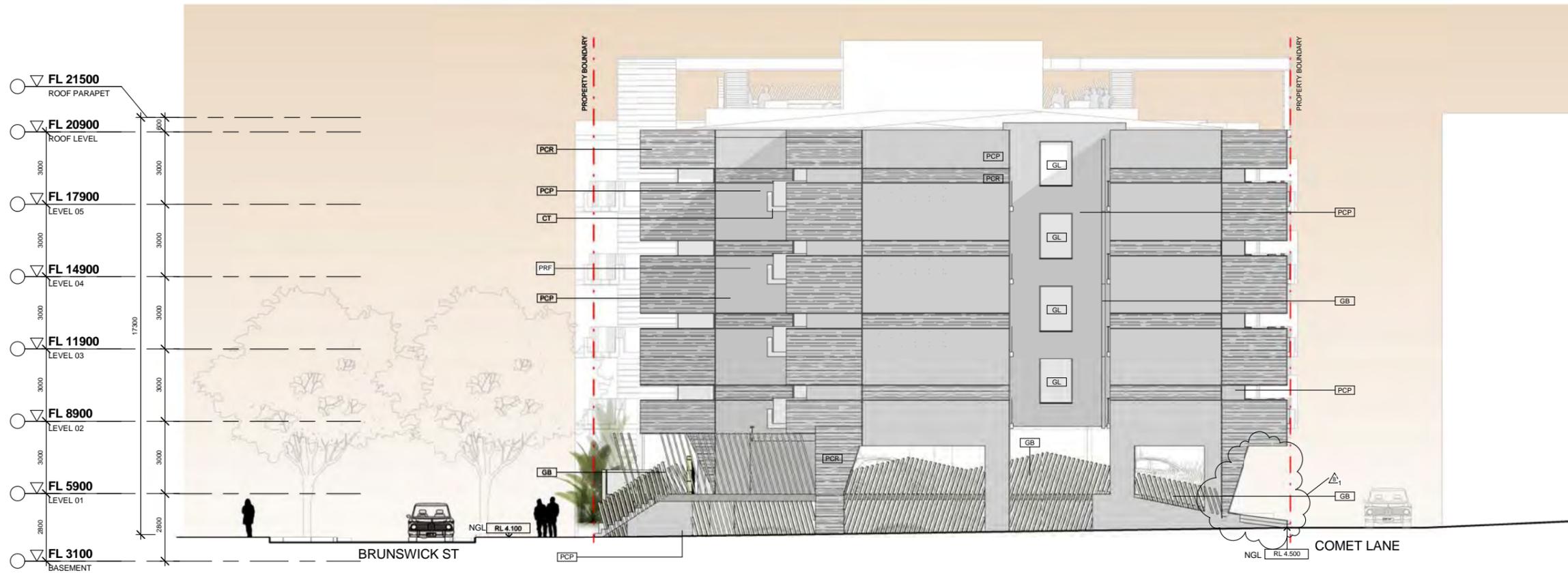
PRELIMINARY



Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	WEST ELEVATION - BRUNSWICK STREET	DA200	A
A	DRAFT DA SUBMISSION	30/09/2014							Scale 1:100 @A1 1:200 @ A3	Drawing Size A1
									Project No. 14052	Date 09/14
										Drawn By DD & RW

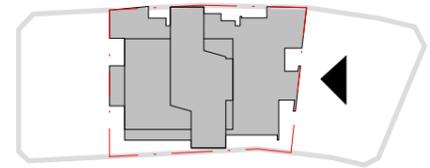
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REVISIONS
1 - PEDESTRIAN/VEHICULAR SIGHTLINES IMPROVED

2  
DA201  
Elevation 3 - South  
1:100



FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

DEVELOPMENT APPLICATION  
ONLY NOT FOR CONSTRUCTION



Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue	
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	SOUTH ELEVATION	DA201	B	
A	DRAFT DA SUBMISSION	30/09/2014							As indicated@A1:200@A3	Drawing Size A1	
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014							Project No. 14052	Date 11/14	Drawn By DD & RW

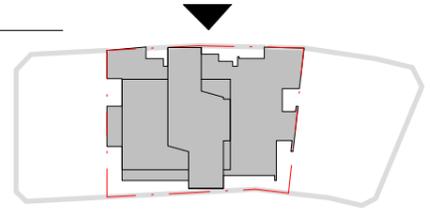
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REVISIONS	
1	PEDESTRIAN/VEHICULAR SIGHTLINES IMPROVED
2	COMET LANE LANDSCAPING REMOVED TO IMPROVE SIGHTLINES

2 Elevation 2 - East (Comet Lane)  
DA202 1:100



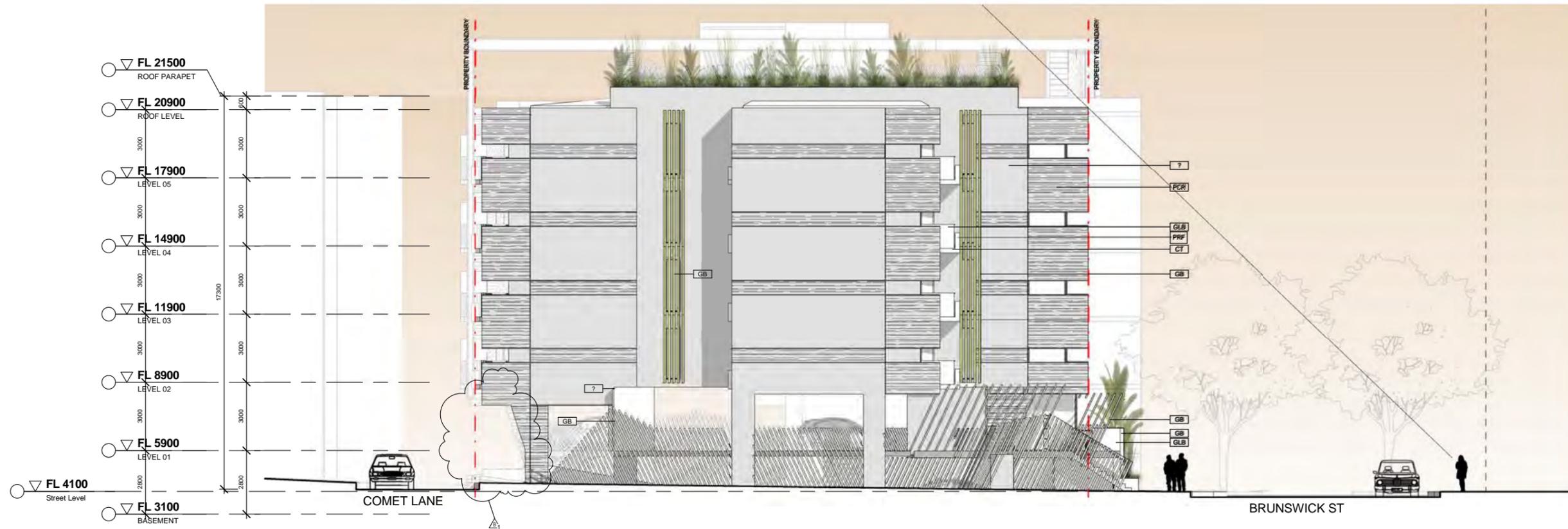
FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

DEVELOPMENT APPLICATION  
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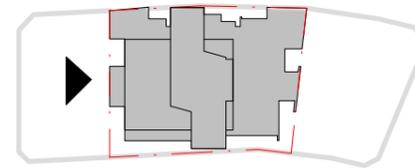


Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	EAST ELEVATION - COMENT LANE	DA202	B
A	DRAFT DA SUBMISSION	30/09/2014							As indicated@A1:200@A3	Drawing Size A1
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014							Project No. 14052	Date 11/14
										Drawn By DD & RW

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2 Elevation 4 - North  
DA203 1:100



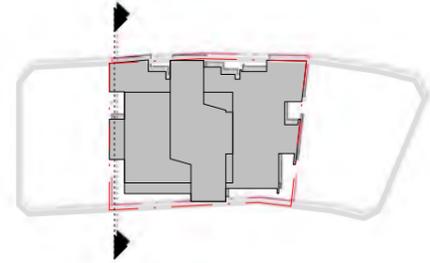
FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
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PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

DEVELOPMENT APPLICATION  
ONLY NOT FOR CONSTRUCTION



Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	NORTH ELEVATION	DA203	B
A	DRAFT DA SUBMISSION	30/09/2014							As indicated@A1:200@A3	Drawing Size A1
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014							Project No. 14052	Date 11/14
										Drawn By DD & RW

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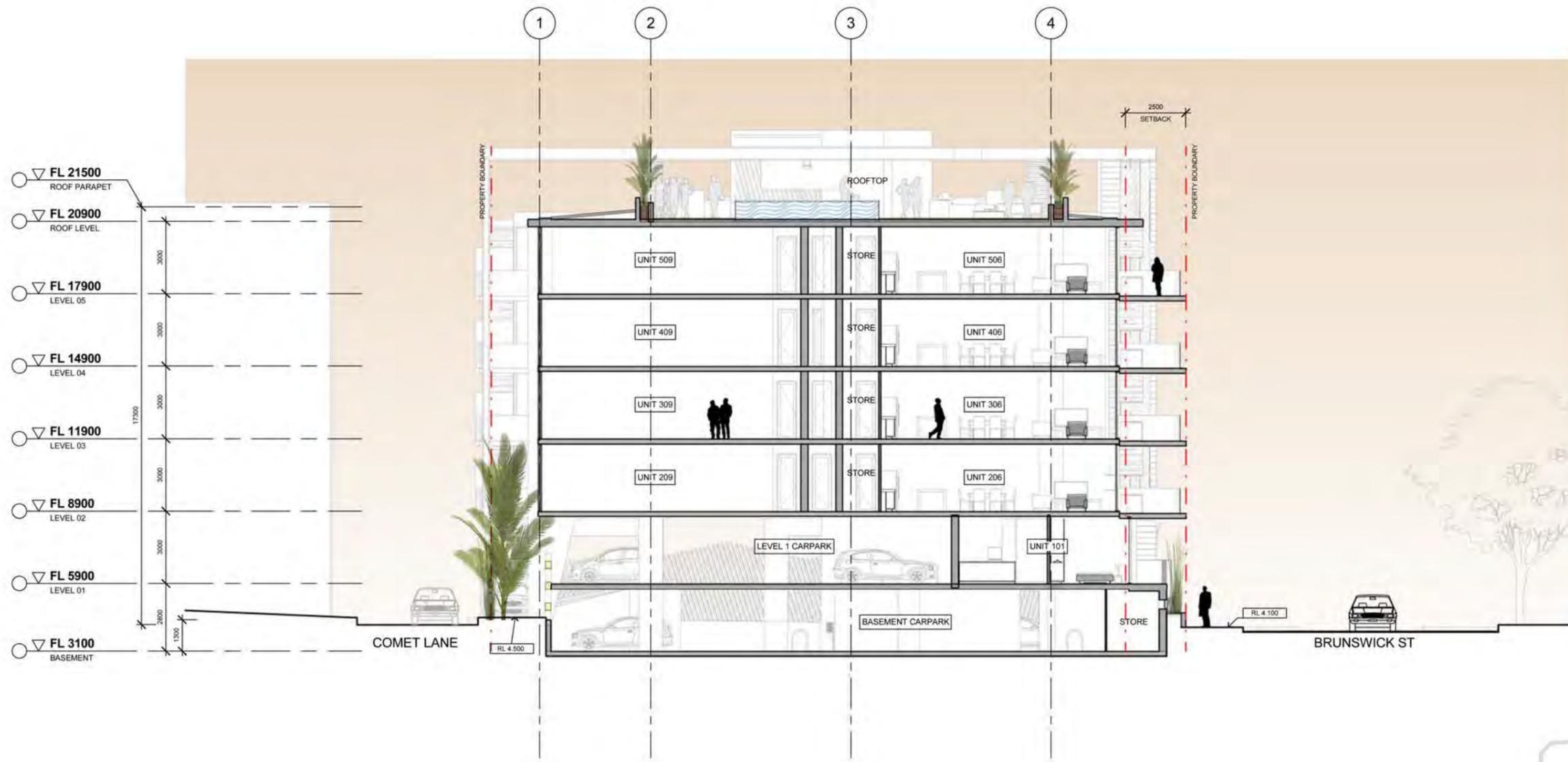


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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	A-A BUILDING SECTION	DA300	A
A	DRAFT DA SUBMISSION	30/09/2014							Scale 1:100 @A1 1:200 @ A3	Drawing Size A1
									Project No. 14052	Date 09/14
										Drawn By DD & RW

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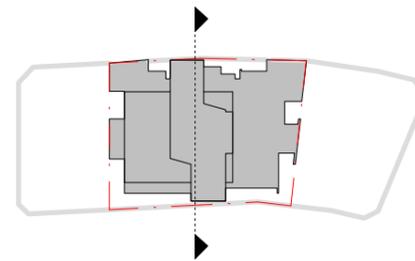
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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	B-B BUILDING SECTION	DA301	A
A	DRAFT DA SUBMISSION	30/09/2014							Scale 1:100 @A1 1:200 @ A3	Drawing Size A1
									Project No. 14052	Date 09/14
										Drawn By DD & RW

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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue	Drawing Size
A	DRAFT DA SUBMISSION	30/09/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	C-C BUILDING SECTION	DA302	A	A1
P									Scale 1:100 @A1 1:200 @ A3		
									Project No. 14052	Date 09/14	Drawn By DD & RW
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd © CAD Reference <small>C:\Users\DDemirou\Documents\14052_Drafting\PortCoogee_DDemirou.rvt</small>		



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	PERSPECTIVE - BRUNSWICK ST	DA900	A
A	DRAFT DA SUBMISSION	30/09/2014							@A1	Drawing Size A1
									Project No. 14052	Date 09/14
										Drawn By DD & RW

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2 Perspective - Sky North West  
A901

1 Perspective - Entry level  
A901



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue	
A	DRAFT DA SUBMISSION	30/09/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	PERSPECTIVE - BRUNSWICK ST CONTEXT	DA901	A	
									Scale @A1	Drawing Size A1	
									Project No. 14052	Date 09/14	Drawn By DD & RW
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd © CAD Reference <small>C:\Users\DDemirou\Documents\14052_Drafting\PortCoogee_DDemirou.rvt</small>		



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	PERSPECTIVE - COMET LANE	DA902	A
A	DRAFT DA SUBMISSION	30/09/2014								
									Scale	Drawing Size
									@A1	A1
									Project No.	Drawn By
									14052	DD & RW
									Date	
									09/14	
									CAD Reference	
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©	
									C:\User\j\Documents\14052_Draft\PortCoogee_DD\Dimension.rvt	



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Issue	Description	Date	Issue	Description	Date	Client
A	DRAFT DA SUBMISSION	30/09/2014				Open Corporation



Project  
 PORT COOGEE  
 LOT 1017-1018 BRUNSWICK STREET  
 PORT COOGEE  
 WESTERN AUSTRALIA

Title  
 COMET LANE & ROOFTOP  
 PERSPECTIVES

Drawing No.	Title	Issue
DA903	COMET LANE & ROOFTOP PERSPECTIVES	A
Scale	Drawing Size	
@A1	A1	
Project No.	Date	Drawn By
14052	09/14	DD & RW

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9AM



12PM

12pm OVERSHADOWING 43%  
ALL SHADOWS TAKEN ON 22/09



3PM

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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	SHADOW DIAGRAM - 9am/12pm/3pm	DA904	A
A	DRAFT DA SUBMISSION	30/09/2014							Scale 1:750@A1	Drawing Size A1
									Project No. 14052	Date 09/14
									Drawn By DD & RW	
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd © CAD Reference C:\User\DDemireou\Documents\140527_Draft_PortCoogee_DDemireou.rvt	

# port coogee

# response contents.

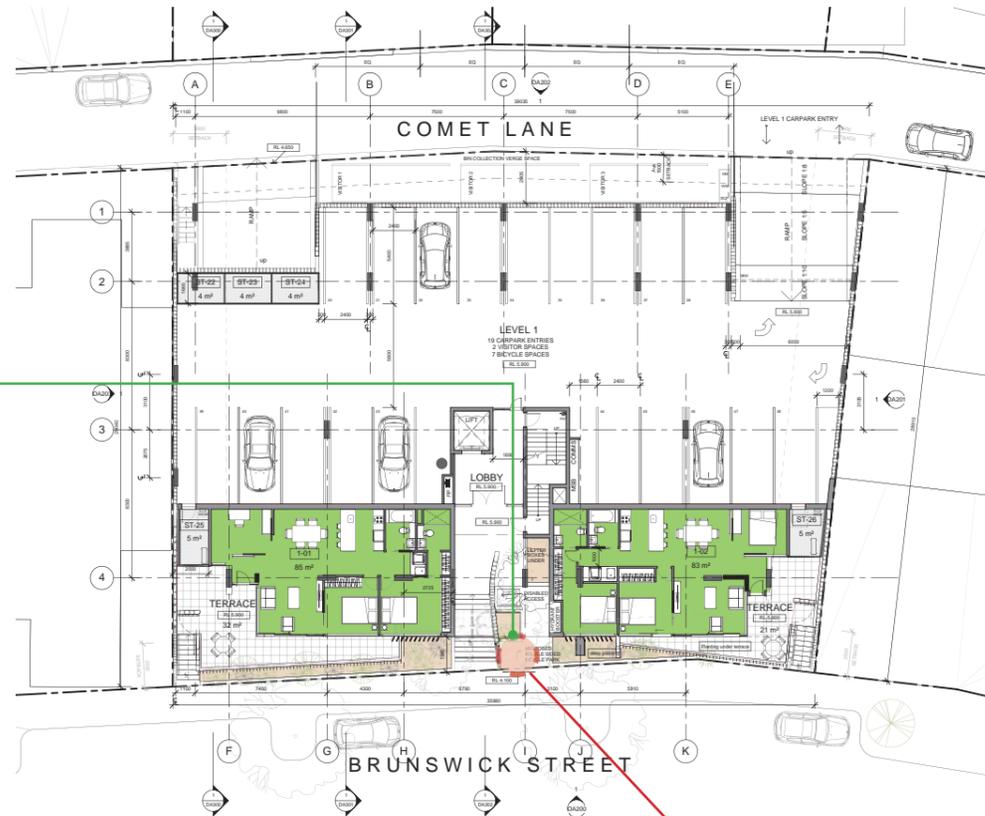
- 1.0 Traffic Management
  - 1.01 Bicycle Parking
  - 1.02 Ramps
  - 1.03 Sightlines
  - 1.04 Bin Storage
  - 1.05 Visitor Parking
- 2.0 Waste Management Option A
  - 2.1 Bin Storage
  - 2.2 Truck Access
  - 2.3 Basement Height
  - 2.4 Bin Numbers
  - 2.5 Verge Space
  - 2.6 Waste Chutes
- 3.0 Waste Management Option A
  - 2.1 Bin Storage
  - 2.2 Truck Access
  - 2.3 Basement Height
  - 2.4 Bin Numbers
  - 2.5 Verge Space
  - 2.6 Waste Chutes

