

PORT COOGEE

PROPOSED 6 STOREY + ROOFTOP TERRACE
RESIDENTIAL PROJECT FOR
OPEN CORPORATION



Drawing List	
NUMBER	DRAWING NAME
DA000	TITLE PAGE + LOCALITY MAP
DA001	SITE ANALYSIS + CONTEXT PLAN
DA002	Site plan
DA100	Basement Floor Plan
DA101	Level 1 Plan
DA102	Typical Floor Plan Levels 2-5
DA103	Roof Plan
DA200	West Elevation - Brunswick Street
DA201	South Elevation
DA202	East Elevation - Comet Lane
DA203	North Elevation
DA300	A-A Building Section
DA301	B-B Building Section
DA302	C-C Building Section
DA900	Perspective - Brunswick St
DA901	Perspective - Brunswick St Context
DA902	Perspective - Comet Lane
DA903	Comet Lane & Rooftop Perspectives
DA907	Shadow Diagram - 9am/12pm/3pm



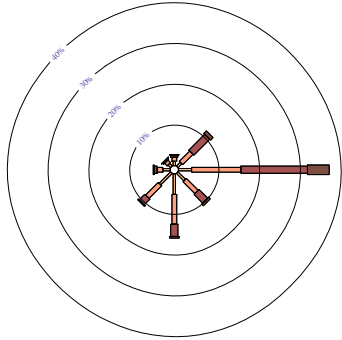
PRELIMINARY



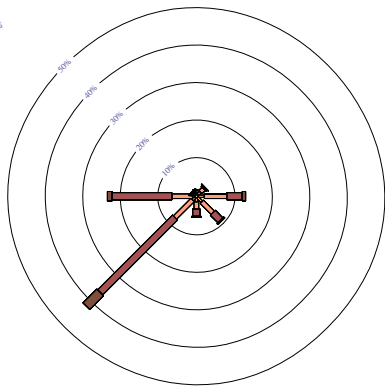
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ABN 25 082 956 929

Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	TITLE PAGE + LOCALITY MAP	DA000	A
A	DRAFT DA SUBMISSION	30/09/2014								
							Scale	Drawing Size		
							@A1	A1		
							Project No.	Date	Drawn By	
							14052	09/14	DD & RW	
							CAD Reference			
							C:\Users\m2\Documents\Documents\140527_DA_PortCoogee_Dimension.rvt			
Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©										

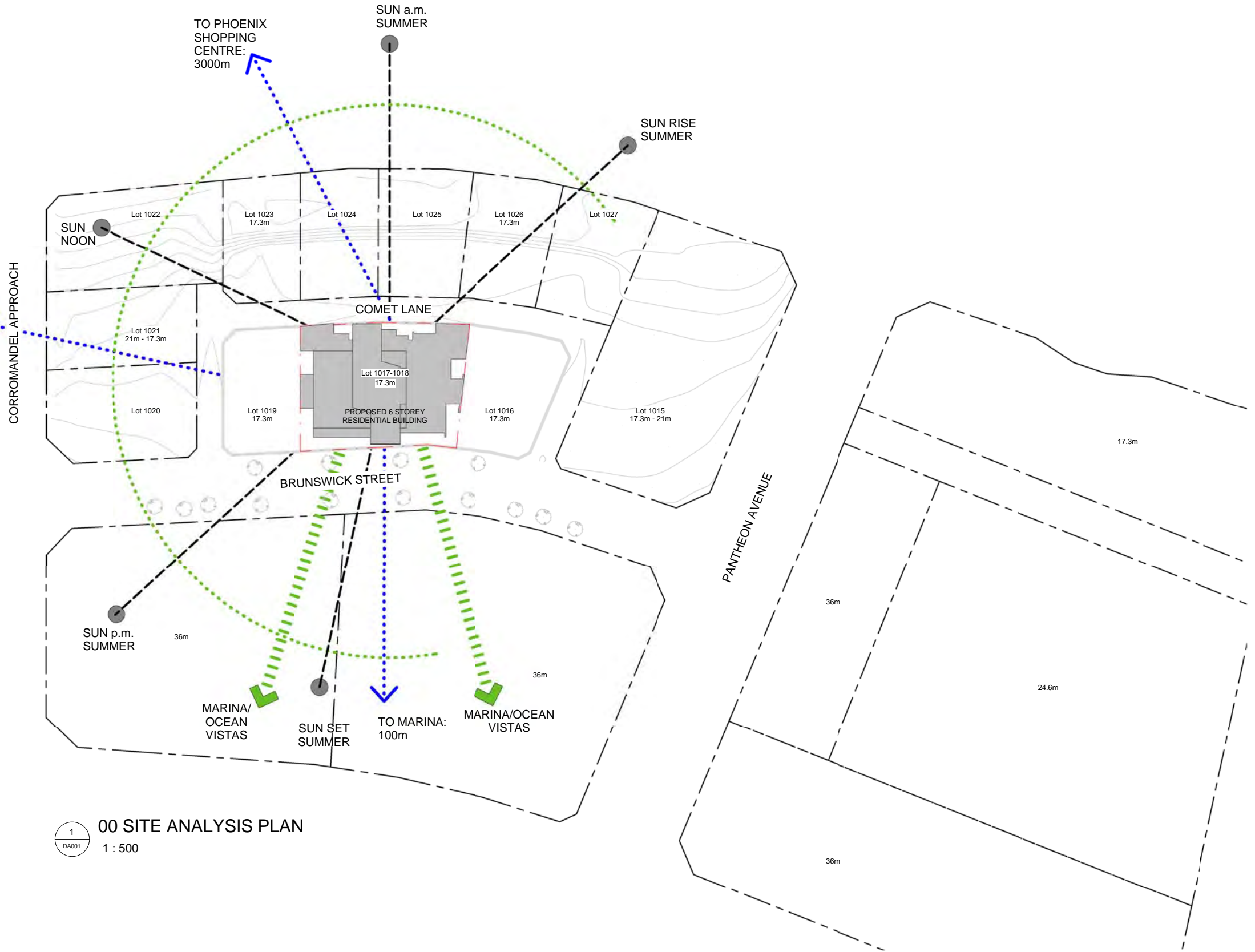
WIND FREQUENCY ANALYSIS (in km/h)
PERTH AIRPORT STATION NUMBER 009021
Latitude: -31.93 ° Longitude: 115.98 °
9 am Jan
1855 Total Observations (1944 to 2004)
Calm 5%