# 1.0 Traffic Management

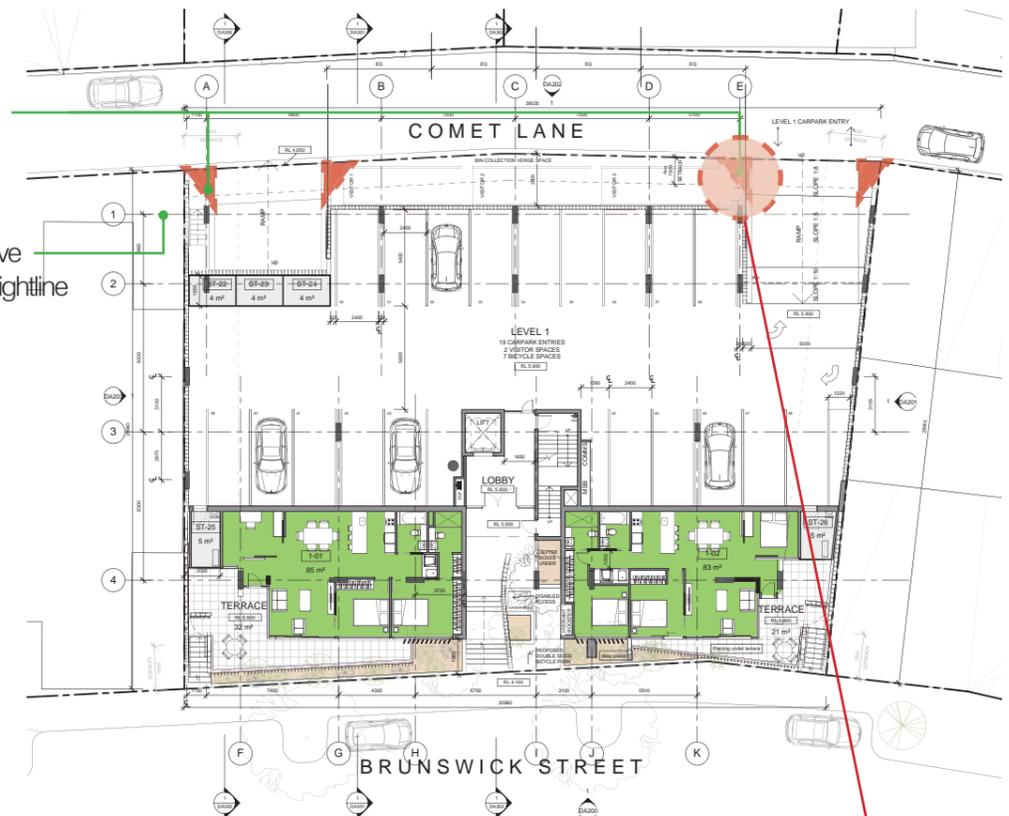
- 1.01 Council comments:  
*The bicycle parking shown in the basement and Level 1 car parks does not comply with AS2890.3 on two counts;*
- Design response:  
Bicycle parking location and numbers have been revised to suit council requirements and codes. Refer to diagram 1.01
- 1.02 Council comments:  
*The access ramps exceed the maximum grade specified in AS2890.1 for the first 6 metres of the ramp;*
- Design response:  
Refer traffic engineers technical notes
- 1.03 Council comments:  
*The sightlines that will be available to vehicles exiting the car parks onto Comet Lane do not appear to comply with the requirements of AS2890.1.;*
- Design response:  
Obstructions to sightline removed. Refer to diagram 1.03
- 1.04 Council comments:  
*The bin storage area on Comet Lane will restrict sight lines when exiting the Level 1 car park and the landscaping adjacent to the Basement car park ramp may restrict sight lines.*
- Design response:  
Location of bin storage has been revised. No restriction to sightline. Refer to figure diagram 1.04 & 2.04 of for new location.
- 1.05 Council comments:  
*There is a clause in the parking requirements for Site 2 in the BFC's which allows for 50% of the visitor car parking to be provided on-street. I was wondering if you were aware of this and whether you would consider this as an option to help address some of the above?*
- Design response  
x1 additional cars added to proposal.  
Total visitor spaces required = 2.7  
Total Visitor spaces provided = 3.0

# 1.0 traffic management

non compliant bicycle parking removed from basement and level 01  
public bicycle park positioned to Brunswick St entrance



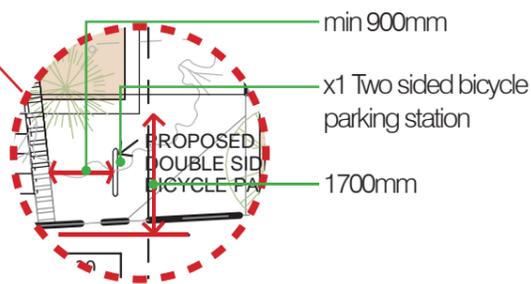
Visual obstructions removed/relocated  
Gridline 1 relocated to the west to remove obstructions from sightline



## 1.01 bicycle parking

1.01 Council comments:  
*The bicycle parking shown in the basement and Level 1 car parks does not comply with AS2890.3 on two counts;*

Design response:  
Bicycle parking location and numbers have been revised to suit council requirements and codes.



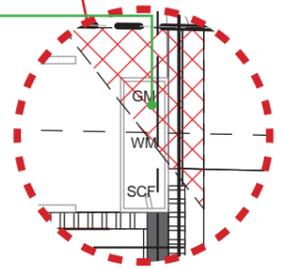
Bicycle Parking Schedule		
Type Comments	Bikes provided	Level
DOUBLE SIDED BICYCLE RACK	2	Street level

## 1.03 sightlines

1.03 Council comments:  
*The sightlines that will be available to vehicles exiting the car parks onto Comet Lane do not appear to comply with the requirements of AS2890.1.;*

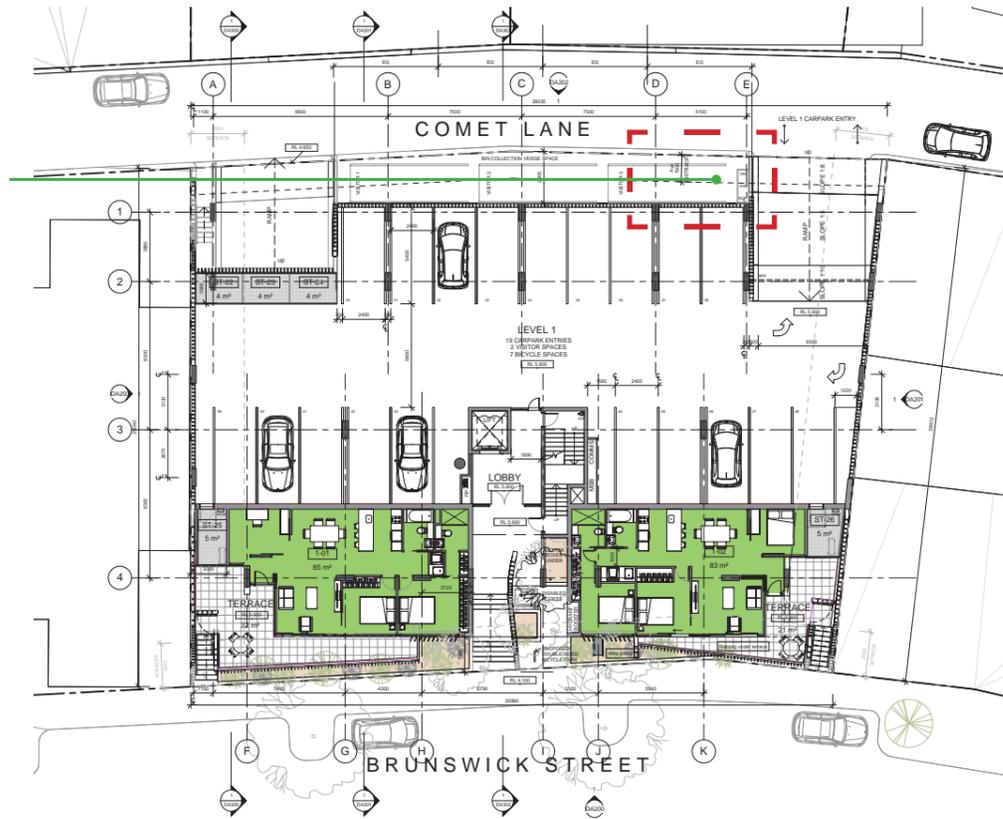
Design response:  
Visual obstructions to sightline removed.

water, sewer & gas meters to be housed in a low height enclosure below ramp level.



# 1.0 traffic management

bin storage relocated from Comet Lane to basement level to reduce visual impact to sightline.



additional visitors car park



## 1.04 bin storage

1.04 Council comments:  
*The bin storage area on Comet Lane will restrict sight lines when exiting the Level 1 car park and the landscaping adjacent to the Basement car park ramp may restrict sight lines.*

Design response:  
 Location of bin storage has been revised. No restriction to sightline.  
 Refer to figure diagram 2.04 of for new location.

## 1.05 visitors parking

1.05 Council comments  
*There is a clause in the parking requirements for Site 2 in the BFC's which allows for 50% of the visitor car parking to be provided on-street. I was wondering if you were aware of this and whether you would consider this as an option to help address some of the above?*

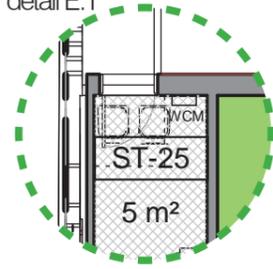
Design response:  
 x1 additional cars added to proposal.  
 Total visitor spaces required = 2.7  
 Total Visitor spaces provided = 3.0

Parking Schedule		
Level	Description	Total Count
BASEMENT	5400 x 2400	29
Street Level	Parallel 2100x6600	3 (Visitors)
LEVEL 01	5400 x 2400	19
51		

# 2.0 waste management option A

- 2.01 Council comments:  
*The bin store must equal 1m<sup>2</sup>/residential unit or a minimum of 38m<sup>2</sup>.*
- Design response:  
Existing bin storage space from comet lane has been relocated to provide 37m Bin storage in the Basement level. Units 101, & 102 have been provided with dedicated bin storage area individual access to the car park refer diagram 2.01.
- 2.02 Council comments:  
*Basement collection using our low profile rear loader can only occur if the truck can complete the operation in a forward gear.*
- Design response:  
240lt bins to be collected from Comet lane. No truck access to basement required.
- 2.03 Council comments:  
*660lt bins are only available for use within a minimum 2.4m high basement via this low profile vehicle.*
- Design response:  
As above
- 2.04 Council comments:  
*One set of bins (1 recycle and 1 general 240lt) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.*
- Design response:  
28 bins provided in total. x24 in Bin Storage room x 4 bins located in individual storage space on level 01 refer diagram 2.04.
- 2.05 Council comments:  
*Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation*
- Design response:  
23.5m Verge space provided at Comet Lane refer to diagram 2.05.
- 2.06 Council comments:  
*Waste chutes are not supported, unless the City can be convinced that a permanent maintenance and cleaning program will prevent odours, rodents and cockroaches.*
- Design response:  
Refer 3.0 Waste Management Plan Option B.

detail E.1



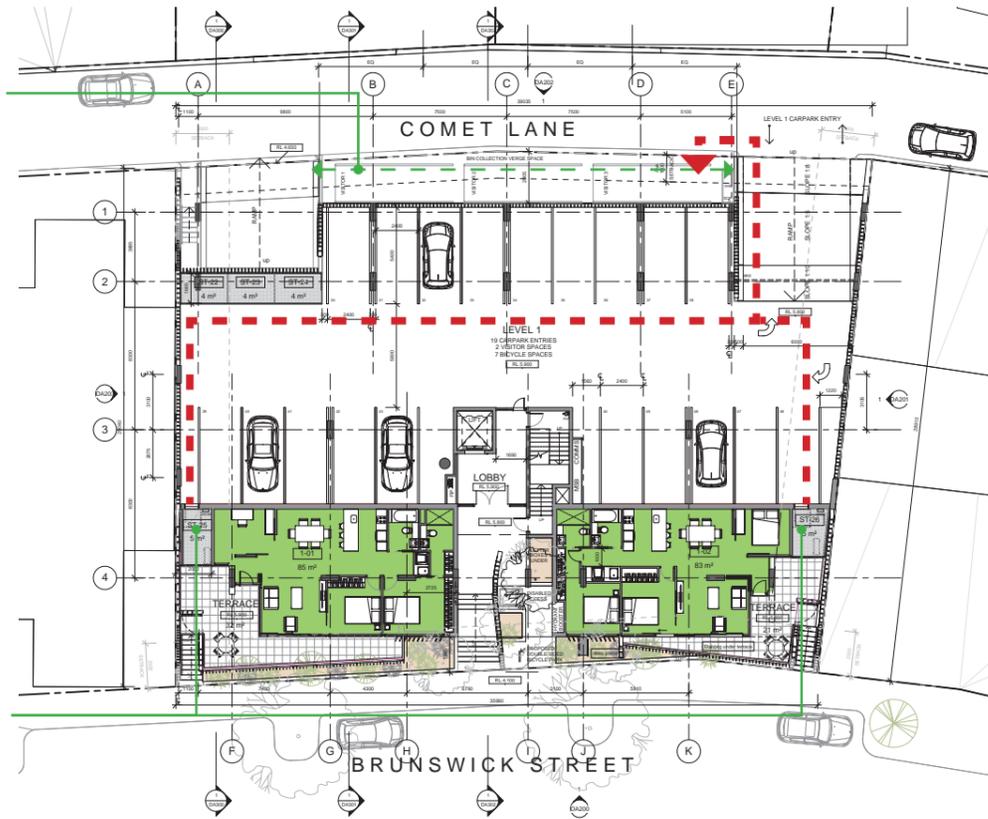
Units 1.01 & 1.02 provided with 1sqm dedicated bin storage area

23.5m dedicated verge space

bin storage relocated from Comet Lane to basement level.

Level 1 Units occupants transport bins to Comet Lane verge space for bin collection

Units 1.01 & 1.02 provided with 1sqm dedicated bin storage area

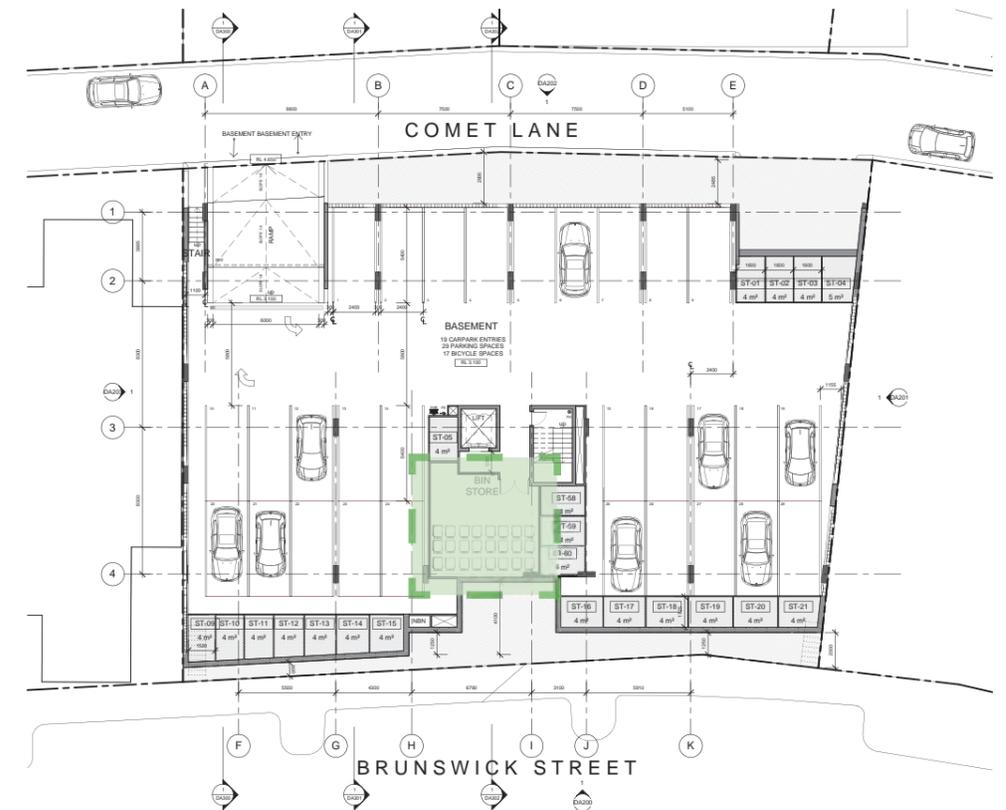


## 2.01 bin storage

2.01 Council comments:  
The bin store must equal 1m2/residential unit or a minimum of 38m2.

Design response:  
Existing bin storage space from comet lane has been relocated to provide 37m Bin storage in the Basement level. Units 101, & 102 have been provided with dedicated bin storage area individual access to the car park.

# 2.0 waste management option A



## 2.04 bin numbers

2.04 Council comments:  
One set of bins (1 recycle and 1 general 240lt) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.

Design response:  
28 bins provided in total. x24 in Bin Storage room 240lt x 4 140lt bins located in individual storage space on level 01.

# 2.0 waste management option A

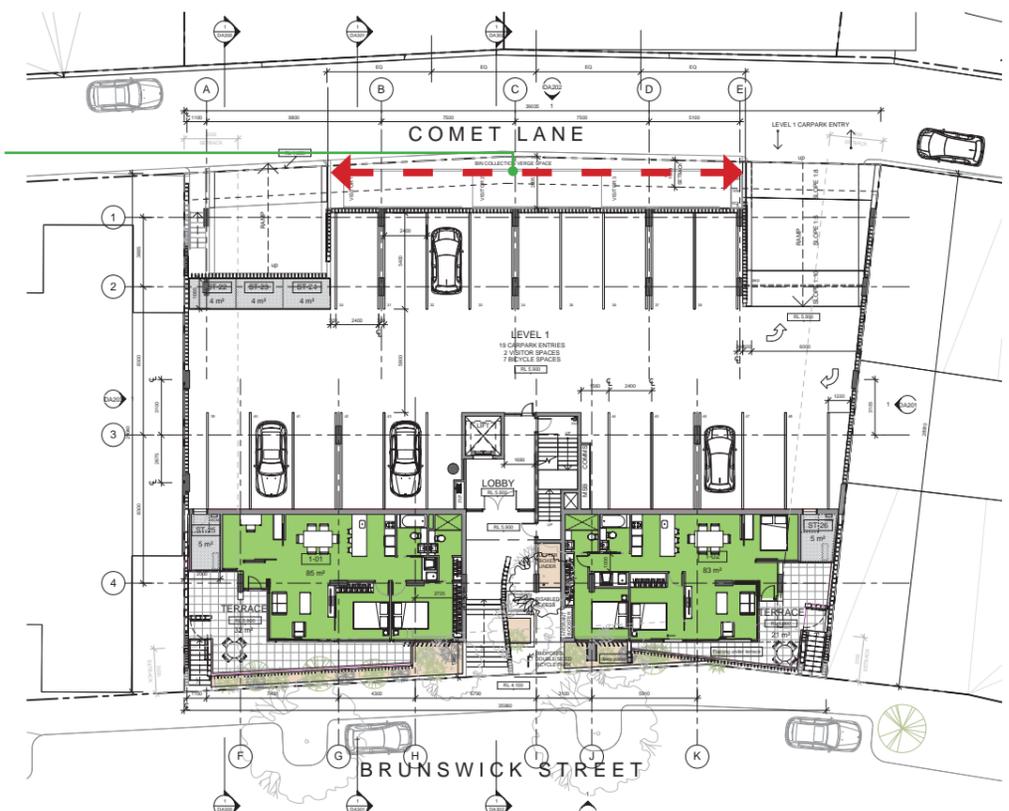
bin storage relocated from Comet Lane to basement level.

Bins transported to verge space individually

Level 1 Units occupants transport bins to Comet Lane verge space for bin collection. Refer figure E



dedicated verge space on comet lane on street level.