WIND FREQUENCY ANALYSIS (in km/h)
PERTH AIRPORT STATION NUMBER 009021
Latitude: -31.93 ° Longitude: 115.98 °
3 pm Jan
1855 Total Observations (1944 to 2004)
Calm 2%



2
DA001
WIND ROSE_dwg
1 : 40



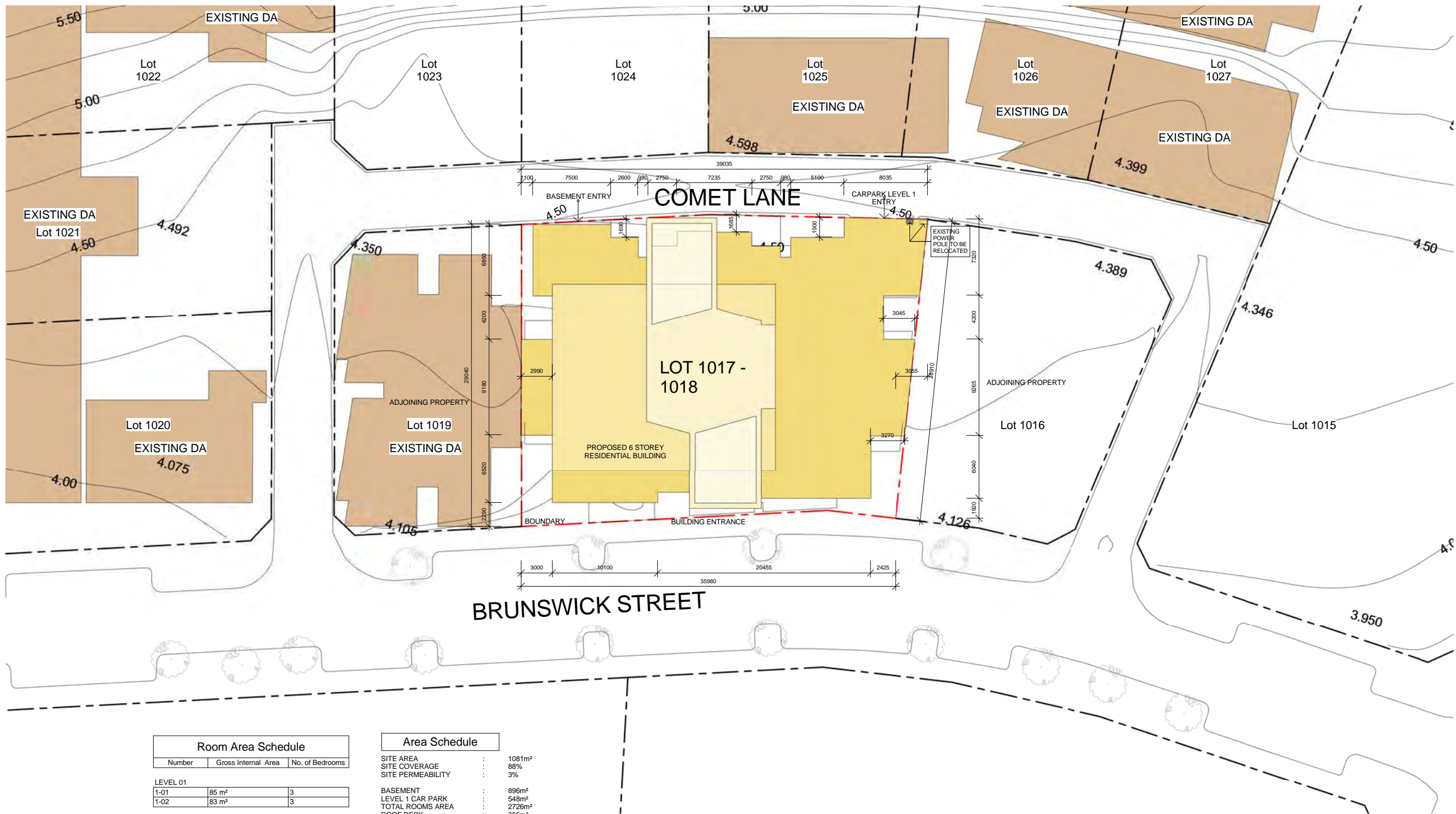
1
DA001
00 SITE ANALYSIS PLAN
1 : 500

PRELIMINARY

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1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	SITE ANALYSIS + CONTEXT PLAN	DA001	A
A	DRAFT DA SUBMISSION	30/09/2014							Scale As indicated@A1	Drawing Size A1
									Project No. 14052	Drawn By DD & RW
									Date 09/14	
									CAD Reference c:\Users\DDemetriou\Documents\140527_DA_PortCoogee_DDemetriou.rvt	

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Room Area Schedule		
Number	Gross Internal Area	No. of Bedrooms
LEVEL 01		
1-01	85 m ²	3
1-02	83 m ²	3
LEVEL 02		
2-01	65 m ²	2
2-02	65 m ²	2
2-03	79 m ²	2
2-04	79 m ²	2
2-05	78 m ²	2
2-06	85 m ²	2
2-07	56 m ²	1
2-08	59 m ²	1
2-09	75 m ²	2
TOTAL ROOMS 38 Total Room Area: 2726m ²		
Level 2 Typical to Level 3, 4 & 5		