## 2.05 verge space

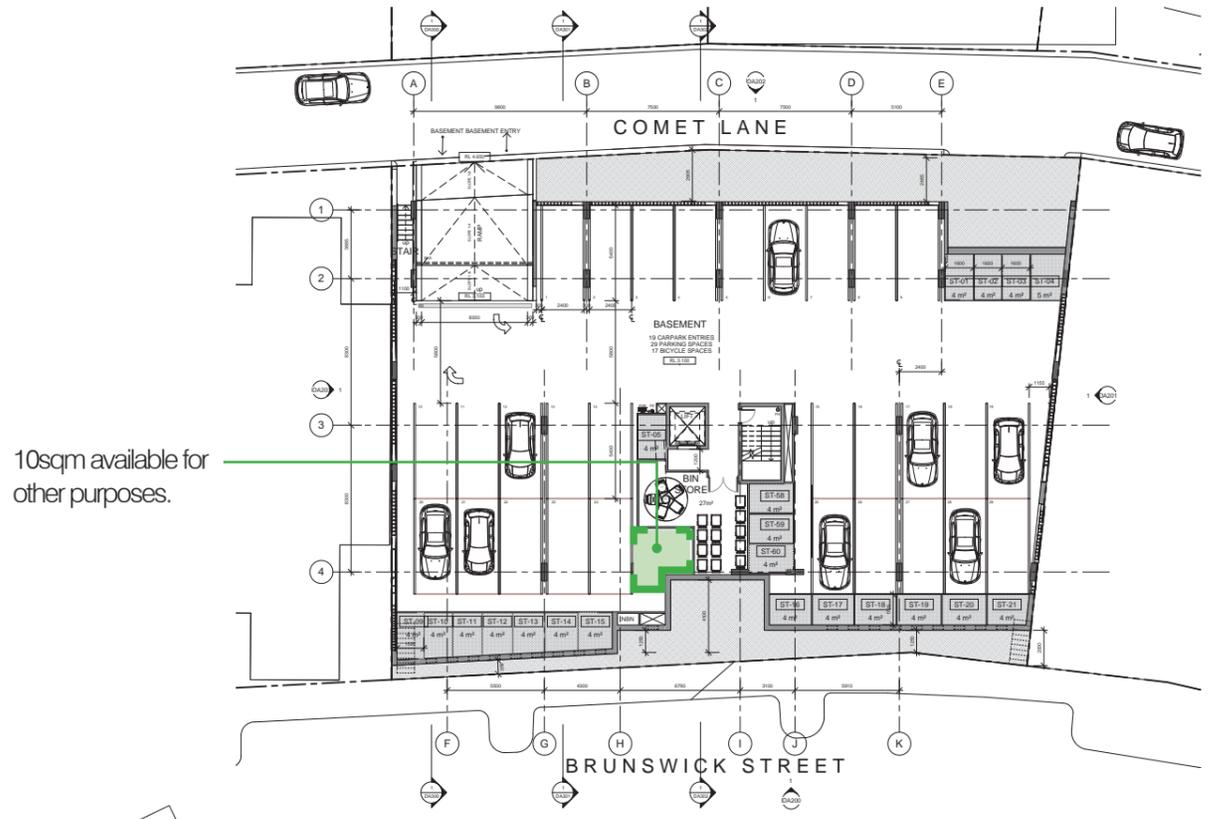
2.05 Council Comments  
 Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation.

Design Response:  
 23.5m Verge space provided at Comet Lane.

# 3.0 waste management option B

- 3.01 Council comments:  
*The bin store must equal 1m<sup>2</sup>/residential unit or a minimum of 38m<sup>2</sup>.*
- Design response:  
Compactor as a separate unit effectively has a 1:5 compression rate with a minimum rate of 1:2, therefore decreasing the amount of area required for bins.
- 3.02 Not required
- 3.03 Not required
- 3.04 Council comments:  
*One set of bins (1 recycle and 1 general 240 lt) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.*
- Design response:  
Compression rate minimizes the amount of verge space required. Decreasing the tipping load and landfill space from 50% to 20%.
- 3.05 Council comments:  
*Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation*
- Design response:  
Required verge space potentially decreases by 30% 16.2m Verge space required
- 3.06 Council comments:  
*Waste chutes are not supported, unless the City can be convinced that a permanent maintenance and cleaning program will prevent odours, rodents and cockroaches.*
- Design response:  
Bin storage room to be fully washable for regular cleaning. Waste chute will be mechanically ventilated to allowing for odour control with a permanent maintenance system.
- The system is to be maintained by cleaners every 3 months and compactor to be serviced.
- Bin room is to be fully enclosed to prevent rodents.

# 3.0 waste management option B



## 3.01 bin storage

3.01 Council comments:  
The bin store must equal 1m<sup>2</sup>/residential unit or a minimum of 38m<sup>2</sup>.

Design response:  
Compactor as a separate unit effectively has a 1:5 compression rate with a minimum rate of 1:2, therefore decreasing the amount of area required for bins.

16 bins in total (12 bins required for level 3-05 required + 4 bins for units 101 & 102)

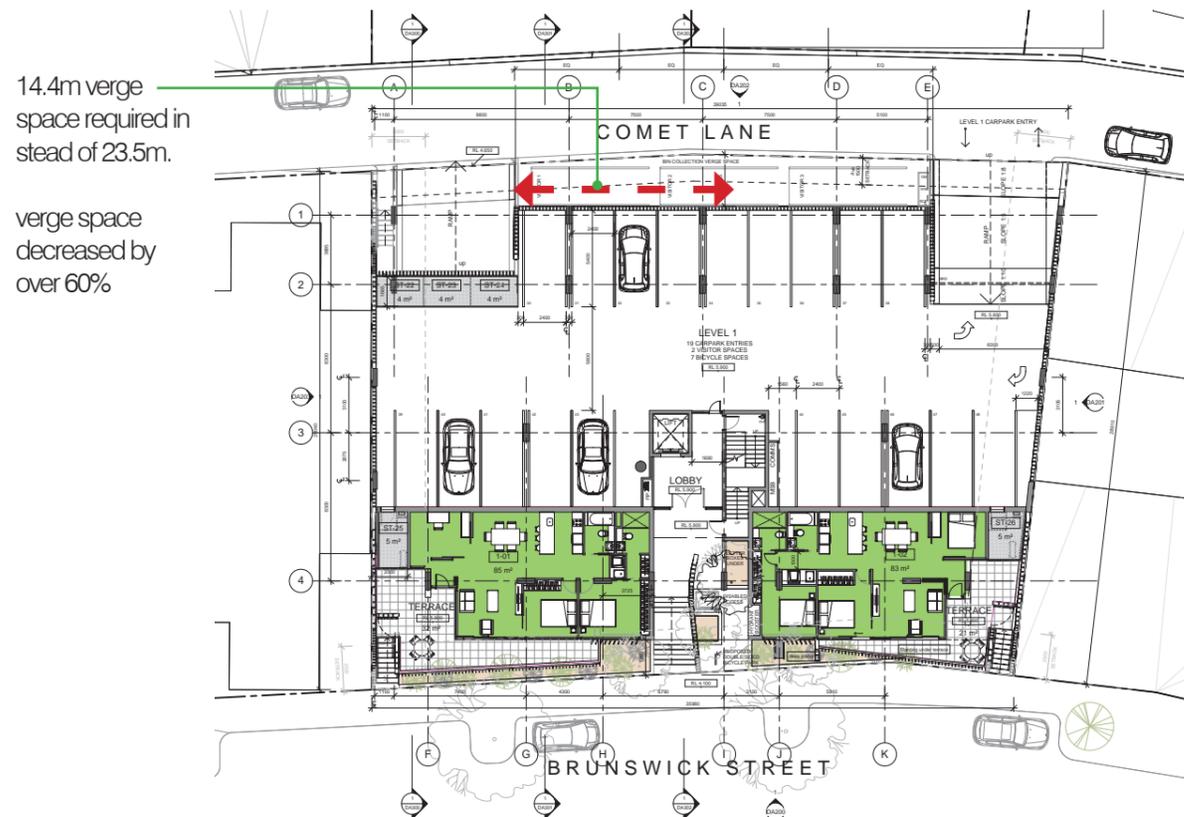


## 3.04 bin numbers

3.04 Council comments:  
One set of bins (1 recycle and 1 general 240 lb) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.

Design response:  
Compression rate minimizes the amount of verge space required. Decreasing the tipping load and landfill space by 50% to 80%.

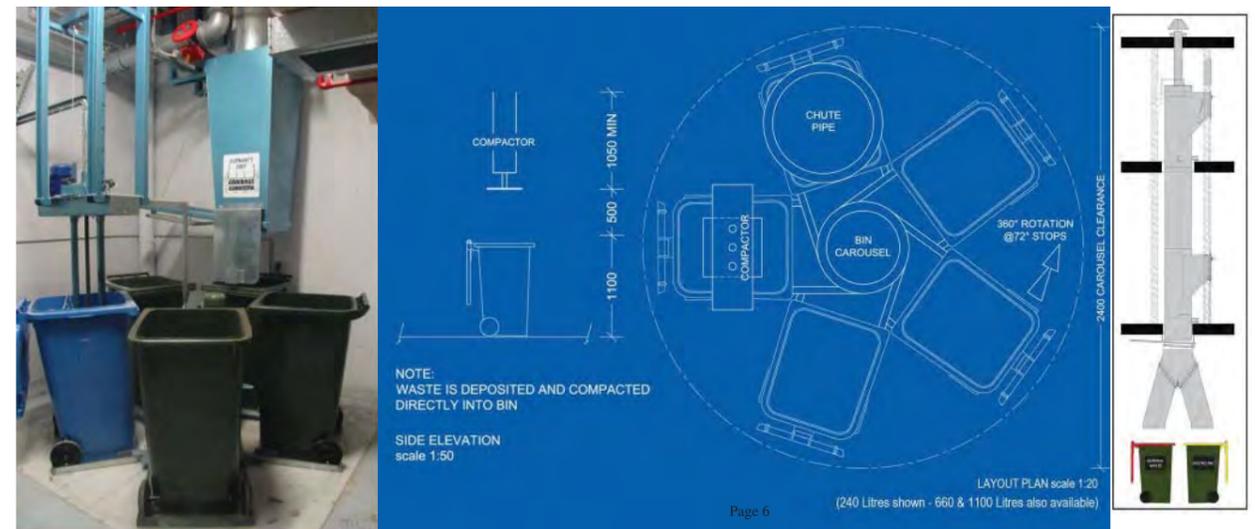
# 3.0 waste management option B



## 3.05 verge space

3.05 Council comments:  
Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation

Design response:  
Required verge space potentially decreases by 60% 14.2m Verge space required



## 3.06 Advantages

Simple and easy to use system for garbage disposal and recycling.

Encourages participation in recycling while allowing care taker to closely monitor recyclable contamination. Allowing for further education for repeat offenders.

Easier transfer of garbage and recycling to the Bin storage area, eliminating leaking bag syndrome, manual transportation of rubbish bags which intern improves amenity of occupants.

Eliminates the possibility of residents cluttering bins and bin storage area with large items that need to be removed by a caretaker or cleaning staff.

System can operate with 240lt bins.

Compactor significantly decreases the requirement for available space & amount of landfill. Significantly decreasing the proposals ongoing impact on the environment.



WMK

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DIVERSIFIED INVESTMENT CORPORATION

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transport planning • traffic engineering • project management

TRANSCORE PTY LTD AS TRUSTEE FOR THE TRANSCORE TRUST  
ACN 094 951 318 ABN 85 707 500 280

**Technical Note:** No 1b

**Date:** 14/11/2014

**Project No:** t14.266

**Project:** Lots 1017 and 1018 Brunswick Street, North Coogee

**Subject:** Car Park Management Plan

## INTRODUCTION

The subject of this technical note is the proposed residential development located at Lots 1017 and 1018 Brunswick Street, North Coogee. The proposed development provides 29 car spaces in the basement level car park and 19 car spaces in the level 1 car park. Each parking level is accessed via a separate ramp connecting to Comet Lane, which runs along the rear of the development.

Based on advice provided by the Project Architect, City of Cockburn technical staff have identified two issues related to the car park ramp grades and sightlines:

- ✚ *The access ramps exceed the maximum grade specified in AS2890.1 for the first 6m of the ramp; and,*
- ✚ *Sightlines available to cars exiting the car park may be restricted by the bin storage area and landscaping.*

Transcore has been commissioned by Open Corporation to develop a Car Park Management Plan and to investigate and address the concerns raised by City of Cockburn regarding the ramp gradient and sightlines.

Accordingly, this management plan investigates traffic management measures to ensure safe and efficient use of the ramps and recommends usage of mirrors to improve sightlines for vehicles exiting the car park. .

## RAMP GRADIENT AND SIGHT LINES FOR OUTBOUND VEHICLES

Section 3.3 of the Australian Standard AS2890.1:2004 requires that a flat area (maximum gradient 1:20), be provided at the top of a ramp for the first 6m into the car park from the property boundary. The flat area is provided to improve outbound vehicle sightlines on approach to a footpath or road edge.

The proposed basement and level 1 car park ramps provide a 1:8 grade ramp on approach to the footpath. Investigation of the proposed car park layout and ramps indicates that the site is physically constrained; providing a 6m length of 1:20 grade ramp would significantly impact the car park layout and number of bays which can be provided in the car park levels.

As both car park ramp driveways connect to a rear laneway with no footpaths, and as a result of low traffic and pedestrian volumes, the risk of any conflict is minimal. However improvement traffic management measures were investigated in order to facilitate outbound vehicle sightlines for both ramps on approach to the laneway and to improve safety.

Accordingly, it is recommended that convex mirrors be installed at either side of the car park exit points to improve visibility between pedestrians and outbound vehicles for both sides of Comet Lane. The approximate location and angle of the mirrors is shown in Figures 1 and 2 of **Appendix A**, and indicative sightlines are included to show the resulting angle of visibility.

The mirrors should be mounted at a suitable height and angled down to improve driver sightlines. Consideration should also be given to installing "Caution Watch for Vehicles" signs on the wall on either side of the driveways (refer to Figure 3 of **Appendix A**), to warn pedestrians to cross the driveways with care. These signs should be installed in a suitable location and angled to be clearly visible to pedestrians.

It is also recommended to install "SLOW pedestrian crossing" signs in both car park ramps at a suitable location to be clearly visible to outbound vehicles, as shown in Figure 3. This will alert outbound motorist to be aware of potential pedestrian movements and clearly defines the right of way to pedestrians.

## CONCLUSIONS

The total number of outbound vehicle movements per day will be low for a residential development of this scale. As the car park ramps connect to a rear laneway with no footpath, the number of vehicular and pedestrian movements crossing the ramp driveways will be minimal. Therefore the frequency of pedestrian and vehicular interaction at the car park ramps is also expected to be minimal.

Nevertheless, installation of the recommended safety mirrors and supplementary signage will ensure clear lines of sight between outbound vehicles and

pedestrians and will alert drivers to beware of potential pedestrian movements on the laneway.

APPENDIX A - RECOMMENDED TRAFFIC MANAGEMENT TREATMENTS

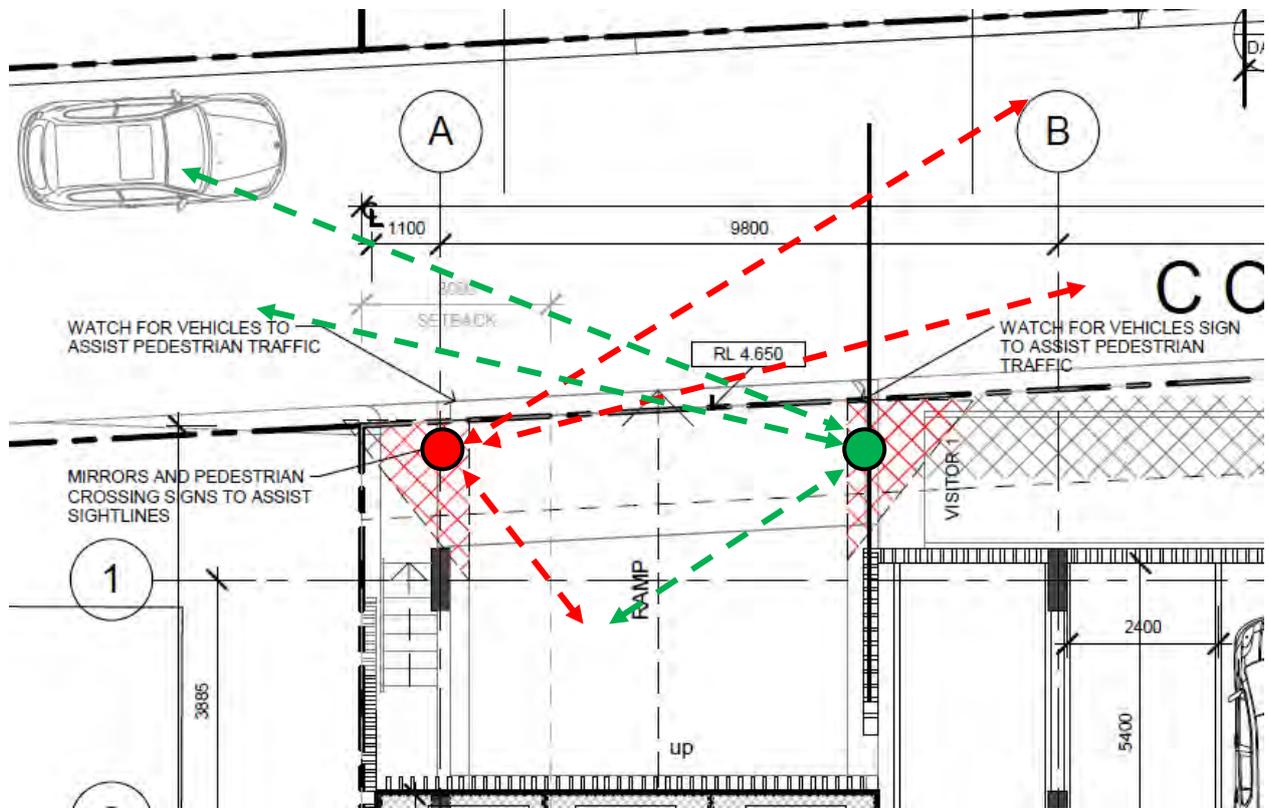


Figure 1. Recommended mirror treatments and proposed signage - basement ramp

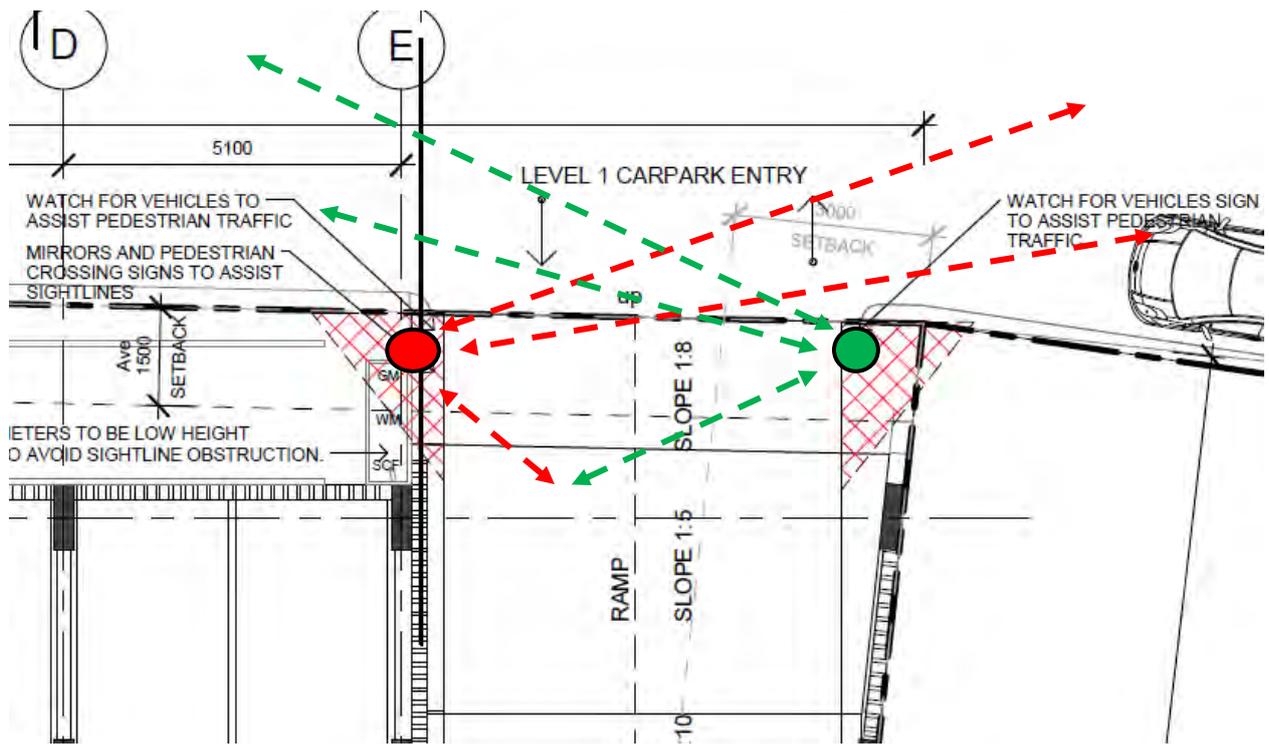


Figure 2. Recommended mirror treatments and proposed signage – level 1 ramp

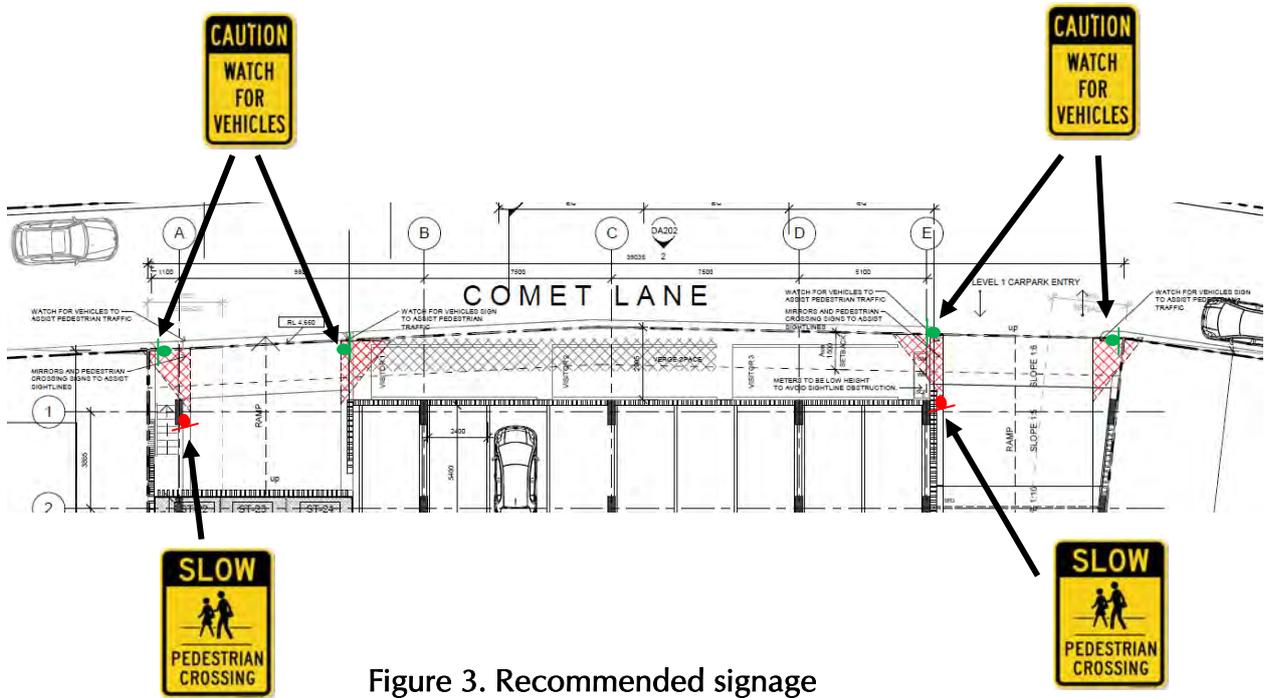
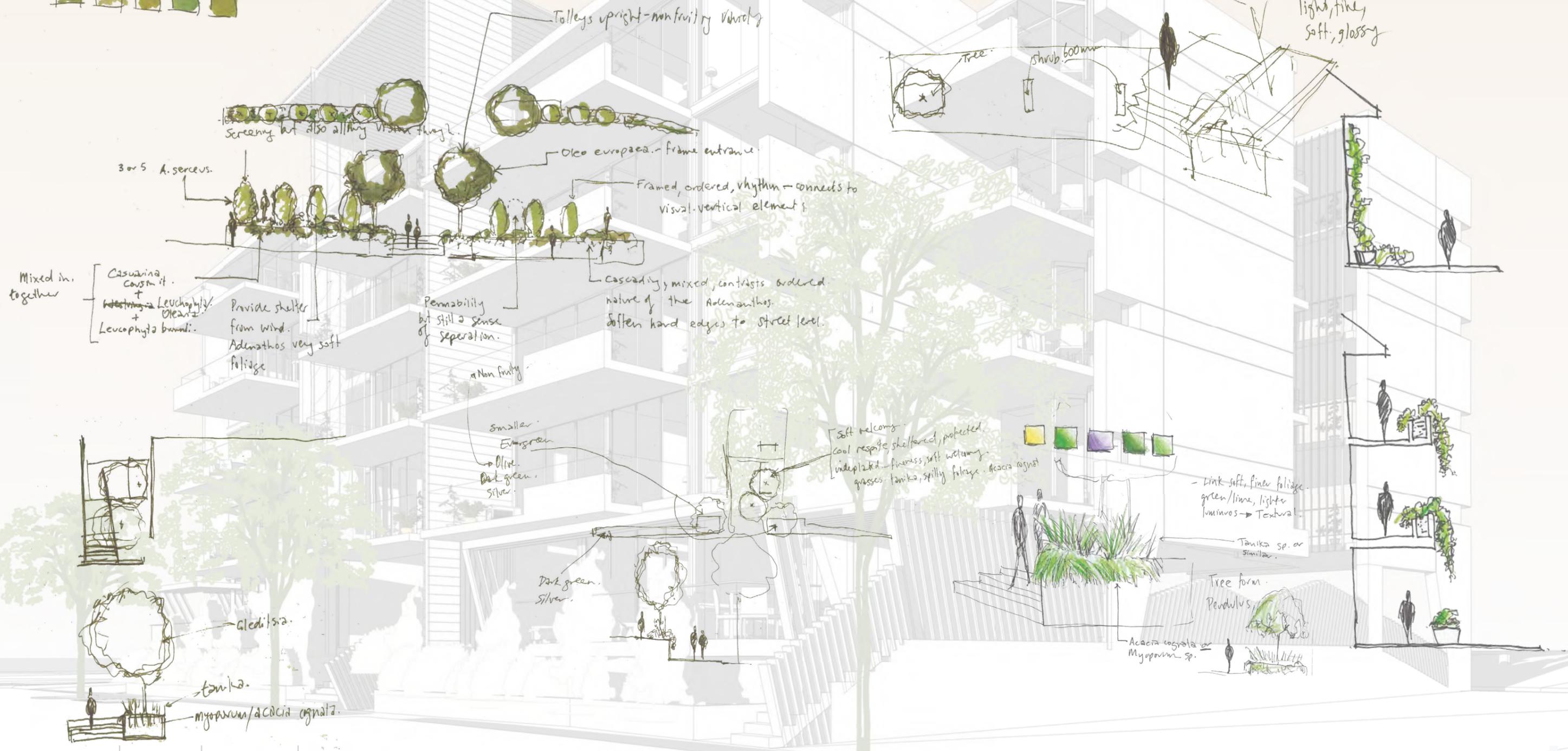


Figure 3. Recommended signage

Brunswick St Entrance.



LANDSCAPE CONCEPT DESIGN

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015



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pg 5\_LEVEL ONE ELEVATION

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CONTENTS

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE NTS

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## Landscape Concept Theming

The landscape concept is highly cognisant of the local context and the challenging coastal conditions in which the site is located.

The planting design seeks to ground the building within its context, simultaneously softening and fragmenting the edge between the built form and the street. Responding to the architectural form the planting design provides a soft and textural element that offers year round visual interest.



### SERVICE\_PRACTICAL\_SURVIELLANCE



The Comet Lane interface ensures access to the meter box, with planting maintained to under 600mm adjacent to the carparks to maintain sight lines and passive surveillance.

### RESPITE\_SHELTERED\_LIGHT\_WELCOMING



The foyer area within the building recess becomes an area of respite from wind and sun, with soft and light planting providing a welcoming entrance.

### SOFTENED\_TEXTURAL\_RHYTHM\_SCREENING



The building is 'grounded' to the street with the use of planting as a means of fragmenting and softening the interface between building and street. The contrasting forms between the cascading and upright planting provides visual interest.

## LANDSCAPE CONCEPT THEMING

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

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# LEVEL 1 PLAN

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:200@A3

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Soft, cascading Casuarina's softens the line between built form and street.

The Adenanthos provide vertical accents with gaps to allow both screening and permeability.

Olive trees (non-fruiting variety) frame the main entry to the building from the street.

The Xanthorrhoea grass trees are planted as entry statements to provide soft and welcoming entry.

The Acacia cognata's cascade over the planter walls to visually soften them.

The layering of planting, with variations in height and colour, create a sense of depth.

The silver Olearia's can be clipped to create soft mounds

## LEVEL 1 ELEVATION

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:150 @A3

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## COMET LANE

### LARGE SHRUB



*Adenanthos sericeus* 'Silver Streak'  
Woolly Bush  
3m x 1.5m

### SMALL SHRUB



*Olearia axillaris* 'Little Smokie'  
Coastal Daisy Bush  
1m x 0.8m

### PLANT NOTES:

All plants listed respond well to pruning, and can be easily managed.  
Plant sizes quoted (height x width) are estimated size of mature plants.

All plant species selected will tolerate the exposed and salty coastal conditions of the site, and blend seamlessly with the surrounding streetscape.

## FOYER ENTRY

### TREE



*Xanthorrhoea preissii*\*  
Grasstree  
2m x 1m (typical)

### SMALL SHRUB



*Acacia cognata* 'Limelight'  
Narrow Leaf Bower Wattle  
1m x 1m

### GRASS



*Lomandra longifolia* 'Tanika'  
Basket Bush  
0.6m x 0.6m

\*NOTE: *Xanthorrhoea preissii* also used in Brunswick St

## BRUNSWICK ST

### TREE



*Olea europaea*  
Olive (non fruiting variety)  
6m x 3m

### LARGE SHRUB



*Adenanthos sericeus* 'Silver Streak'  
Woolly Bush  
3m x 1.5m

### SMALL SHRUB



*Olearia axillaris* 'Little Smokie'  
Coastal Daisy Bush  
0.8m x 0.8m

### GROUND COVER



*Casuarina glauca* 'Cousin It'  
Swamp Oak  
0.3m x 1m

## PLANT PALETTE

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

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SCALE NTS

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Artificial turf is used as a low maintenance option adjacent to the pool and deck area.

The Pandanus are able to withstand the open and exposed environment whilst providing shade around the seating areas.

The herb gardens are planted with hardy, drought tolerant herbs.

Agave's are used feature specimens in planter beds, with underplantings of contrasting and colourful plants.



# ROOFTOP GARDEN CONCEPT

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:200 @ A3

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TREE



*Pandanus utilis*  
Swan Neck Agave  
2.5m x 2.5m

PLANTERS AND POTS WITH PLANTING



The use of additional pots and planters as required as a means of providing additional wind protection and screening.



FEATURE PLANTING



*Agave attenuata*  
Swan Neck Agave  
0.6m x 0.6m

GROUNDCOVERS



*Carpobrotus edulis*  
Ice Plant  
0.2m x 0.5m



*Echeveria elegans*  
Chickens and Hens  
0.1m x 0.5m



*Aeonium purpurea*  
Schwartzkop  
0.2m x 0.5m



*Senecio serpens*  
Chalksticks  
0.2m x 0.5m



*Tradescantia pallida*  
Purple Queen  
0.2m x 0.5m



*Lomandra "Seascape"*  
0.2m x 0.5m

HERBS FOR HERB GARDEN



*Thymus vulgaris*  
Thyme  
0.2m x 0.2m



*Cymbopogon citratus*  
Lemongrass  
0.6m x 0.6m



*Oreganum vulgare*  
Oreganum  
0.2m x 0.2m



*Rosmarinus officinalis*  
Rosemary  
0.6m x 0.6m



*Salvia officinalis*  
Sage  
0.3m x 0.3m

ROOFTOP GARDEN PLANTING PALETTE

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

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CLIMBERS FOR GREEN FACADE



**Hardenbergia comptonia**  
 Native Wisteria  
 3.0m x 3.0m  
 An endemic coastal climbing plant of the Perth region. Hardenbergia is an excellent climber, with most of the vegetative mass of the plant is concentrated toward the top half.



**Trachelospermum jasminoides**  
 Star Jasmine  
 2.0m x 2.0m  
 Can be used as both a ground cover and climber and is suitable in sunny and coastal aspects. Star Jasmine also grows densely from the base to give a dense vegetative covering over walls



Examples of green facades with climbing plants growing on stainless steel wires.



VERTICAL GARDENS PLANT PALETTE & LOCATION

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:200 @ A3

Level 1, 55 St Georges Tce,  
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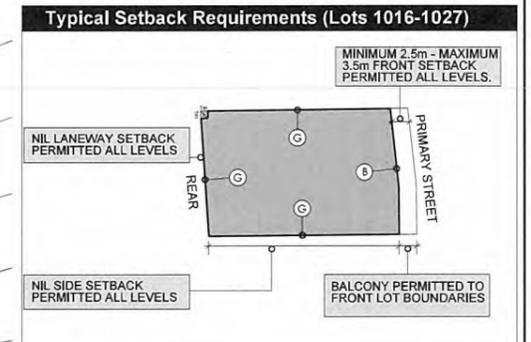
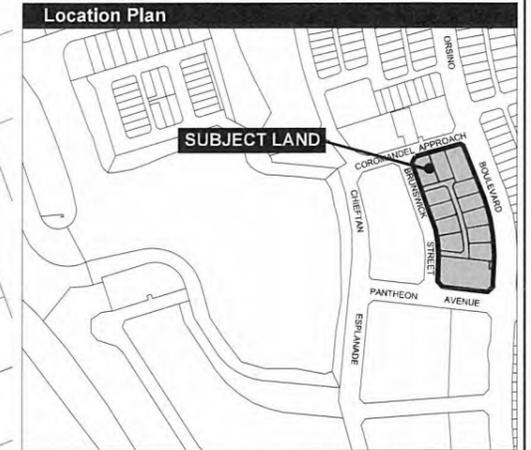
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### Detailed Area Plan – Site Specific Building Requirements

This Detailed Area Plan (DAP) replaces the Site Specific Building Requirements of the Port Coogee Marina Village Built Form Codes in relation to Site 2 only. All other provisions of the Port Coogee Marina Village Built Form Codes apply to the land the subject of this DAP (Lots 1015 to 1027).

Land Uses		
Ground Floor:	Pantheon Avenue:	Commercial/Retail. A non residential use is mandatory at ground level
	Orsino Boulevard:	Commercial/Retail/Residential
	Coromandel Approach/Brunswick Street:	Commercial/Residential
Level 1:	All streets	Commercial/Residential
Level 2 and above:	All streets	Residential
Setbacks		
Basement:	All boundaries:	- Nil permitted
Ground Floor/Level 1/ Level 2:	Pantheon Avenue:	C Nil required.
	Orsino Boulevard/Coromandel Approach/Brunswick Street:	B 2.5m minimum / 3.5m maximum. Note: nil setback permitted to Lot 1015 Orsino Boulevard frontage as indicated on DAP.
Level 3 and above:	Lot side boundaries	G Nil permitted
	Internal Laneway/Access Road	G Nil permitted
	Pantheon Avenue	C Nil minimum / 3.0m maximum
	Orsino Boulevard/Coromandel Approach/Brunswick Street	B 2.5m minimum / 3.5m maximum
Projections:	Lot side boundaries	G Nil permitted
	Internal Laneway/Access Road	G Nil permitted
	Balconies and architectural elements may extend to the lot boundary. On commercial and retail frontages, awnings and canopies at ground level may extend beyond the boundary by a maximum of 3.0m or to within 0.5m from the back of adjacent kerb.	
Height		
Overall:	Lots 1015, 1020-1022: 21.0m maximum height. Lots 1016-1019 & 1023-1027: 17.3m maximum height. Development fronting Orsino Boulevard is required to be a minimum of three storeys.	
Minimum floor level height to Pantheon Avenue:	Ground to first floor:	4.0m minimum
	Top of Podium Parapet (Level 1):	8.0m-10.0m [measured from finished pavement level]
	Top of Podium Parapet (Level 2):	11.5m-13.5m [measured from finished pavement level]
Wind	All designs are subject to wind assessment in accordance with the Wind General Provision as detailed within the Built Form Codes.	
Design Elements		
Gateway Building (Lot 1015)	Each dwelling shall be provided with a minimum of one balcony. Build form to corners noted as 'gateway buildings' shall be designed with consideration to framing the street via setback, height, mass and detail elements, in accordance with the Icon and Gateway Building General Provisions as detailed within the Built Form Codes.	
Hotel Site (Lot 1015)	This site has been allocated as a possible location for the required hotel under the Port Coogee Local Structure Plan.	
Community Purpose Site (Lot 1015)	This site has been allocated as a possible location for the community purpose use required by the Port Coogee Local Structure Plan.	
Vehicular Access	All lots shall obtain vehicle access via the laneway. No vehicular access is permitted to lots via Orsino Boulevard, Pantheon Avenue, Coromandel Approach or Brunswick Street.	
Parking	A total of 18 on-street parking bays are available for inclusion in the calculation of visitor parking provided for multiple dwellings on Lots 1015-1027. The 18 on-street parking bays are allocated specifically as provided below: - Lots 1016 to 1027: 1 on-street parking bay per lot for visitor parking for multiple dwellings. - Lot 1015: 6 on-street parking bays for visitor parking for multiple dwellings.	
Fencing	Orsino Boulevard/Coromandel Approach/Brunswick Street:	Type 1/Type 2 (Refer to Built Form Codes for typical fencing configuration)
	Pantheon Avenue:	No fencing permitted other than Type 3 to service areas only.
	Laneways/Internal Access Roads:	Type 3 to service areas only/Type 4



Legend	
[Grey Box]	BUILDING ENVELOPE
[Dotted Line]	NO VEHICULAR ACCESS PERMITTED
[Star Symbol]	GATEWAY BUILDINGS
[Dashed Line]	MANDATORY AWNINGS/PEDESTRIAN WEATHER PROTECTION THAT SHALL ADDRESS THE OBJECTIVES FOR AWNINGS AND CANOPIES IN ACCORDANCE WITH THE GENERAL PROVISIONS OF THE BUILT FORM CODES
[Dash-Dash Line]	CHANGE IN MAXIMUM HEIGHT
[21.0m Box]	MAXIMUM BUILDING HEIGHT MEASURED TO THE HIGHEST POINT. SERVICES SUCH AS LIFT OVERRUNS AND BALUSTRADES AND NON-HABITABLE ARCHITECTURAL ELEMENTS ARE EXCLUDED FROM THE HEIGHT CALCULATION
[Power Line Symbol]	BUILDING ENVELOPE DOES NOT EXTEND OVER EASEMENTS
[C in Circle]	DENOTES SETBACK TYPE, SEE NOTES
[C in Square]	COMMUNITY PURPOSE SITE ALTERNATIVE LOCATION
[H in Hexagon]	HOTEL ALTERNATIVE LOCATIONS

### Endorsement Table

This Detailed Area Plan has been adopted by Council and signed by the Principal Planner:

Principal Planner:

Date: 29.11.2011

## Site Specific Building Requirements Detailed Area Plan - Marina Village (Lots 1015-1027)

PORT COOGEE  
AN AUSTRALAND PROJECT

**Taylor Burrell Barnett**

plan: 907057/2106  
date: 18/10/2011  
PC 94

checked: MW  
checked: SD  
drawn: TB

scale: 1:1000@A3  
0 10 20m

Taylor Burrell Barnett Town Planning & Design  
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## Form 2 - Responsible Authority Report (Regulation 17)

<b>Property Location:</b>	No. 53-74/9 (Lot 802) McCabe Street, North Fremantle
<b>Application Details:</b>	Variations to Five Storey Multiple Dwelling Development (26 Multiple Dwellings) and Basement Vehicle Parking Level
<b>DAP Name:</b>	Metro South-West Joint Development Assessment Panel
<b>Applicant:</b>	TPG Town Planning, Urban Design and Heritage
<b>Owner:</b>	Taskers Living Pty Ltd
<b>LG Reference:</b>	DAPV50003/14
<b>Responsible Authority:</b>	City of Fremantle
<b>Authorising Officer:</b>	Manager Development Approvals
<b>Department of Planning File No:</b>	DP/13/00843
<b>Report Date:</b>	22 December 2014
<b>Application Receipt Date:</b>	18 November 2014
<b>Application Process Days:</b>	50 days
<b>Attachment(s):</b>	1: Location Plan 2: Development Plans dated 12 November 2014 (A0.01, A1.01, 1.02, 1.03, 1.04, 1.05 & 1.06, A2.01, 2.02 & 2.03) 3: Previous Responsible Authority Report (DAP80006/13) (DP/13/00843)

### Officer Recommendation:

**That the Metro South-West Joint Development Assessment Panel resolves to:**

**Approve DAP Application reference DP/13/00843 and accompanying plans dated 12 November 2014, A0.01, A1.01, 1.02, 1.03, 1.04, 1.05 & 1.06, A2.01, 2.02 & 2.03 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4.**

### Amended Conditions:

- 1. This approval relates only to the development as indicated on the approved plans, dated 12 November 2014. It does not relate to any other development on this lot and must substantially commence within four years from the date of this decision letter. If the subject development is not substantially commenced within a 4 year period, the approval shall lapse and be of no further effect.**
- 2. The design and construction of the development is to meet the 4 star green star standard as per Local Planning Policy 2.13 or alternatively to an equivalent standard as agreed upon by the Chief Executive Officer, City of Fremantle. Any costs associated with generating, reviewing or modifying the alternative equivalent standard is to be incurred by the owner of the development site. Within 12 months of an issue of a**

certificate of Building Compliance for the development, the owner shall submit either of the following to the City to the satisfaction of the City of Fremantle;

- a) a copy of documentation from the Green Building Council of Australia certifying that the development achieves a Green Star Rating of at least 4 Stars, or
  - b) a copy of agreed equivalent documentation for instance where there is no green star rating tool available certifying that the development achieves a Green Star Rating of at least 4 Stars.
3. All storm water discharge shall be contained and disposed of on site or otherwise approved by the City of Fremantle.