Area Schedule	
SITE AREA	1081m ²
SITE COVERAGE	88%
SITE PERMEABILITY	3%
BASEMENT	896m ²
LEVEL 1 CAR PARK	549m ²
TOTAL ROOMS AREA	2726m ²
ROOF DECK	385m ²

PRELIMINARY



Parking Schedule		
Level	Description	Total Count
BASEMENT	5400 x 2400	29
Street Level	Parallel 2100x6600	3
LEVEL 01	5400 x 2400	19
51		

Bicycle Parking Schedule		
Type	Comments	Bikes provided
DOUBLE SIDED BICYCLE RACK		2
		Street level

STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m²	20	STORAGE
BASEMENT	5 m²	1	STORAGE
LEVEL 01	4 m²	3	STORAGE
LEVEL 01	5 m²	2	STORAGE
LEVEL 02	4 m²	2	STORAGE
LEVEL 02	6 m²	1	STORAGE
LEVEL 03	4 m²	2	STORAGE
LEVEL 03	6 m²	1	STORAGE
LEVEL 04	4 m²	2	STORAGE
LEVEL 04	6 m²	1	STORAGE
LEVEL 05	4 m²	2	STORAGE
LEVEL 05	6 m²	1	STORAGE
Grand total: 38			

LEGEND	
ST	STORAGE
COMMS	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER CHECK METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
SRD	SECTIONAL ROLLER DOOR
SD	STRIP DRAIN
	CONDENSING UNIT
	BICYCLE RACK (SINGLE SIDED PARKING RAIL)
	RISER

REVISIONS	
1	OBSTRUCTIONS CLEARED FROMSIGHTLINES
2	CARPAKING LAYOUT ADJUSTED
3	GRIDLINE A RELOCATED TO WEST

DEVELOPMENT APPLICATION
ONLY NOT FOR CONSTRUCTION



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Issue	Description	Date
1	PRE-LODGEEMENT MEETING	12/08/2014
2	REVISED LAYOUT/ROOF TOP	28/08/2014
A	DRAFT DA SUBMISSION	30/08/2014
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014

Issue	Description	Date
1	PRE-LODGEEMENT MEETING	12/08/2014
2	REVISED LAYOUT/ROOF TOP	28/08/2014
A	DRAFT DA SUBMISSION	30/08/2014
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014

Client
Open Corporation

Project
PORT COOGEE
LOT 1017-1018 BRUNSWICK STREET
PORT COOGEE
WESTERN AUSTRALIA

Title
BASEMENT FLOOR PLAN

Drawing No.
DA100

Issue
B

Scale
1: 100@A1

1:200@A3

Drawing Size
A1

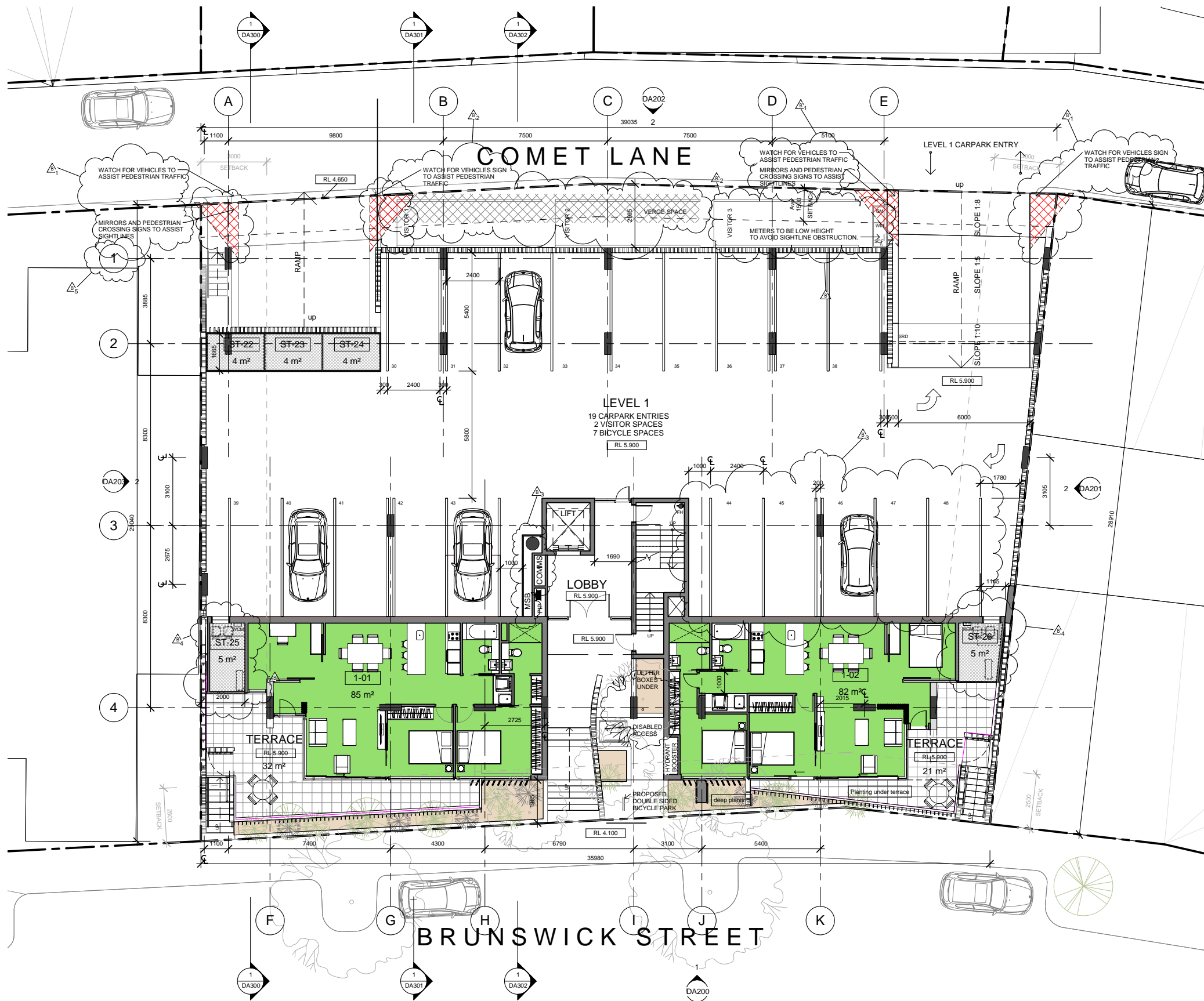
Project No.
14052

Date
11/14

Drawn By
DD & RW

CAD Reference
C:\Users\j\Documents\14107_DA_PortCoogee_review_traffic_and_waste_management\14107_DA_PortCoogee_review_traffic_and_waste_management.dwg

Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©



Parking Schedule		
Level	Description	Total Count

BASEMENT	5400 x 2400	29
Street Level	Parallel 2100x6600	3
LEVEL 01	5400 x 2400	19

Bicycle Parking Schedule		
Type	Comments	Bikes provided

DOUBLE SIDED BICYCLE RACK	2	Street level
---------------------------	---	--------------

STORAGE			
Level	Area	Count	Comments

BASEMENT	4 m²	20	STORAGE
BASEMENT	5 m²	1	STORAGE
LEVEL 01	4 m²	3	STORAGE
LEVEL 01	5 m²	2	STORAGE
LEVEL 02	4 m²	2	STORAGE
LEVEL 02	6 m²	1	STORAGE
LEVEL 03	4 m²	2	STORAGE
LEVEL 03	6 m²	1	STORAGE
LEVEL 04	4 m²	2	STORAGE
LEVEL 04	6 m²	1	STORAGE
LEVEL 05	4 m²	2	STORAGE
LEVEL 05	6 m²	1	STORAGE

Grand total: 38

ROOM SCHEDULE			
Number	Level	Area	Occupancy

1-01	LEVEL 01	85 m²	3
1-02	LEVEL 01	82 m²	3
2-01	LEVEL 02	65 m²	2
2-02	LEVEL 02	65 m²	2
2-03	LEVEL 02	79 m²	2
2-04	LEVEL 02	79 m²	2
2-05	LEVEL 02	78 m²	2
2-06	LEVEL 02	85 m²	2
2-07	LEVEL 02	56 m²	1
2-08	LEVEL 02	59 m²	1
2-09	LEVEL 02	75 m²	2

Level 2 Typical to Level 3,4 & 5

LEGEND	
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ST	-	STORAGE
COMMS-	-	COMMUNICATIONS BOX
NBN	-	MAIN COMMUNICATIONS BOX
WM	-	WATER METERS
WCM	-	WATER CHECK METERS
FHR	-	FIRE HOSE RHEEL
MP	-	METER PANEL
MSB	-	MAIN SWITCH BOARD
SRD	-	SECTIONAL ROLLER DOOR
SD	-	STRIP DRAIN
		CONDENSING UNIT
		BICYCLE RACK (SINGLE SIDED PARKING RAIL)
		RISER

REVISIONS	
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- 1 - OBSTRUCTIONS CLEARED FROM SIGHTLINES
- 2 - PEDESTRIAN & VEHICULAR AWARENESS
- 3 - CARPARKING LAYOUT ADJUSTED
- 4 - STORAGE SPACE INCREASED TO INCLUDE INDIVIDUAL BIN STORAGE SPACE
- 5 - GRIDLINE A RELOCATED TO WEST

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2	REVISED LAYOUT/ROOF TOP	28/08/2014	2	REVISED LAYOUT/ROOF TOP	28/08/2014				Scale	Drawing Size
A	DRAFT DA SUBMISSION	30/08/2014	A	DRAFT DA SUBMISSION	30/08/2014				1:100@A1	A1
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014	B	TRAFFIC & WASTE MANAGEMENT	14/11/2014				Project No.	Drawn By
									14052	DD & RW
									Date	
									11/14	
									CAD Reference	
									C:\Users\j\Documents\14107_DA_PortCoogee_review_traffic_and_waste_management\14107_DA_PortCoogee_review_traffic_and_waste_management.rvt	

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Level	Area	Count	Comments
BASEMENT	4 m²	20	STORAGE
BASEMENT	5 m²	1	STORAGE
LEVEL 01	4 m²	3	STORAGE
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LEVEL 02	4 m²	2	STORAGE
LEVEL 02	6 m²	1	STORAGE
LEVEL 03	4 m²	2	STORAGE
LEVEL 03	6 m²	1	STORAGE
LEVEL 04	4 m²	2	STORAGE
LEVEL 04	6 m²	1	STORAGE
LEVEL 05	4 m²	2	STORAGE
LEVEL 05	6 m²	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m²	3
1-02	LEVEL 01	82 m²	3
2-01	LEVEL 02	65 m²	2
2-02	LEVEL 02	65 m²	2
2-03	LEVEL 02	79 m²	2
2-04	LEVEL 02	79 m²	2
2-05	LEVEL 02	78 m²	2
2-06	LEVEL 02	85 m²	2
2-07	LEVEL 02	56 m²	1
2-08	LEVEL 02	59 m²	1
2-09	LEVEL 02	75 m²	2

LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
SRD	SECTIONAL ROLLER DOOR
SD	STRIP DRAIN
	CONDENSING UNIT
	BICYCLE RACK (SINGLE SIDED PARKING RAIL)
	RISER

REVISIONS	
1	COLUMN MOVED
2	GRIDLINE A RELOCATED TO WEST

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A	DRAFT DA SUBMISSION	30/08/2014
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Client
Open Corporation