#### **Additional conditions**

11. Prior to the issue of a Demolition Permit a Demolition and Construction Management Plan shall be submitted to the City to the satisfaction of the City to Fremantle addressing the following matters:
- a) Use of City car parking bays for construction related activities;
  - b) Protection of infrastructure within the road reserve;
  - c) Protection of street trees;
  - d) Security fencing around construction sites;
  - e) Gantries;
  - f) Access to site by construction vehicles;
  - g) Contact Details;
  - h) Site offices;
  - i) Noise - Construction Work and Deliveries;
  - j) Sand drift and dust management;
  - k) Waste management;
  - l) Dewatering;
  - m) Traffic management; and
  - n) Works affecting pedestrian areas.

The approved Demolition and Construction Management Plan shall be adhered to throughout the demolition of the existing building on site and construction of the new development.

12. Prior to occupation, twelve (12) bicycle parking bays, in accordance with design element 5.3.3 of the Residential Design Codes, shall be provided and thereafter maintained to the satisfaction of the City of Fremantle.

#### ***Advice note(s)***

- i. The applicant is advised that determination as to whether conditions of approval have been satisfied is delegated to the Chief Executive Officer – City of Fremantle.*
- ii. The applicant is advised that the installation of air-conditioning plant and equipment is exempt from the need to obtain planning approval when it is positioned so as to not extend more than 1.0m above the highest point of an existing roof. Further planning approval of the City will be required if this height is exceeded.*

## Background:

Property Address:	No. 53-74/9 (Lot 802) McCabe Street, North Fremantle
Zoning	MRS: Urban
	LPS: Development Area
Use Class:	Multiple Dwellings – N/A
Strategy Policy:	Local Planning Area 3 – North Fremantle
Development Scheme:	City of Fremantle – Local Planning Scheme No. 4 – McCabe Street Structure Plan
Lot Size:	Lot 802: 5608m <sup>2</sup>
Existing Land Use:	N/A - Vacant
Value of Development:	Total cost of development: \$8,000,000 Variation to previous proposal: \$500,000 in additional costs

The application includes modifications to an application previously determined by the joint Development Assessment Panel (JDAP) pursuant to DAP80006/13 (DP/13/00843). A detailed discussion of the site background and history is contained in the preliminary section of **Attachment 3**.

## Details: outline of development application

The application seeks planning approval for 'Modification to approved five storey Multiple Dwelling Development (26 Multiple Dwellings) and basement vehicle parking level' (see **Attachment 2**). The modified proposal differs from that previously approved by the JDAP pursuant to DP/13/00843 in the following ways;

- An increase in the number of Multiple Dwellings from 20 dwellings to 26 dwellings;
- An increase in the external wall height of the proposal from 46.50m AHD to 49.50 AHD (+3.0m);
- An increase in the maximum roof height of the proposal from 47.10m AHD to 49.50m AHD (+2.4m);
- An increase in the volume of vehicle parking provided from 43 bays to 52 bays (inclusive of 4 external visitors bays);
- An increase in the plot ratio of the proposed building from 2,677m<sup>2</sup> to 2,730m<sup>2</sup> (+53m<sup>2</sup>).

## Legislation & policy:

The application has been assessed against the following legislative documents:

- City of Fremantle Local Planning Scheme No. 4 (LPS4) – application for development on the site is to be determined in accordance with provisions of Part 10 of the Scheme.

## Legislation

The legislative framework and policy base providing for the assessment and determination of the subject application is as follows:

- City of Fremantle Local Planning Scheme No. 4 (LPS4), including the following provisions;
  - Table 2 - Zoning;
  - Clause 6.2.3.2 – Structure plan requirements;
  - Clause 10.2 - Matters to be considered by Council;
  - Clause 11.8 – Design Advisory Committee; and,
  - Schedule 1 – Dictionary of Defined Words and Expressions.
- State Planning Policy 3.1 Residential Design Codes (R-Codes);
- Local Planning Policies including;
  - Local Planning Policy 1.3 – Public notifications of planning proposals;
  - Local Planning Policy 1.9 – Design Advisory Committee & Principles Of Design;
  - Local Planning Policy 2.13 - Sustainable Buildings Design Requirements; and,
  - Local Planning Policy 3.11 – McCabe Street Area – Height of New Buildings; and,

**Consultation:**

Design Advisory Committee (DAC) (Internal referral)

The original proposal was reviewed by the DAC at its meetings of 15 November and 4 December 2013. The proposal was supported by the DAC subject to a condition relating to further details relating to finishes and materials.

The proposal includes internal modifications which are likely to be of interest to the City's DAC. The previously recommended condition of the DAC regarding external colours, finishes and textures is still included. It is noted that the City's DAC considered matters of design and internal amenity. A number of the dwellings proposed include rooms which have no direct access to natural light. The City's DAC has typically raised concerns regarding such elements however it is considered that this matter alone is not a primary statutory planning matter that would require a redesign or a recommendation for refusal. On this basis it is balance recommended this aspect of the design is supported.

Should however these matters be of significant concern to Council, the following alternative recommendation is provided in the 'Conclusion' section of this report;

- That a condition of approval be applied requiring DAC consideration prior to the issue of a Building Permit.

Manners by which the DAC may wish to deal with the matter of internalised bedrooms includes configuration of internal spaces, inclusion of light wells/skylights where possible, amalgamation of some two bedroom apartment into single bedroom apartments.

Public Consultation

The application was required to be advertised in accordance with Clause 9.4 of the LPS4. In accordance with Local Planning Policy 1.3 – Public Notification of Planning Proposal, the application was advertised as a ‘Significant Application’. LPP1.3 specifies that significant applications should be advertised for 28 days. Due to the processing time constraints, the proposal can only be advertised for 26 days only (closing 16 December 2014). Advertising of the application included notice by the following means;

- A sign being placed on-site throughout the duration of the advertising period;
- Notice of the application being included on the City’s website, social media pages and in the local newspaper circulations;
- A community information session being held on 11 December 2014;;
- Written notice being provided to adjoining landowners within 100m of the subject site advising them of the proposal and inviting them to the information session specified above;
- Written notice being provided to elected members and ‘Precinct Groups’ of the City.

At the conclusion of the advertising period, the City had received 14 submissions including 14 objections. The objections raised in the submissions are summarised as follows;

- The proposal results in no benefit to the wider community and does not achieve a diversity in housing;
- The height of the proposal is too great and will impact on amenity and streetscape;
- There is a lack of parking available and the additional traffic volumes will impact on the safety of the local road network;
- The density of the development and the surrounding site is too great;
- View corridors will be impacted by the additional height and bulk of the proposal
- The proposal is inconsistent with the adopted structure plan;
- There is a lack of open space resulting from the proposal;
- The additional height should include less bulk and greater graduation of scale;
- Responses from the Town of Mosman Park should be given greater weight;
- Sufficient social, economic, transport, environmental and security assessments have not been undertaken;
- The proposal will result in a loss of privacy and solar access;
- If approved, the proposal will set an undesirable precedent for the introduction of greater height in the area.

#### Ordinary Council Meeting

The application was presented to the Ordinary Council Meeting (OCM) of the City on 17 December 2014. The OCM resolved as follows;

*‘Cr J Strachan MOVED to defer the item to the next appropriate Ordinary Meeting of Council to consider Application reference DP/13/00843 and accompanying plans dated 15 October 2013, SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4, so*

*that the proposal can be presented to the 12 January 2014 Design Advisory Committee meeting of the City of Fremantle for comment.'*

The conclusion section of this report includes an alternative recommendation and conditions which reflect the above resolution should the JDAP seek comments from the City's DAC regarding the proposal.

**Planning assessment:**

The subject land is zoned 'Development Area' and is not given a density coding. As a result, the assessment of the proposed development is largely provided for in the Structure Plan.

Clause 6.2.3.2 of LPS4 requires that;

*'The subdivision and development of land within a Development Area is to generally be in accordance with any structure plan that applies to the land.'*

Consideration of the development is therefore made, generally, against the content of the Structure Plan.

External Wall Height

The adopted Structure Plan provides a maximum external wall of 43.50m AHD and a maximum ridge height of 48.00m AHD for the subject site. Local Planning Policy 3.11 – McCabe Street (Height of New Buildings) provides a maximum height for development of 14.0m. As the Structure Plan provides a more specific height requirement for the land, the Structure Plan is preferred and the higher order statutory document (the policy was used to inform the structure plan).

The table below outlines the required (as per the adopted Structure Plan), previously approved and currently proposed AHD levels of the external wall height and maximum roof ridge height.

<b>Element</b>	<b>Structure Plan</b>	<b>Previously approved</b>	<b>Proposed</b>	<b>Variation to Structure Plan</b>
Maximum Wall	43.50m AHD	46.50m AHD	49.50m AHD	+6.00m AHD
Maximum Roof	48.00m AHD	47.10m AHD	49.50m AHD	+1.50m AHD

The merit based decision is supported for the following reasons;

- The Structure Plan outlines that buildings in the location of the proposed development should be limited to four storeys. The building proposed is clearly a five storey proposal but the addition storey is setback from the main building wall to all elevations. This not only reduces the bulk and scale of the building but takes advantage of space that would otherwise form a pitched roof. Given the setback of the additional floor, the proposal is not considered to be of greater bulk, scale or impact compared to a four storey proposal with a pitched roof or loft level;

- The portions of wall height that exist above the Structure Plan requirement are setback from the edge of the main building face, reducing the overall bulk of the proposal;
- The additional height, in particular the additional external wall height is not considered to unreasonably restrict access to direct light or views of significance to surrounding properties having regard to the separation distance between the development and the nearest residential property;
- The proposed modifications are considered minor in nature having regard to previous approvals and represent only a marginal departure from the recommendations of the Structure Plan;
- The additional height is unlikely to contribute to a loss of privacy or solar access given the nearest residential property is located approximately 30m away from the building.

### Bicycle parking

The modified proposal does not include any direct provision for bicycle parking bays. The deemed-to-comply criteria of the R-Codes specifies that 11.2 (12) bays could be provided and the proposal comply. A condition of approval requiring provision of these bays is therefore recommended.

### Visual Privacy

The Structure Plan outlines that the desired density of the subject site ought to be equivalent to R80; despite the fact that no density coding is directly applicable under the scheme maps of LPS4. On the basis of the R80 code, the proposal is considered to meet all of the deemed-to-comply requirements of the R-Codes in the following ways;

- All major openings and raised outdoor living areas to boundaries of adjoining residential properties (including the internal separation between the proposed development and other residential developments on the subject site) meet the cone-of-vision setback requirement of the R-Codes; or
- The elevations where a lesser setback is sought address non-residential premises at No. 19-21 McCabe Street) and in those instance, the visual privacy provisions do not apply.

### Sustainable Building Design

The proposed development is required to achieve a rating of not less than 4 Star Green Star (or equivalent) using the relevant Green Building Council of Australia rating tool pursuant to Local Planning Policy 2.13 – Sustainable Building Design (LPP2.13).

The applicant has proposed the use of an alternative energy efficiency rating system in lieu of the 4 Star Green Star requirements. Use of this rating tool is consistent with previous resolutions of the City and Joint Development Assessment panels relating to recent applications.

A condition of approval requiring achievement of an equivalent rating under the alternative tool is recommended.

## Construction Management

The officer's recommendation includes a condition relating to the submission of a construction management plan. The condition is intended to ensure construction of the development does not result in a loss of amenity to adjoining sites and also coordinates development in a complex location, such as at the subject site.

### **Conclusion:**

The application seeks planning approval for modifications to an approved five storey multiple dwelling development (26 Multiple dwellings) at the subject site. The proposal is summarised as follows;

- The subject site is zoned 'Development Area (DA18) pursuant to LPS4 and is subject to the adopted Structure Plan;
- In accordance with clause 6.2.3.2 of LPS4, the proposal is required to be generally in accordance with the Structure Plan;
- The proposed building envelope is very similar to that already approved by JDAP;
- The proposal is generally consistent with the Structure Plan in terms of building position, scale, density, height and vehicle traffic; and,
- The proposal is required to meet the sustainable building requirements of LPP2.13 and a condition of approval is recommended to this affect;

The application is considered to meet the relevant planning framework and is therefore recommended for conditional approval.

Given the scope of DAC comments, or potential concerns, are unknown it may be considered unreasonable to determine the application without any DAC comments. It is noted the City's OCM recommended that the application be deferred for this reason. The following alternative recommendation is provided to allow the application to be deferred so DAC comments can be sought;

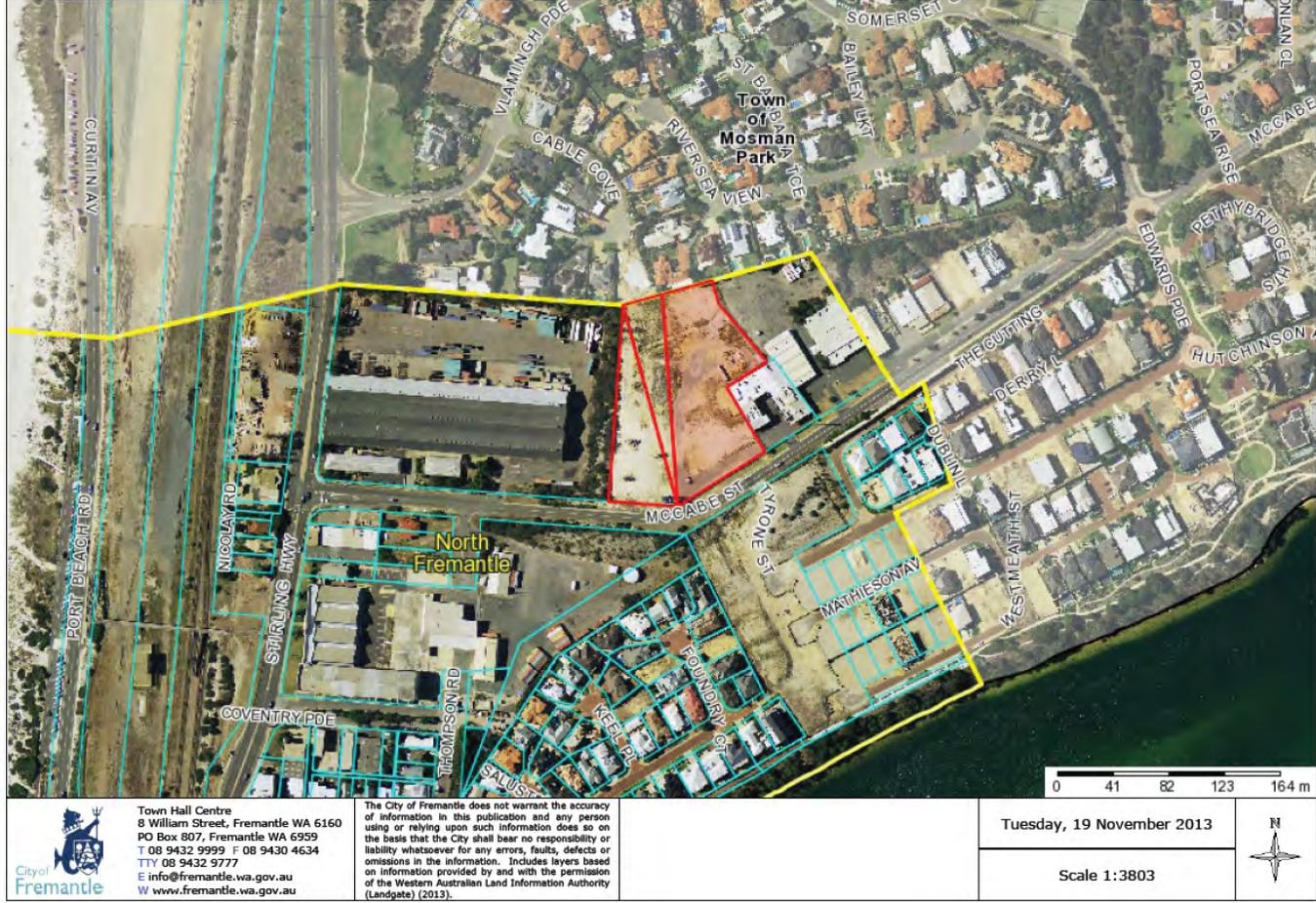
*'That the Metro South-West Joint Development Assessment Panel resolves to:*

*DEFER DAP Application reference DP/13/00843 and accompanying plans dated 15 October 2013, SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4, so that the proposal can be presented to the 12 January 2014 Design Advisory Committee meeting of the City of Fremantle for comment.'*

Alternatively, an additional condition could be included to ensure DAC consideration prior to the issue of a Building Permit. Such a condition (#13) could be as follows;

*'Prior to the issue of a Building Permit, the plans hereby approved are to be presented to the City's Design Advisory Committee for comment specifically relating to the internal bedrooms. Any recommendation made by the Design Advisory Committee shall be given regard and the plans modified thereto, to the satisfaction of the Chief Executive Officer – City of Fremantle.'*

## **ATTACHMENT 1: Location Plan**




 Town Hall Centre  
 8 William Street, Fremantle WA 6160  
 PO Box 807, Fremantle WA 6959  
 T 08 9432 9999 F 08 9430 4634  
 TTY 08 9432 9777  
 E info@fremantle.wa.gov.au  
 W www.fremantle.wa.gov.au

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Tuesday, 19 November 2013

Scale 1:3803



**ATTACHMENT 2: Development Plans dated 12 November 2014**

Project : TASKERS  
Lots 800, 801, 802 & 803, McCabe Street, North Fremantle.  
Multi Residential Development - "Tekona" - West Building