Project
PORT COOGEE
LOT 1017-1018 BRUNSWICK STREET
PORT COOGEE
WESTERN AUSTRALIA

Title
LEVEL 2 PLAN

Drawing No.
DA102

Issue
B

Scale
1: 100@A1

1:200@A3

Drawing Size
A1

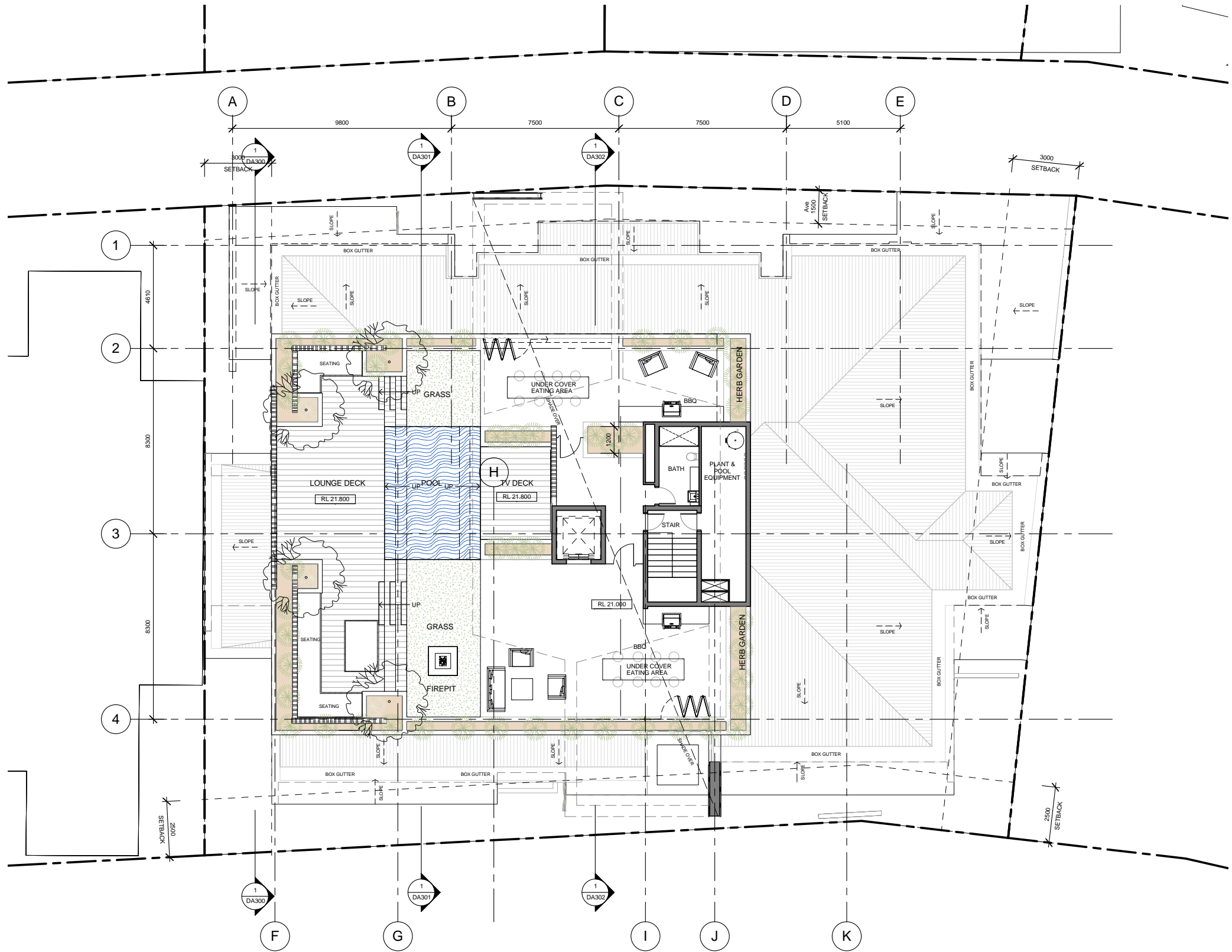
Project No.
14052

Date
11/14

Drawn By
DD & RW

CAD Reference
C:\Users\j\Documents\141017_DA_PortCoogee_review_traffic_and_waste_management\141017_DA_PortCoogee_review_traffic_and_waste_management.dwg

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1	PRE-LODGEEMENT MEETING	12/08/2014	1	PRE-LODGEEMENT MEETING	12/08/2014	Open Corporation	PORT COOGEE	ROOF PLAN	DA106	A
2	REVISED LAYOUT/ROOF TOP	28/08/2014	2	REVISED LAYOUT/ROOF TOP	28/08/2014		LOT 1017-1018 BRUNSWICK STREET		1:100@A1	1:200@A3
A	DRAFT DA SUBMISSION	30/09/2014	A	DRAFT DA SUBMISSION	30/09/2014		PORT COOGEE		Project No.	Date
							WESTERN AUSTRALIA		14052	09/14
									CAD Reference	Drawn By
									c:\Users\j\Documents\140527_DA_PortCoogee_Dimensions.rvt	DD & RW

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STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m²	20	STORAGE
BASEMENT	5 m²	1	STORAGE
LEVEL 01	4 m²	3	STORAGE
LEVEL 01	5 m²	2	STORAGE
LEVEL 02	4 m²	2	STORAGE
LEVEL 02	6 m²	1	STORAGE
LEVEL 03	4 m²	2	STORAGE
LEVEL 03	6 m²	1	STORAGE
LEVEL 04	4 m²	2	STORAGE
LEVEL 04	6 m²	1	STORAGE
LEVEL 05	4 m²	2	STORAGE
LEVEL 05	6 m²	1	STORAGE
Grand total: 38			

ROOM SCHEDULE			
Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m²	3
1-02	LEVEL 01	82 m²	3
2-01	LEVEL 02	65 m²	2
2-02	LEVEL 02	65 m²	2
2-03	LEVEL 02	79 m²	2
2-04	LEVEL 02	79 m²	2
2-05	LEVEL 02	78 m²	2
2-06	LEVEL 02	85 m²	2
2-07	LEVEL 02	56 m²	1
2-08	LEVEL 02	59 m²	1
2-09	LEVEL 02	75 m²	2

LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
FHR	FIRE HOSE RHEEL
MP	METER PANEL
MSB	MAIN SWITCH BOARD
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SD	STRIP DRAIN
	CONDENSING UNIT
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	RISER

REVISIONS	
1	COLUMN MOVED
2	GRIDLINE A RELOCATED TO WEST

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Client
Open Corporation

Project
PORT COOGEE
LOT 1017-1018 BRUNSWICK STREET
PORT COOGEE
WESTERN AUSTRALIA

Title
LEVEL 4 PLAN

Drawing No.
DA104

Issue
B

Scale
1:100@A1

1:200@A3

Drawing Size
A1

Project No.
14052

Date
11/14

Drawn By
DD & RW

CAD Reference
C:\Users\j\Documents\14052_PortCoogee_review_Traffic & Waste Management_Dimension.rvt

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STORAGE			
Level	Area	Count	Comments
BASEMENT	4 m²	20	STORAGE
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LEVEL 03	6 m²	1	STORAGE
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Number	Level	Area	Occupancy
1-01	LEVEL 01	85 m²	3
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LEVEL 2 TYPICAL TO LEVELS 3, 4, 5

LEGEND	
ST	STORAGE
COMMS	COMMUNICATIONS BOX
NBN	MAIN COMMUNICATIONS BOX
WM	WATER METERS
WCM	WATER CHECK METERS
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	BICYCLE RACK
	(SINGLE SIDED PARKING RAIL)
	RISER

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1	COLUMN MOVED
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Client
Open Corporation

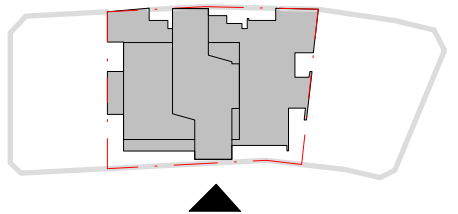
Project
PORT COOGEE
LOT 1017-1018 BRUNSWICK STREET
PORT COOGEE
WESTERN AUSTRALIA

Title
LEVEL 3 PLAN

Drawing No. DA103	Issue B
Scale 1:100@A1	Drawing Size A1
Project No. 14052	Drawn By DD & RW
Date 11/14	

CAD Reference
C:\Users\j\Documents\14052_PortCoogee_review_Traffic & Waste Management\14052_PortCoogee_review_Traffic & Waste Management.dwg

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FINISHES LEGEND

CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW/ GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

PRELIMINARY



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A	DRAFT DA SUBMISSION	30/09/2014

Issue	Description	Date	Client
			Open Corporation

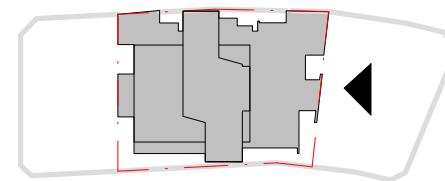
Project
PORT COOGEE
LOT 1017-1018 BRUNSWICK STREET
PORT COOGEE
WESTERN AUSTRALIA

Title
WEST ELEVATION - BRUNSWICK
STREET

Drawing No. DA200	Issue A
Scale 1:100 @A1 1:200 @ A3	Drawing Size A1
Project No. 14052	Drawn By DD & RW
Date 09/14	

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CAD Reference
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FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

DEVELOPMENT APPLICATION
ONLY NOT FOR CONSTRUCTION



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGE/MENT MEETING	12/08/2014				Open Corporation	PORT COOGEE	SOUTH ELEVATION	DA201	B
A	DRAFT DA SUBMISSION	30/09/2014					LOT 1017-1018 BRUNSWICK STREET		Scale	Drawing Size
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014								

Scale
As indicated@A1:200@A3

Project No.	Date	Drawn By
14052	11/14	DD & RW

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CAD Reference
C:\Users\DDemetriou\Documents\141107_DA_PortCoogee_review_Traffic & Waste Management_DDemetriou.rvt



- REVISIONS
- 1 - PEDESTRIAN/VEHICULAR SIGHTLINES IMPROVED

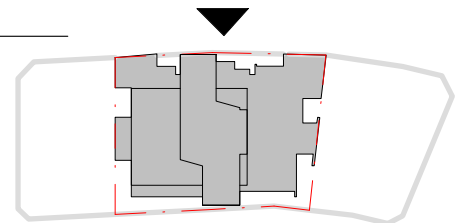
2 - COMET LANE LANDSCAPING REMOVED TO IMPROVE SIGHTLINES

2

DA202

Elevation 2 - East (Comet Lane)