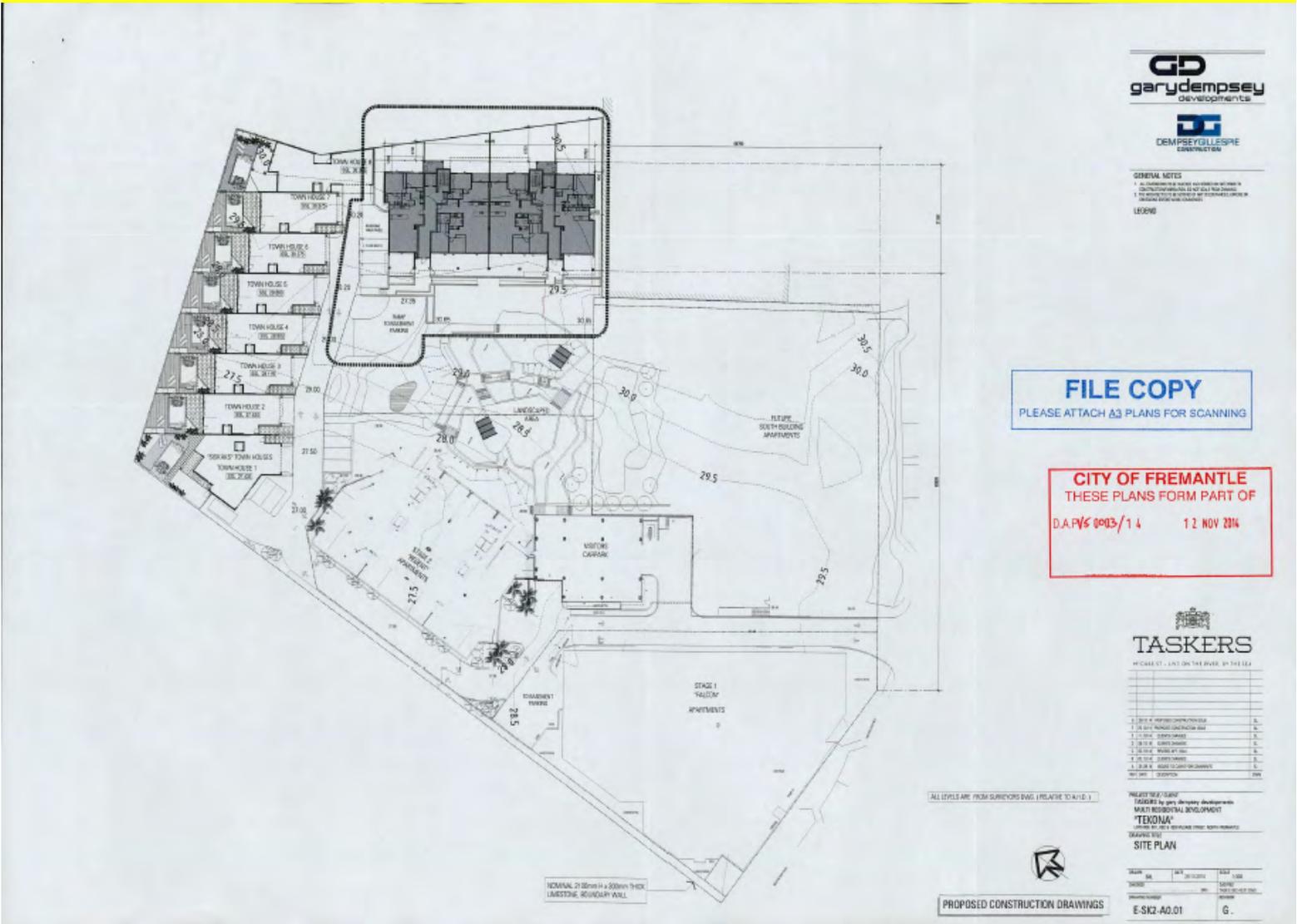
Architectural Drawings :

- A0.01 1:300 Site Plan
- A1.01 1:100 Basement Level Floor Plan
- A1.02 1:100 Level 1 Floor Plan
- A1.03 1:100 Level 2 Floor Plan
- A1.04 1:100 Level 3 Floor Plan
- A1.05 1:100 Level 4 Floor Plan
- A1.06 1:100 Level 5 Floor Plan
- A2.01 1:100 Elevations West & South
- A2.02 1:100 Elevations East & North
- A2.03 1:100 Sections

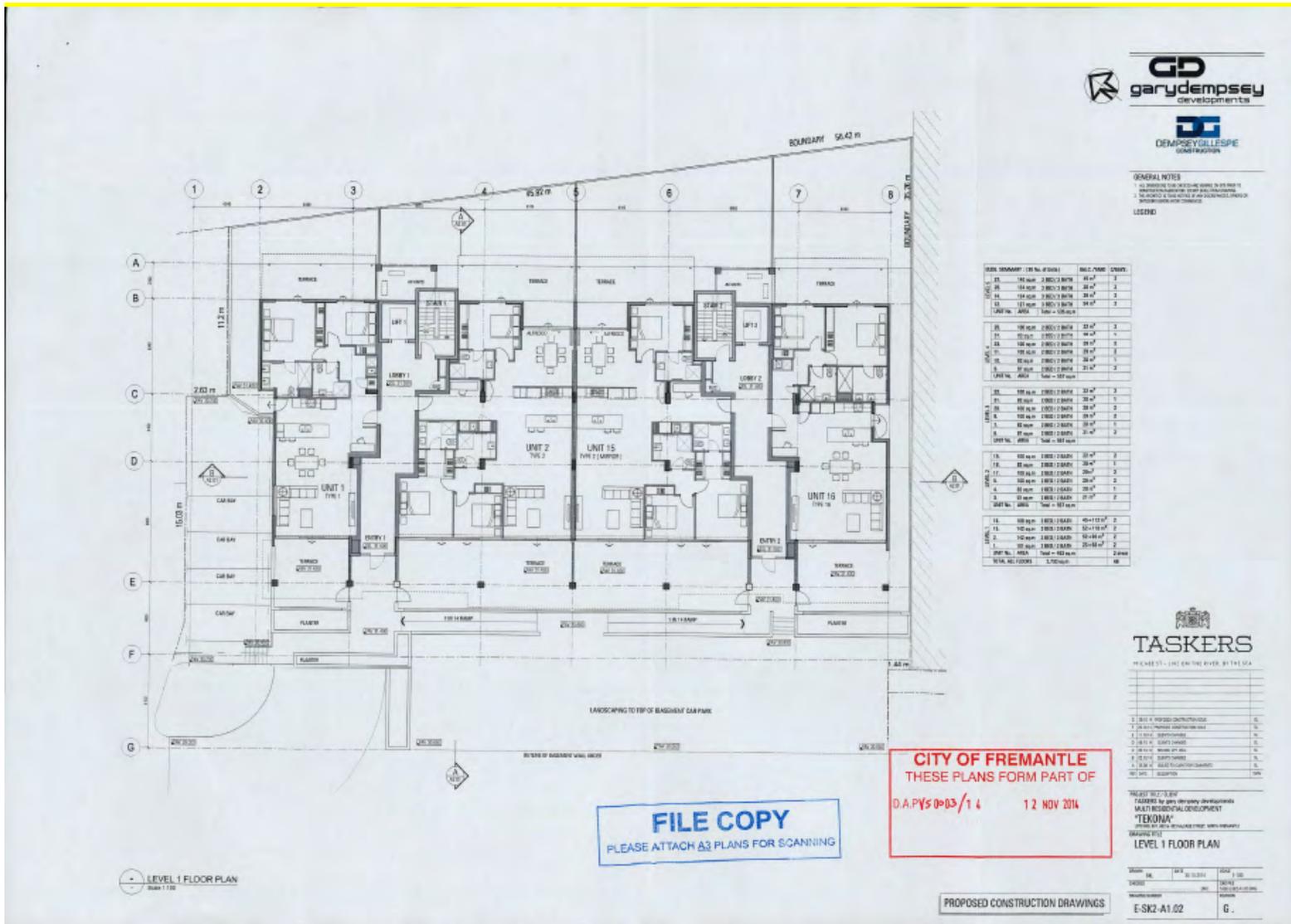
PROPOSED CONSTRUCTION DRAWINGS

**FILE COPY**  
PLEASE ATTACH A3 PLANS FOR SCANNING

**CITY OF FREMANTLE**  
THESE PLANS FORM PART OF  
D.A.P/S 0003/14 12 NOV 2014







**GENERAL NOTES**  
 1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE SPECIFIED.  
 2. DIMENSIONS TO FACE UNLESS OTHERWISE SPECIFIED.  
 3. DIMENSIONS TO FACE UNLESS OTHERWISE SPECIFIED.  
**LEGEND**

UNIT SUMMARY (BY No. of Units)	NO. OF UNITS	UNIT TYPE
1. 15 - 150 sqm - 3 BDRM 2 BATH	15	1
2. 15 - 150 sqm - 3 BDRM 2 BATH	15	2
3. 15 - 150 sqm - 3 BDRM 2 BATH	15	3
4. 15 - 150 sqm - 3 BDRM 2 BATH	15	4
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
5. 15 - 150 sqm - 3 BDRM 2 BATH	15	5
6. 15 - 150 sqm - 3 BDRM 2 BATH	15	6
7. 15 - 150 sqm - 3 BDRM 2 BATH	15	7
8. 15 - 150 sqm - 3 BDRM 2 BATH	15	8
9. 15 - 150 sqm - 3 BDRM 2 BATH	15	9
10. 15 - 150 sqm - 3 BDRM 2 BATH	15	10
11. 15 - 150 sqm - 3 BDRM 2 BATH	15	11
12. 15 - 150 sqm - 3 BDRM 2 BATH	15	12
13. 15 - 150 sqm - 3 BDRM 2 BATH	15	13
14. 15 - 150 sqm - 3 BDRM 2 BATH	15	14
15. 15 - 150 sqm - 3 BDRM 2 BATH	15	15
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
16. 15 - 150 sqm - 3 BDRM 2 BATH	15	16
17. 15 - 150 sqm - 3 BDRM 2 BATH	15	17
18. 15 - 150 sqm - 3 BDRM 2 BATH	15	18
19. 15 - 150 sqm - 3 BDRM 2 BATH	15	19
20. 15 - 150 sqm - 3 BDRM 2 BATH	15	20
21. 15 - 150 sqm - 3 BDRM 2 BATH	15	21
22. 15 - 150 sqm - 3 BDRM 2 BATH	15	22
23. 15 - 150 sqm - 3 BDRM 2 BATH	15	23
24. 15 - 150 sqm - 3 BDRM 2 BATH	15	24
25. 15 - 150 sqm - 3 BDRM 2 BATH	15	25
26. 15 - 150 sqm - 3 BDRM 2 BATH	15	26
27. 15 - 150 sqm - 3 BDRM 2 BATH	15	27
28. 15 - 150 sqm - 3 BDRM 2 BATH	15	28
29. 15 - 150 sqm - 3 BDRM 2 BATH	15	29
30. 15 - 150 sqm - 3 BDRM 2 BATH	15	30
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
31. 15 - 150 sqm - 3 BDRM 2 BATH	15	31
32. 15 - 150 sqm - 3 BDRM 2 BATH	15	32
33. 15 - 150 sqm - 3 BDRM 2 BATH	15	33
34. 15 - 150 sqm - 3 BDRM 2 BATH	15	34
35. 15 - 150 sqm - 3 BDRM 2 BATH	15	35
36. 15 - 150 sqm - 3 BDRM 2 BATH	15	36
37. 15 - 150 sqm - 3 BDRM 2 BATH	15	37
38. 15 - 150 sqm - 3 BDRM 2 BATH	15	38
39. 15 - 150 sqm - 3 BDRM 2 BATH	15	39
40. 15 - 150 sqm - 3 BDRM 2 BATH	15	40
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
41. 15 - 150 sqm - 3 BDRM 2 BATH	15	41
42. 15 - 150 sqm - 3 BDRM 2 BATH	15	42
43. 15 - 150 sqm - 3 BDRM 2 BATH	15	43
44. 15 - 150 sqm - 3 BDRM 2 BATH	15	44
45. 15 - 150 sqm - 3 BDRM 2 BATH	15	45
46. 15 - 150 sqm - 3 BDRM 2 BATH	15	46
47. 15 - 150 sqm - 3 BDRM 2 BATH	15	47
48. 15 - 150 sqm - 3 BDRM 2 BATH	15	48
49. 15 - 150 sqm - 3 BDRM 2 BATH	15	49
50. 15 - 150 sqm - 3 BDRM 2 BATH	15	50
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
51. 15 - 150 sqm - 3 BDRM 2 BATH	15	51
52. 15 - 150 sqm - 3 BDRM 2 BATH	15	52
53. 15 - 150 sqm - 3 BDRM 2 BATH	15	53
54. 15 - 150 sqm - 3 BDRM 2 BATH	15	54
55. 15 - 150 sqm - 3 BDRM 2 BATH	15	55
56. 15 - 150 sqm - 3 BDRM 2 BATH	15	56
57. 15 - 150 sqm - 3 BDRM 2 BATH	15	57
58. 15 - 150 sqm - 3 BDRM 2 BATH	15	58
59. 15 - 150 sqm - 3 BDRM 2 BATH	15	59
60. 15 - 150 sqm - 3 BDRM 2 BATH	15	60
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
61. 15 - 150 sqm - 3 BDRM 2 BATH	15	61
62. 15 - 150 sqm - 3 BDRM 2 BATH	15	62
63. 15 - 150 sqm - 3 BDRM 2 BATH	15	63
64. 15 - 150 sqm - 3 BDRM 2 BATH	15	64
65. 15 - 150 sqm - 3 BDRM 2 BATH	15	65
66. 15 - 150 sqm - 3 BDRM 2 BATH	15	66
67. 15 - 150 sqm - 3 BDRM 2 BATH	15	67
68. 15 - 150 sqm - 3 BDRM 2 BATH	15	68
69. 15 - 150 sqm - 3 BDRM 2 BATH	15	69
70. 15 - 150 sqm - 3 BDRM 2 BATH	15	70
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
71. 15 - 150 sqm - 3 BDRM 2 BATH	15	71
72. 15 - 150 sqm - 3 BDRM 2 BATH	15	72
73. 15 - 150 sqm - 3 BDRM 2 BATH	15	73
74. 15 - 150 sqm - 3 BDRM 2 BATH	15	74
75. 15 - 150 sqm - 3 BDRM 2 BATH	15	75
76. 15 - 150 sqm - 3 BDRM 2 BATH	15	76
77. 15 - 150 sqm - 3 BDRM 2 BATH	15	77
78. 15 - 150 sqm - 3 BDRM 2 BATH	15	78
79. 15 - 150 sqm - 3 BDRM 2 BATH	15	79
80. 15 - 150 sqm - 3 BDRM 2 BATH	15	80
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
81. 15 - 150 sqm - 3 BDRM 2 BATH	15	81
82. 15 - 150 sqm - 3 BDRM 2 BATH	15	82
83. 15 - 150 sqm - 3 BDRM 2 BATH	15	83
84. 15 - 150 sqm - 3 BDRM 2 BATH	15	84
85. 15 - 150 sqm - 3 BDRM 2 BATH	15	85
86. 15 - 150 sqm - 3 BDRM 2 BATH	15	86
87. 15 - 150 sqm - 3 BDRM 2 BATH	15	87
88. 15 - 150 sqm - 3 BDRM 2 BATH	15	88
89. 15 - 150 sqm - 3 BDRM 2 BATH	15	89
90. 15 - 150 sqm - 3 BDRM 2 BATH	15	90
<b>UNIT No. AREA</b>	<b>Total = 600 sqm</b>	
91. 15 - 150 sqm - 3 BDRM 2 BATH	15	91
92. 15 - 150 sqm - 3 BDRM 2 BATH	15	92
93. 15 - 150 sqm - 3 BDRM 2 BATH	15	93
94. 15 - 150 sqm - 3 BDRM 2 BATH	15	94
95. 15 - 150 sqm - 3 BDRM 2 BATH	15	95
96. 15 - 150 sqm - 3 BDRM 2 BATH	15	96
97. 15 - 150 sqm - 3 BDRM 2 BATH	15	97
98. 15 - 150 sqm - 3 BDRM 2 BATH	15	98
99. 15 - 150 sqm - 3 BDRM 2 BATH	15	99
100. 15 - 150 sqm - 3 BDRM 2 BATH	15	100

**FILE COPY**  
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**CITY OF FREMANTLE**  
 THESE PLANS FORM PART OF  
 D.A.PV5 0-03/1 4      1 2 NOV 2014



**REVISIONS**

NO.	DATE	DESCRIPTION
1	12 NOV 2014	ISSUED FOR PERMIT
2		
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10		

**LEVEL 1 FLOOR PLAN**

NO.	DATE	DESCRIPTION
1	12 NOV 2014	ISSUED FOR PERMIT
2		
3		
4		
5		
6		
7		
8		
9		
10		

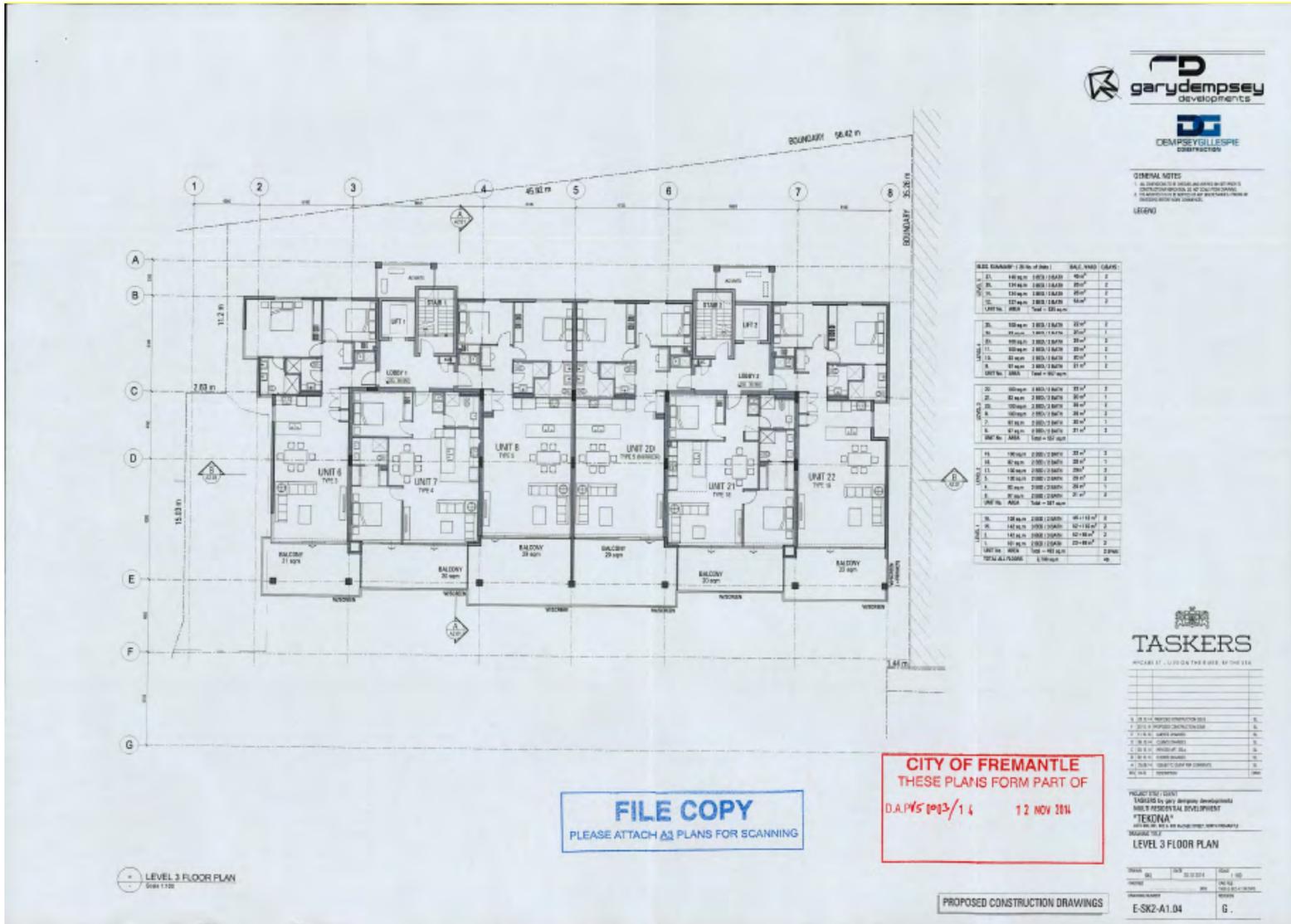
**PROPOSED CONSTRUCTION DRAWINGS**

NO.	DATE	DESCRIPTION
1	12 NOV 2014	ISSUED FOR PERMIT
2		
3		
4		
5		
6		
7		
8		
9		
10		

**E-SK2-A1.02**

LEVEL 1 FLOOR PLAN  
 Scale 1:100





**GENERAL NOTES**  
 1. ALL DIMENSIONS ARE UNLESS OTHERWISE SPECIFIED IN METRIC UNITS.  
 2. DIMENSIONS TO FACE UNLESS OTHERWISE SPECIFIED.  
 3. ALL DIMENSIONS TO FACE UNLESS OTHERWISE SPECIFIED.  
 4. ALL DIMENSIONS TO FACE UNLESS OTHERWISE SPECIFIED.  
**LEGEND**