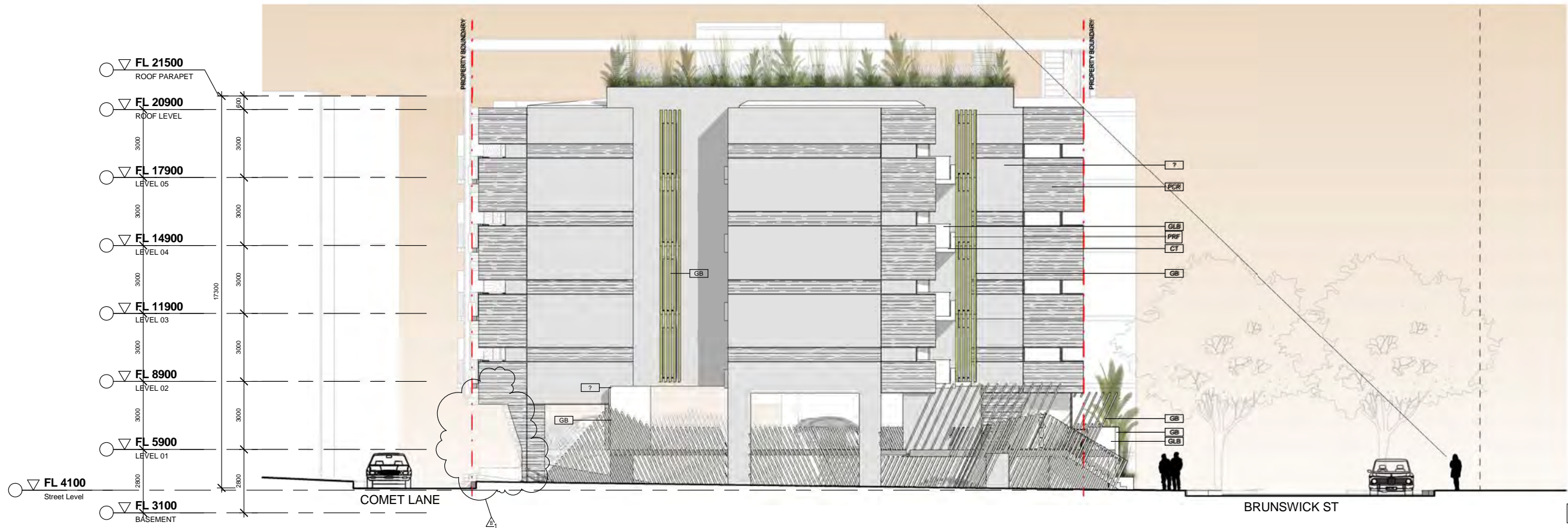
1 : 100



FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW/ GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

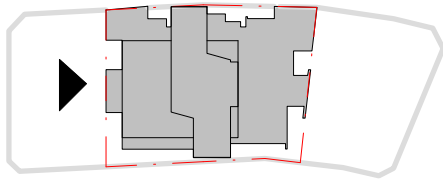
DEVELOPMENT APPLICATION
ONLY NOT FOR CONSTRUCTION

Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	EAST ELEVATION - COMENT LANE	DA202	B
A	DRAFT DA SUBMISSION	30/09/2014							Scale As indicated@A1:200@A3	Drawing Size A1
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014							Project No. 14052	Drawn By DD & RW
								Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©		
								CAD Reference C:\Users\DDemetriou\Documents\141017_DA_PortCoogee_review_traffic & Waste Management_DDemetriou.rvt		



REVISIONS
1 - PEDESTRIAN/VEHICULAR SIGHTLINES IMPROVED

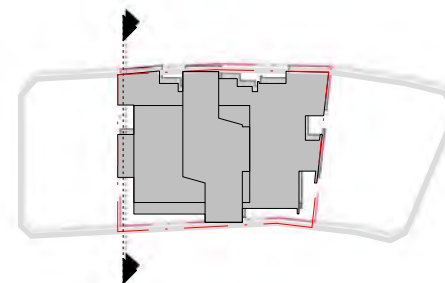
2 Elevation 4 - North
DA203 1 : 100



FINISHES LEGEND	
CT	CONCRETE TILES
FG	FRAMELESS GLAZING TO BALUSTRADES
FR	FEATURE ROOF
GB	GALVANISED BEAM - 235mm x 64mm SECTION WITH LIGHT TIMBER-LOOK POWDERCOAT FINISH
GF	GALVANISED FINISH
GW	GREEN WALL
PCA	POWDERCOAT ALUMINIUM FRAMING TO DOOR/WINDOW GLAZING - BLACK
PCP	PRECAST CONCRETE - PAINTED GUN METAL GREY
PCR	PRECAST CONCRETE REBATE FEATURE - TEXTURED AND PAINTED GUN METAL GREY
PRF	PAINTED RENDER FINISH - SURFMIST
TD	TIMBER DECKING - TASSY OAK/YELLOW GUM
TS	TIMBER SCREENING - 90mm x 35mm TIMBER BATTENS w/10mm SPACING

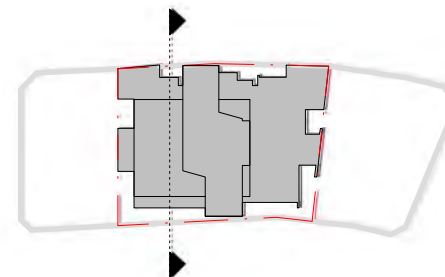
DEVELOPMENT APPLICATION
ONLY NOT FOR CONSTRUCTION

Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	NORTH ELEVATION	DA203	B
A	DRAFT DA SUBMISSION	30/09/2014							Scale As indicated@A1:200@A3	Drawing Size A1
B	TRAFFIC & WASTE MANAGEMENT	14/11/2014							Project No. 14052	Drawn By DD & RW
								Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©	CAD Reference c:\Users\DDemetro\Documents\141052_DA_PortCoogee_review_traffic_Waste Management_DDemetro.rvt	



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE			A
A	DRAFT DA SUBMISSION	30/09/2014					LOT 1017-1018 BRUNSWICK STREET	A-A BUILDING SECTION	DA300	
							PORT COOGEE			
							WESTERN AUSTRALIA			
									Drawing Size	
									1:100 @ A1	1:200 @ A3
									A1	
									Drawn By	
									DD & RW	
									CAD Reference	
									C:\Users\m0demistrou\Documents\140527_Drafting\PortCoogee_Dimensions.rvt	
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Issue	Description	Date
1	PRE-LODGEEMENT MEETING	12/08/2014
A	DRAFT DA SUBMISSION	30/09/2014