NO.	DESCRIPTION	AREA (sqm)	NO.	DESCRIPTION	AREA (sqm)
1	UNIT 1	45.00	11	UNIT 11	45.00
2	UNIT 2	45.00	12	UNIT 12	45.00
3	UNIT 3	45.00	13	UNIT 13	45.00
4	UNIT 4	45.00	14	UNIT 14	45.00
5	UNIT 5	45.00	15	UNIT 15	45.00
6	UNIT 6	45.00	16	UNIT 16	45.00
7	UNIT 7	45.00	17	UNIT 17	45.00
8	UNIT 8	45.00	18	UNIT 18	45.00
9	UNIT 9	45.00	19	UNIT 19	45.00
10	UNIT 10	45.00	20	UNIT 20	45.00
21	UNIT 21	45.00	22	UNIT 22	45.00
23	UNIT 23	45.00	24	UNIT 24	45.00
25	UNIT 25	45.00	26	UNIT 26	45.00
27	UNIT 27	45.00	28	UNIT 28	45.00
29	UNIT 29	45.00	30	UNIT 30	45.00
31	UNIT 31	45.00	32	UNIT 32	45.00
33	UNIT 33	45.00	34	UNIT 34	45.00
35	UNIT 35	45.00	36	UNIT 36	45.00
37	UNIT 37	45.00	38	UNIT 38	45.00
39	UNIT 39	45.00	40	UNIT 40	45.00
41	UNIT 41	45.00	42	UNIT 42	45.00
43	UNIT 43	45.00	44	UNIT 44	45.00
45	UNIT 45	45.00	46	UNIT 46	45.00
47	UNIT 47	45.00	48	UNIT 48	45.00
49	UNIT 49	45.00	50	UNIT 50	45.00
51	UNIT 51	45.00	52	UNIT 52	45.00
53	UNIT 53	45.00	54	UNIT 54	45.00
55	UNIT 55	45.00	56	UNIT 56	45.00
57	UNIT 57	45.00	58	UNIT 58	45.00
59	UNIT 59	45.00	60	UNIT 60	45.00
61	UNIT 61	45.00	62	UNIT 62	45.00
63	UNIT 63	45.00	64	UNIT 64	45.00
65	UNIT 65	45.00	66	UNIT 66	45.00
67	UNIT 67	45.00	68	UNIT 68	45.00
69	UNIT 69	45.00	70	UNIT 70	45.00
71	UNIT 71	45.00	72	UNIT 72	45.00
73	UNIT 73	45.00	74	UNIT 74	45.00
75	UNIT 75	45.00	76	UNIT 76	45.00
77	UNIT 77	45.00	78	UNIT 78	45.00
79	UNIT 79	45.00	80	UNIT 80	45.00
81	UNIT 81	45.00	82	UNIT 82	45.00
83	UNIT 83	45.00	84	UNIT 84	45.00
85	UNIT 85	45.00	86	UNIT 86	45.00
87	UNIT 87	45.00	88	UNIT 88	45.00
89	UNIT 89	45.00	90	UNIT 90	45.00
91	UNIT 91	45.00	92	UNIT 92	45.00
93	UNIT 93	45.00	94	UNIT 94	45.00
95	UNIT 95	45.00	96	UNIT 96	45.00
97	UNIT 97	45.00	98	UNIT 98	45.00
99	UNIT 99	45.00	100	UNIT 100	45.00

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**CITY OF FREMANTLE**  
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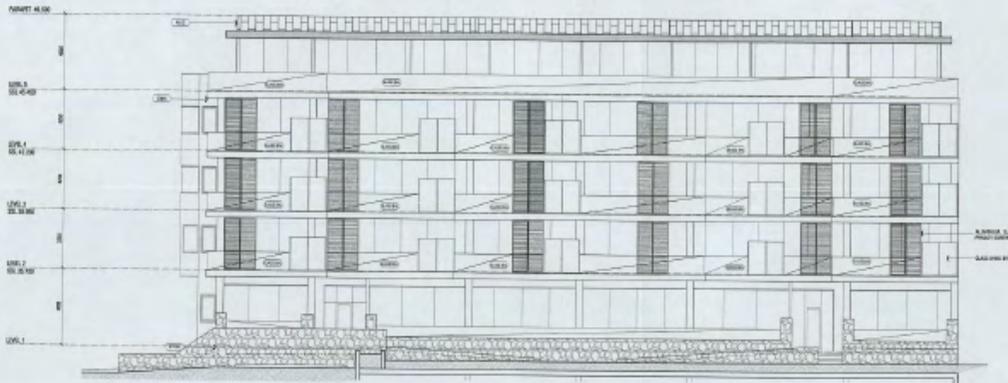
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 PLEASE ATTACH ALL PLANS FOR SCANNING

PROPOSED CONSTRUCTION DRAWINGS  
 E-SK2-A1.04 G.

LEVEL 3 FLOOR PLAN  
 Sheet 1/12

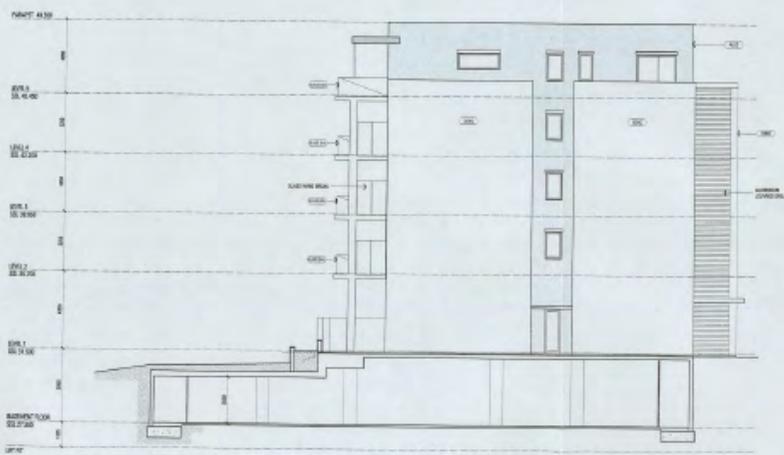






**GENERAL NOTES**  
 1. ALL DIMENSIONS ARE UNLESS OTHERWISE STATED IN THE NOTES.  
 2. DIMENSIONS TO FACE UNLESS OTHERWISE STATED.  
 3. DIMENSIONS TO CENTERLINE UNLESS OTHERWISE STATED.  
 4. DIMENSIONS TO CENTERLINE UNLESS OTHERWISE STATED.  
**LEGEND**

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2 SOUTH ELEVATION  
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**CITY OF FREMANTLE**  
 THESE PLANS FORM PART OF  
 D.P.V.S 0003/14 12 NOV 2014



NO.	DESCRIPTION	DATE
1	ISSUE FOR PERMIT APPLICATION	12/11/14
2	ISSUE FOR PERMIT APPLICATION	12/11/14
3	ISSUE FOR PERMIT APPLICATION	12/11/14
4	ISSUE FOR PERMIT APPLICATION	12/11/14
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9	ISSUE FOR PERMIT APPLICATION	12/11/14
10	ISSUE FOR PERMIT APPLICATION	12/11/14

THESE PLANS FORM PART OF  
 TASKERS by gary dempsey developments  
 MULTI RESIDENTIAL DEVELOPMENT  
 'TEKONAY'  
 (SECTION 44(1) OF THE BUILDING ACT 2011)  
**ELEVATIONS WEST & SOUTH**

PROPOSED CONSTRUCTION DRAWINGS  
 E-SK2-A2.01 G.

**3 EAST ELEVATION**  
Scale 1:50

**4 NORTH ELEVATION**  
Scale 1:50

PROPOSED CONSTRUCTION DRAWINGS

**GD**  
garydempsey  
developments

**D**  
DEMPSEYGILLESPIE  
CONSTRUCTION

**GENERAL NOTES**

1. ALL DIMENSIONS GIVEN UNLESS OTHERWISE SPECIFIED.
2. ALL WORK TO BE ACCORDING TO THE CURRENT EDITIONS OF THE NATIONAL BUILDING REGULATIONS AND ALL APPLICABLE STANDARDS.
3. ALL WORK TO BE ACCORDING TO THE CURRENT EDITIONS OF THE NATIONAL BUILDING REGULATIONS AND ALL APPLICABLE STANDARDS.

**LEGEND**

SEKAS' TRISE No. 8  
43/300 MO

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**CITY OF FREMANTLE**  
THESE PLANS FORM PART OF  
D.A.P.V.S.D.013/14 12 NOV 2014

**TASKERS**  
ARCHITECTS

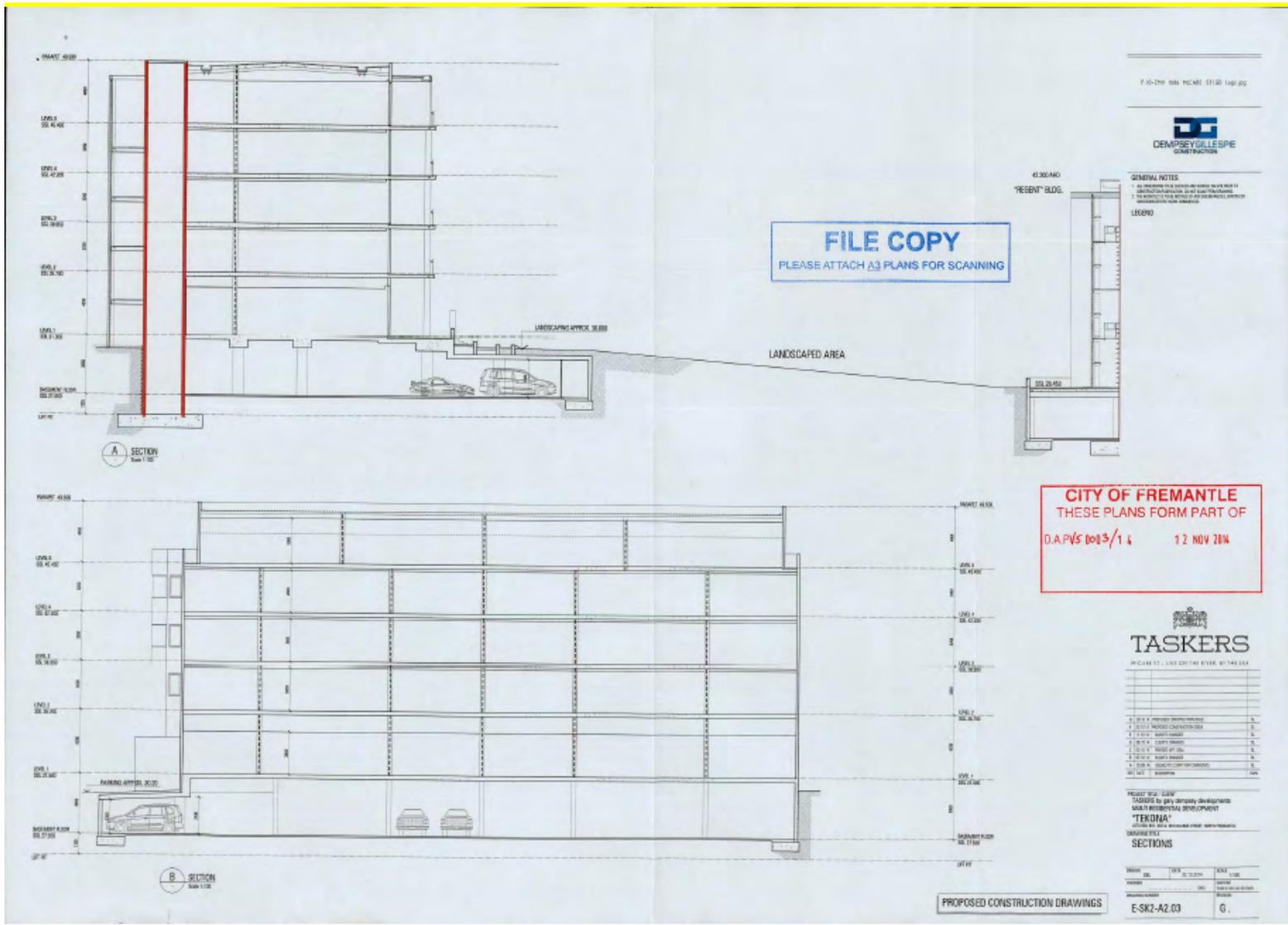
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PROJECT TITLE: TASKERS  
TASKERS by Gary Dempsey Developments  
1000 W. INDUSTRIAL DEVELOPMENT  
"TERRAZA"  
1000 W. INDUSTRIAL DEVELOPMENT

**WORKING TITLE:**  
ELEVATIONS EAST & NORTH

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**ATTACHMENT 3: Previous Responsible Authority Report (DAP80006/13)  
(DP/13/00843)**

**Form 1 - Responsible Authority Report  
(Regulation 12)**

<b>Application Details:</b>	Five Storey Multiple Dwelling Development (20 Multiple Dwellings) and Basement Vehicle Parking Level
<b>Property Location:</b>	No. 9-11 (Lots 18, 315 and 326) McCabe Street, North Fremantle
<b>DAP Name:</b>	Metro South-West JDAP
<b>Applicant:</b>	TPG Town Planning, Urban Design and Heritage
<b>Owner:</b>	Taskers Living Pty Ltd
<b>LG Reference:</b>	DAP80006/13
<b>Responsible Authority:</b>	City of Fremantle
<b>Authorising Officer:</b>	Manager Statutory Planning
<b>Department of Planning File No:</b>	DP/13/00843
<b>Report Date:</b>	20 December 2013
<b>Application Receipt Date:</b>	14 October 2013
<b>Application Process Days:</b>	90 days
<b>Attachment(s):</b>	1: Location Plan 2: Development Plans dated 15 October 2013 (SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01)

**Recommendation:**

**That the Metro South-West Joint Development Assessment Panel resolves to:**

**Approve DAP Application reference DP/13/00843 and accompanying plans dated 15 October 2013, SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4, subject to the following conditions:**

- 1. This approval relates only to the development as indicated on the approved plans, dated 15 October 2013. It does not relate to any other development on this lot and must substantially commence within four years from the date of this decision letter.**
- 2. The design and construction of the development is to meet the 4 star green star standard as per Local Planning Policy 2.13 or alternatively to an equivalent standard as agreed upon by the Chief Executive Officer, City of Fremantle when a green star rating tool is not available. Any costs associated with generating, reviewing or modifying the alternative equivalent standard is to be incurred by the owner of the development site. Within 12 months of an issue of a certificate of**

**Building Compliance for the development, the owner shall submit either of the following to the City to the satisfaction of the Chief Executive Officer – City of Fremantle;**

- i. a copy of documentation from the Green Building Council of Australia certifying that the development achieves a Green Star Rating of at least 4 Stars, or**
  - ii. a copy of agreed equivalent documentation for instance where there is no green star rating tool available certifying that the development achieves a Green Star Rating of at least 4 Stars.**
- 3. All storm water discharge shall be contained and disposed of on-site.**
- 4. Prior to occupation, all air-conditioning plant, satellite dishes, antennae and any other plant and equipment to the roof of the building shall be located or screened so as not to be highly visible from beyond the boundaries of the development site to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 5. All landscaped areas are to be maintained on an ongoing basis for the life of the development on the site to the satisfaction of the Chief Executive Officer, City of Fremantle**
- 6. Prior to the occupation of the development, vehicle crossovers shall be constructed in either paving block, concrete, or bitumen and thereafter maintained to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 7. Prior to occupation of the development, the car parking and loading area(s), and vehicle access and circulation areas shown on the approved site plan, including the provision of disabled car parking, shall be constructed, drained, and line marked and provided in accordance with Clause 5.7.1(a) of the City of Fremantle Local Planning Scheme No.4, to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 8. All car parking, and vehicle access and circulation areas shall be maintained and available for car parking/loading, and vehicle access and circulation on an ongoing basis to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 9. Prior to the submission of a Building Permit application, the owner is to submit further details on the storage and management of the waste generated by the development to the satisfaction of and approval by the Chief Executive Officer, City of Fremantle.**
- 10. Prior to the issue of a building permit the Applicant is to provide details of facade design and a schedule of colours, finishes and textures to the satisfaction of the Chief Executive Office, City of Fremantle, on advice of the Design Advisory Committee.**

## Background:

Property Address:	No. 9-11 (Lots 18, 315 and 326) McCabe Street, North Fremantle
Zoning	MRS: Urban
	LPS: Development Area
Use Class:	N/A
Strategy Policy:	Local Planning Area 3 – North Fremantle
Development Scheme:	City of Fremantle – Local Planning Scheme No. 4 – McCabe Street Structure Plan
Lot Size:	Lot 18: 8,059m <sup>2</sup> , Lot 315: 3,743m <sup>2</sup> , Lot 326: 2318m <sup>2</sup>
Existing Land Use:	N/A - Vacant
Value of Development:	\$8,000,000

The subject site consists of three lots; Lot 18, 315 and 326 McCabe Street, North Fremantle ('the site') (see **Attachment 1 – Location Plan**). The subject site is mostly vacant of improvements however a multiple dwelling development subject to prior approval of the City is under construction in the south-west corner of the subject site.

The subject site is located within the North Fremantle Local Planning Area pursuant to Local Planning Scheme No. 4 ('LPS4') and exists in the portion of McCabe Street bound by Stirling Highway to the west, and Edwards Parade, Mosman Park, to the east.

The subject site also subject to the following classifications;

- Zoned 'Development Area (DA18)' pursuant to LPS4 and subject to the McCabe Street Structure Plan;
- Contained within the North Fremantle Heritage Area but is not individually listed on the City's Heritage List. The subject site also falls within the 'Rocky Bay' Aboriginal Heritage Area; and,
- The subject site is noted as being adjacent to high voltage transmission lines located on McCabe Street.

A summary of the relevant planning history relating to the site is as follows;

- On 3 March 2009, the SAT set aside a decision made by the City on 25 June 2008 to refuse the Structure Plan for the subject site and approved the Structure Plan. The Structure Plan outlines a maximum wall height of 43.50m AHD and a maximum ridge height of 48.00m AHD. The Structure Plan contemplates a ridged roof design, as opposed to a flat or skillion style roof with higher wall or gable ends;
- On 9 June 2010, the City granted planning approval for 'Residential Grouped and Multiple Dwelling Development' (DA0087/10). This application consisted of the construction of ninety-five (95) multiple dwellings across four buildings and eight (8) grouped dwellings in an additional building at the land. A further variation to this approval increasing the number of multiple dwellings from 95 to 96 and making

minor modifications to common areas and on-site parking was approved by the City on 13 April 2011 (VA0009/11). Condition 14 of the original approval (DA0087/10) required that the land be amalgamated into one lot;

- On 9 June 2012, planning approval DA0087/10 and VA0009/11, being the original and subsequently modified approval for the redevelopment of the land expired;
- On 25 October 2012, the City approved an application for 'Six Storey (Two Basement), 51 Apartment Multiple Dwelling Development' at the subject site (DA0417/12). This proposal broadly reflected that approved as part of DA0087/10 and VA0009/11;
- At its meeting of 15 May 2013, the City granted planning approval for modifications to DA0417/12 that included modifications to the external height of the proposal (DA0176/13); and,
- The application forms part of the staged development of the subject site. The first stage of the proposal, being the 6 storey multiple dwelling development approved as DA0417/12 (and subsequently DA0176/13), is currently under construction. Two further applications DA0448/13 (8 Grouped Dwellings) and DA0449/13 (21 multiple dwellings), represent the second and third stages of the proposal. The current proposal DAP80006/13 (20 multiple dwellings) represents the fourth stage of the development. At this time, the fifth and final stage, shown indicatively on the development plans in the south-east corner of the subject site has not been submitted to the City.

### **Details: outline of development application**

The application seeks planning approval for a 'Five Storey Multiple Dwelling Development (20 Multiple Dwellings) and Basement Vehicle Parking Level' ('the proposed development') (see **Attachment 2 – Development Plans dated 15 October 2013**) at the site including;

- A five (5) storey multiple dwelling building containing 20 residential dwellings and an additional basement level containing vehicle parking;
- The building having a maximum external wall height of 46.50m AHD and a maximum roof height of 47.1m AHD;
- 39 vehicle parking bays being provided in the basement level of the building including 14 tandem bays. 4 visitor bays are also provided to the northern side of the building;
- The ground floor containing lobby and service areas as well as 5 multiple dwelling;
- The first, second and third floors all containing service areas and an additional 5 dwellings per floor;
- The fourth (most upper floor) containing additional living and bedroom areas for three of the dwelling accessed via the third floor; and,
- The total plot ratio of dwelling proposed as determined by the R-Codes being 2,677.2m<sup>2</sup>.

### **Legislation & policy:**

The application has been assessed against the following legislative documents:

- City of Fremantle Local Planning Scheme No. 4 (LPS4) – application for development on the site is to be determined in accordance with provisions of Part 10 of the Scheme.

### Legislation

The following provisions of LPS4 are considered relevant in the assessment of the planning application;

- Clause 6.2.3 – Subdivision and Development in Development Areas;
- Clause 9.4 – Advertising of Applications;
- Clause 2.3 – Relationship of Local Planning Policies to Scheme; and,
- Clause 10.2 – Matters to be considered by the Council.
- Clause 11.8 – Design Advisory Committee

### State Government Policies

There are no State Government policies relevant to the assessment of the application.

### **Local Policies**

The proposed development is subject to the following Local Planning Policies, adopted under LPS4;

- Local Planning Policy 3.11 – McCabe Street Area – Height of New Buildings; and,
- Local Planning Policy 2.13 – Sustainable Building Design Requirements.
- Local Planning Policy 1.9 - *Design Advisory Committee and Principles of Design*;

### **Consultation:**

#### Public Consultation

The application was required to be advertised in accordance with Clause 9.4 of the LPS4. In accordance with Local Planning Policy 1.3 – Public Notification of Planning Proposal, the application was advertised as a ‘Significant Application’. At the conclusion of the advertising period, being 15 November 2013, the City had received one submission including 1 objection. The issues raised are summarised as follows;

- *Vehicle traffic*; Concern is raised in regards to the vehicle traffic the proposal will generate once the site is fully developed;
- *Cycle route*; No major cycle route along McCabe Street will become hazardous, especially at the intersection of Stirling Highway.

Consideration of the matters raised is discussed further in the Planning Assessment section of this report.

Planning Services Committee (Internal referral)

The application was presented to the City's Planning Services Committee ('PSC') meeting of 4 December 2013. The PSC resolved to adopt the same recommendation as recommend in this report.

Design Advisory Committee (DAC) (Internal referral)

The proposal was presented to the City's Design Advisory Committee at its meeting of 15 November 2013. The following recommendations were made;

*"The Committee was not able to provide detailed advice as the required information was not provided.*

*It is acknowledged that the development previously had planning approval however it is the role of this committee to provide advice relating to design quality. The statutory planning framework has changed since the original planning approval was issued and now requires that this development be referred to the DAC for comment.*

*The Committee would be happy to give further consideration to the proposal once additional information has been provided. This will need to include:*

1. *Streetscape character analysis;*
2. *Design matters raised in Local Planning Policy 1.9 - Design Advisory Committee and Principles of Design;*
3. *Response to the CABA Alarm Bells;*
4. *Site sections and sectional elevations;*
5. *Materials and finishes;*
6. *Detailing;*
7. *Durability;*
8. *Comprehensibility (how an individual navigates the site)."*

As explained earlier in this report, the application was presented to the City's PSC meeting of 4 December 2013. At this meeting, the Committee resolved to recommend approval of the application subject to a condition requiring further design details to the satisfaction of the Chief Executive Office, City of Fremantle.

The application has not been re-presented to the City's DAC. Consistent with the advice of the PSC, a condition of approval requiring further design details is recommended.

**Planning assessment:**

The subject land is zoned 'Development Area' and is not given a density coding. As a result, the assessment of the proposed development is largely provided for in the Structure Plan.

Clause 6.2.3.2 of LPS4 requires that;

*'The subdivision and development of land within a Development Area is to generally be in accordance with any structure plan that applies to the land.'* [Emphasis added].

Consideration of the development is therefore made, generally, against the content of the Structure Plan.

### **Building location & form**

The design context for the proposed development has been broadly set by decisions at earlier stages of the planning process, notably the approval of a structure plan for the site by the State Administrative Tribunal in March 2009. The structure plan included details of the location and orientation of new buildings within the site and their overall massing including maximum heights. Subsequent development applications have proposed building forms in general conformity with the content of the approved structure plan as is required under clause 6.2.3.2 of LPS4. These previous applications were not considered by the DAC as the committee was only established after the original planning approvals were granted..

The applicant was previously advised of the requirements for presentations to the DAC that include the 8 matters raised above. The architect for the proposal was not present at the DAC meeting to give an explanation of the architectural design rationale for the proposal. Consideration of matters by the DAC is important to the assessment of the application in terms of its design and function and is required to be given due regard pursuant to clause 11.8.5 of LPS4.

The proposed development is considered to be generally consistent with the requirements of the Structure Plan, however as per the recommendation of the DAC, it is considered that there is insufficient information provided in respect to certain design elements of the proposal.

As a result a condition of approval requiring further design details being submitted and endorsed by the City is recommended.

### **External Wall Height**

The adopted Structure Plan provides a maximum external wall of 43.50m AHD and a maximum ridge height of 48.00m AHD for the subject site. Local Planning Policy 3.11 – McCabe Street (Height of New Buildings) provides a maximum height for development of 14.0m. As the Structure Plan provides a more specific height requirement for the land, the Structure Plan is preferred and the higher order statutory document (the policy was used to inform the structure plan).

The table below outlines the required (as per the adopted Structure Plan), previously approved and currently proposed AHD levels of the external wall height and maximum roof ridge height.

<b>Element</b>	<b>Structure Plan Requirement</b>	<b>Current Proposal</b>	<b>Variation to Structure Plan</b>
Maximum Wall	43.50m AHD	46.50m AHD	3.00m AHD

Maximum Roof	48.00m AHD	47.10m AHD	Nil
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The discretionary decision is supported for the following reasons;

- The Structure Plan outlines that buildings in the location of the proposed development should be limited to four storeys. The building proposed is clearly a five storey proposal but the addition storey is setback from the main building wall to all elevations. This not only reduces the bulk and scale of the building but takes advantage of space that would otherwise form a pitched roof. Given the setback of the additional floor, the proposal is not considered to be of greater bulk, scale or impact compared to a four storey proposal with a pitched roof or loft level;
- The portions of wall height that exist above the Structure Plan requirement are setback from the edge of the main building face, reducing the overall bulk of the proposal;
- The additional height, in particular the additional external wall height is not considered to unreasonably restrict access to direct light or views of significance to surrounding properties having regard to the separation distance between the development and the nearest residential property; and,
- The proposed modifications are considered minor in nature having regard to previous approvals and represent only a marginal departure from the recommendations of the Structure Plan.

### **Visual Privacy**

The Structure Plan outlines that the desired density of the subject site ought to be equivalent to R80; despite the fact that no density coding is directly applicable under the scheme maps of LPS4. On the basis of the R80 code, the proposal is considered to meet all of the deemed-to-comply requirements of the R-Codes in the following ways;

- All major openings and raised outdoor living areas to boundaries of adjoining residential properties (including the internal separation between the proposed development and other residential developments on the subject site) meet the cone-of-vision setback requirement of the R-Codes; or
- The elevations where a lesser setback is sought address non-residential premises at No. 19-21 McCabe Street) and in those instance, the visual privacy provisions do not apply.

### **Sustainable Building Design**

The proposed development is required to achieve a rating of not less than 4 Star Green Star (or equivalent) using the relevant Green Building Council of Australia rating tool pursuant to Local Planning Policy 2.13 – Sustainable Building Design ('LPP2.13').

The applicant has proposed the use of an alternative energy efficiency rating system in lieu of the 4 Star Green Star requirements. Use of this rating tool is

consistent with previous resolutions of the City and Joint Development Assessment panels relating to recent applications.

A condition of approval requiring achievement of an equivalent rating under the alternative tool is recommended.

### **Vehicle traffic**

As part of DA0087/10 previously approved by the City, the applicant provided to the City a Transport Statement prepared by Shawmac, dated July 2009. The statement stated that the transport capabilities of the site and surrounding connecting roads were sufficient. This report was based on a similar yield of dwellings to what is currently proposed and remains relevant.

Given the relevancy of this report and the general compliance with the adopted Structure Plan, the submission received relating to vehicle traffic are not considered to be of planning concern.

### **Conclusion:**

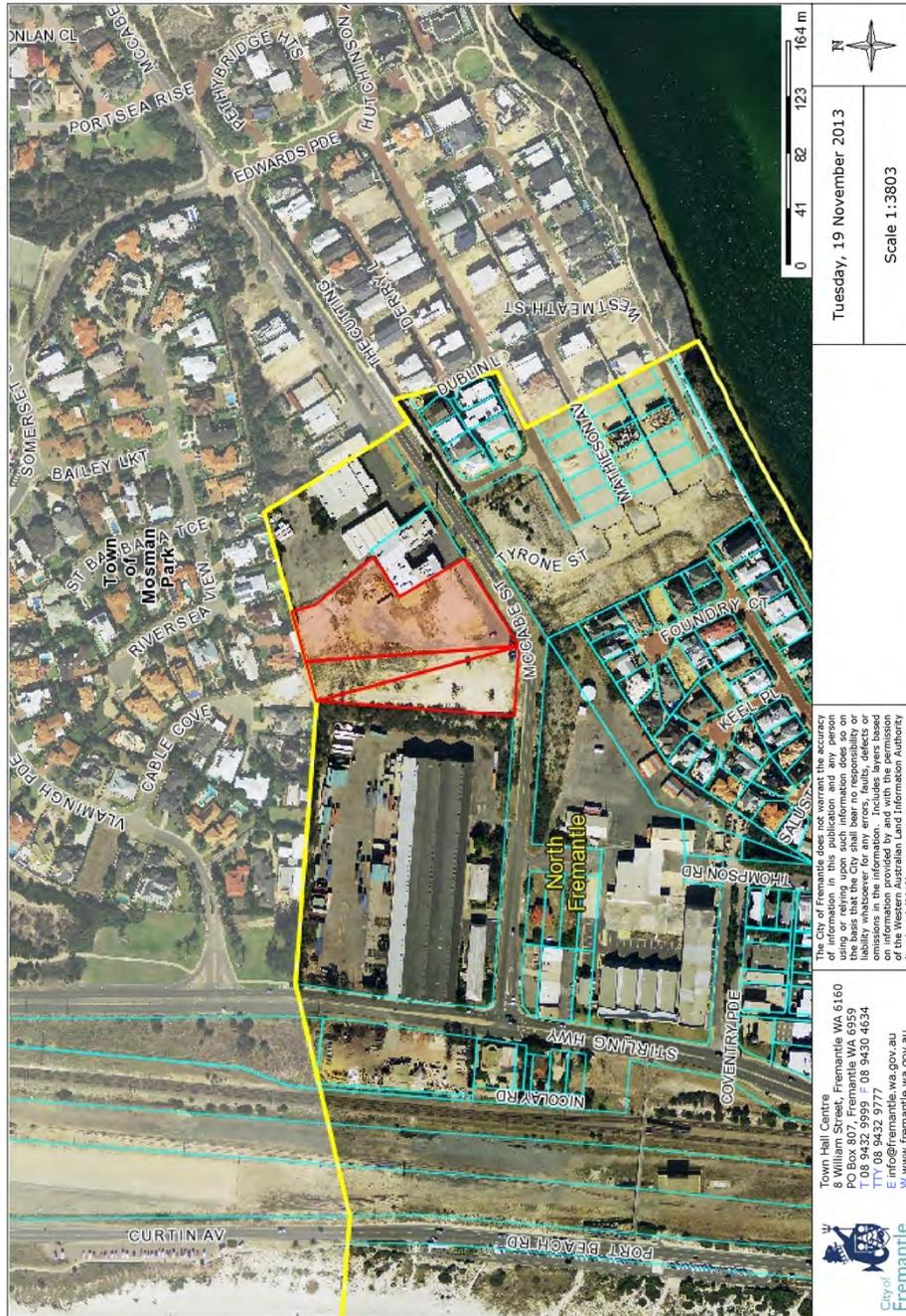
The application seeks planning approval for the development of a five storey multiple dwelling development (20 residential dwellings) at the subject site. The proposal is summarised as follows;

- The subject site is zoned 'Development Area (DA18) pursuant to LPS4 and is subject to the adopted Structure Plan;
- In accordance with clause 6.2.3.2 of LPS4, the proposal is required to be generally in accordance with the Structure Plan;
- The proposal is generally consistent with the Structure Plan in terms of building position, scale, density, height and vehicle traffic; and,
- The proposal is required to meet the sustainable building requirements of LPP2.13 and a condition of approval is recommended to this affect;

The application was presented to the DAC which determined that there was insufficient information required to comment on the proposal. The application was presented to the PSC who resolved to recommend approval of the application, subject to a condition requiring further design details.

The application is considered to meet the relevant planning framework and is therefore recommended for conditional approval.

# Attachment 1: Location Plan



**Attachment 2: Development Plans dated 15 October 2013**





**CITY OF FREMANTLE**  
 THESE PLANS FORM PART OF

15 OCT 2013

APP 60006/13

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 PLEASE ATTACH A3 PLANS FOR SCANNING

**PROPOSED MULTI RESIDENTIAL DEVELOPMENT**  
 LOTS 18, 315, 328 McCABE ST, NORTH FREMANTLE

**DEVELOPMENT APPLICATION**  
 EAST APT  
 LEVEL 1 PLAN

**ZUIDVELD MARCHANT HUR**  
 ARCHITECTURE PLANNING INTERIOR DESIGN  
 100/102 WESTERN AVENUE, FREMANTLE WA 6155  
 TEL: (08) 9442 1111 FAX: (08) 9442 1112  
 WWW.ZMHUR.COM.au

SCALE: 1:100  
 DATE: 10/08/12  
 DRAWING NO: 1000-04-02201 Rev 0



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15 OCT 2013

80006/13

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**PROPOSED MULTI RESIDENTIAL DEVELOPMENT**  
LOTS 18, 315, 326 McCABE ST, NORTH FREMANTLE

**DEVELOPMENT APPLICATION**

EAST APT  
LEVEL 2 PLAN



**ZUIDVELD MARCHANT HUR**

ARCHITECTURE PLANNING INTERIOR DESIGN

100/101 Stirling Street, Perth WA 6000  
Tel: (08) 9442 1111 Fax: (08) 9442 1112  
www.zuidveldmarchant.com.au

SCALE 1:200  
DATE 15/10/13  
DWG NO. 100-15A-DWG-02 Rev 4





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15 OCT 2013

DAP 80006 / 13

PROPOSED MULTI RESIDENTIAL DEVELOPMENT  
LOTS 10, 315, 326 McCABE ST, NORTH FREMANTLE

DEVELOPMENT APPLICATION

EAST APT  
LEVEL 4 PLAN



**ZUIDVELD MARCHANT HUR**

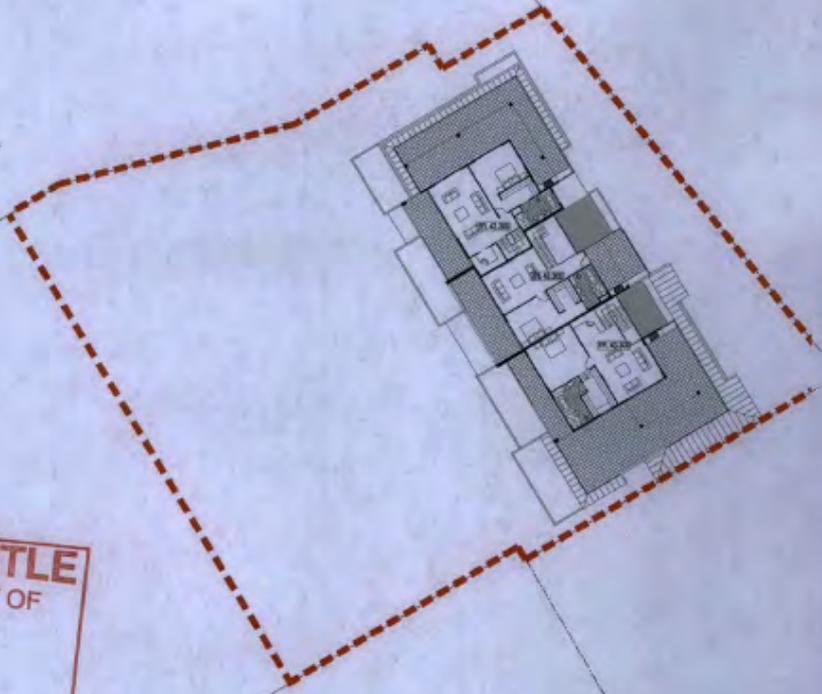
ARCHITECTURAL PLANNING & DESIGN (Pty) Ltd

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15 OCT 2013  
D.A.P 80006/13



PROPOSED MULTI RESIDENTIAL DEVELOPMENT  
LOTS 18, 315, 328 McCABE ST, NORTH FREMANTLE



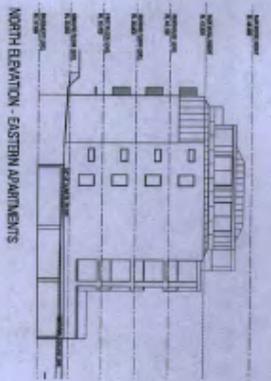
**ZUIDVELD MARCHANT HUR**

ARCHITECTURE PLANNING INTERIOR DESIGN

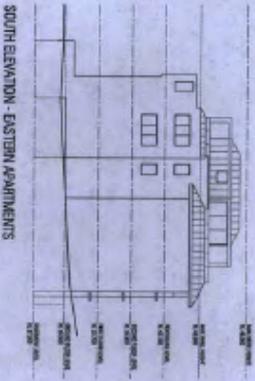
DEVELOPMENT APPLICATION  
EAST APT  
LEVEL 5 PLAN

SCALE 1:200  
DATE 12/09/12  
DWG NO. 100-04-02-05 Rev A

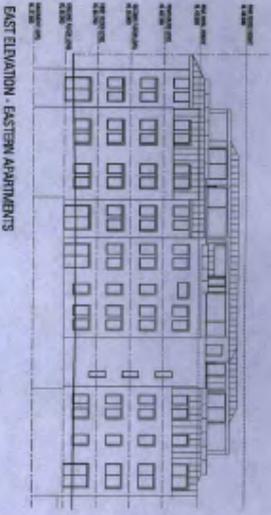
**FILE COPY**  
PLEASE ATTACH A3 PLANS FOR SCANNING



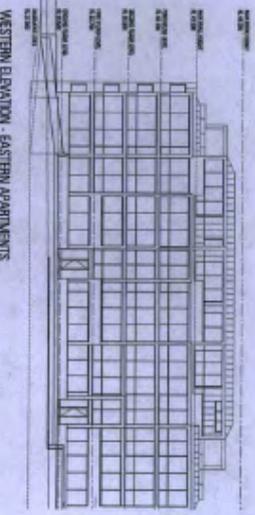
NORTH ELEVATION - EASTERN APARTMENTS



SOUTH ELEVATION - EASTERN APARTMENTS



EAST ELEVATION - EASTERN APARTMENTS



WESTERN ELEVATION - EASTERN APARTMENTS

**CITY OF FREMANTLE**  
THESE PLANS FORM PART OF  
15 OCT 2013  
D.A. 80006/13

PROPOSED MULTI RESIDENTIAL DEVELOPMENT  
LOTS 18, 315, 326 McCABE ST, NORTH FREMANTLE

**DEVELOPMENT APPLICATION**  
EAST APT  
ELEVATIONS

**ZUIDVELD MARCHANT HUR**  
ARCHITECTURE PLANNING INTERIOR DESIGN

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SCALE: 1:200  
DATE: 15/08/11  
DWG NO: 18R-01R-001-01 Rev A