Issue	Description	Date

Client
Open Corporation

Project
PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA

Title
B-B BUILDING SECTION

Drawing No.	Issue
DA301	A

Scale	Drawing Size
1:100 @ A1 1:200 @ A3	A1

Project No.	Date	Drawn By
14052	09/14	DD & RW

CAD Reference
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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
A	DRAFT DA SUBMISSION	30/09/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	C-C BUILDING SECTION	DA302	A
P										
									Scale	Drawing Size
									1:100 @ A1	A1
									1:200 @ A3	A1
									Project No.	Drawn By
									14052	DD & RW
									Date	
									09/14	
									CAD Reference	
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©	
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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE	PERSPECTIVE - BRUNSWICK ST	DA900	A
A	DRAFT DA SUBMISSION	30/09/2014					LOT 1017-1018 BRUNSWICK STREET			
							PORT COOGEE		Scale	Drawing Size
							WESTERN AUSTRALIA		@A1	A1
									Project No.	Drawn By
									14052	DD & RW
									Date	
									09/14	
									CAD Reference	
									c:\Users\j\Documents\Documents\140527_DA_PortCoogee_DD.demonstrat	
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©	



2 Perspective - Sky North West
A901

1 Perspective - Entry level
A901



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
A	DRAFT DA SUBMISSION	30/09/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	PERSPECTIVE - BRUNSWICK ST CONTEXT	DA901	A
									Scale @A1	Drawing Size A1
									Project No. 14052	Drawn By DD & RW
									Date 09/14	
									CAD Reference C:\Users\DDemetriou\Documents\140527_DA_PortCoogee_DDemetriou.rvt	
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©	



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Issue	Description	Date	Issue	Description	Date	Client	Project	Title	Drawing No.	Issue
1	PRE-LODGEEMENT MEETING	12/08/2014				Open Corporation	PORT COOGEE LOT 1017-1018 BRUNSWICK STREET PORT COOGEE WESTERN AUSTRALIA	PERSPECTIVE - COMET LANE	DA902	A
A	DRAFT DA SUBMISSION	30/09/2014								
									Scale	Drawing Size
									@A1	A1
									Project No.	Drawn By
									14052	DD & RW
									Date	
									09/14	
									CAD Reference	
									Dimensioned Drawings to take precedence over scaling. Contractor to verify all dimensions on site before construction. All inconsistencies to be reported to the Architect immediately. This drawing and its contents remain the copyright of WMK Architecture Pty Ltd ©	
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Issue	Description	Date	Issue	Description	Date	Client
A	DRAFT DA SUBMISSION	30/09/2014				Open Corporation

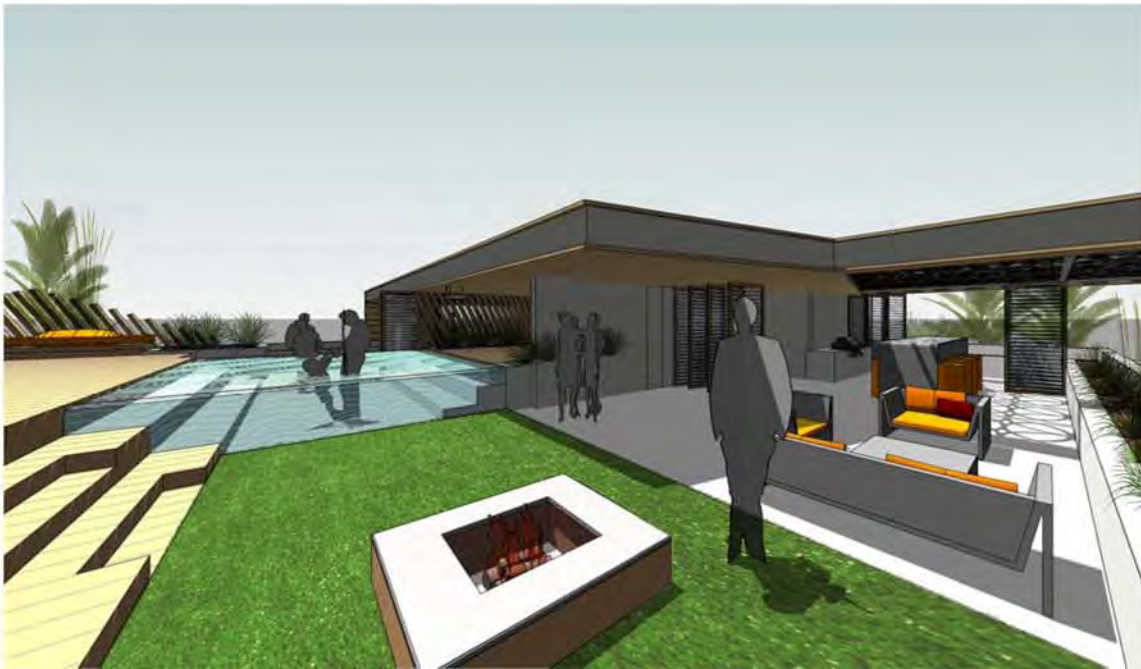
Project
PORT COOGEE
LOT 1017-1018 BRUNSWICK STREET
PORT COOGEE
WESTERN AUSTRALIA

Title
COMET LANE & ROOFTOP
PERSPECTIVES

Drawing No. DA903	Issue A
Scale @A1	Drawing Size A1
Project No. 14052	Drawn By DD & RW
Date 09/14	

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9AM



12PM

12pm OVERSHADOWING 43%
ALL SHADOWS TAKEN ON 22/09



3PM

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ABN 25 082 956 929

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A coastal landscape at sunset. In the foreground, there is a wooden fence made of vertical posts and horizontal rails, with a wire mesh or netting attached to it. Tall, thin grasses are growing in front of the fence. The background shows a calm sea and a hazy horizon under a sky with soft, golden clouds.

port coogee

response contents.

1.0	Traffic Management
	1.01 Bicycle Parking
	1.02 Ramps
	1.03 Sightlines
	1.04 Bin Storage
	1.05 Visitor Parking
2.0	Waste Management Option A
	2.1 Bin Storage
	2.2 Truck Access
	2.3 Basement Height
	2.4 Bin Numbers
	2.5 Verge Space
	2.6 Waste Chutes
3.0	Waste Management Option A
	2.1 Bin Storage
	2.2 Truck Access
	2.3 Basement Height
	2.4 Bin Numbers
	2.5 Verge Space
	2.6 Waste Chutes

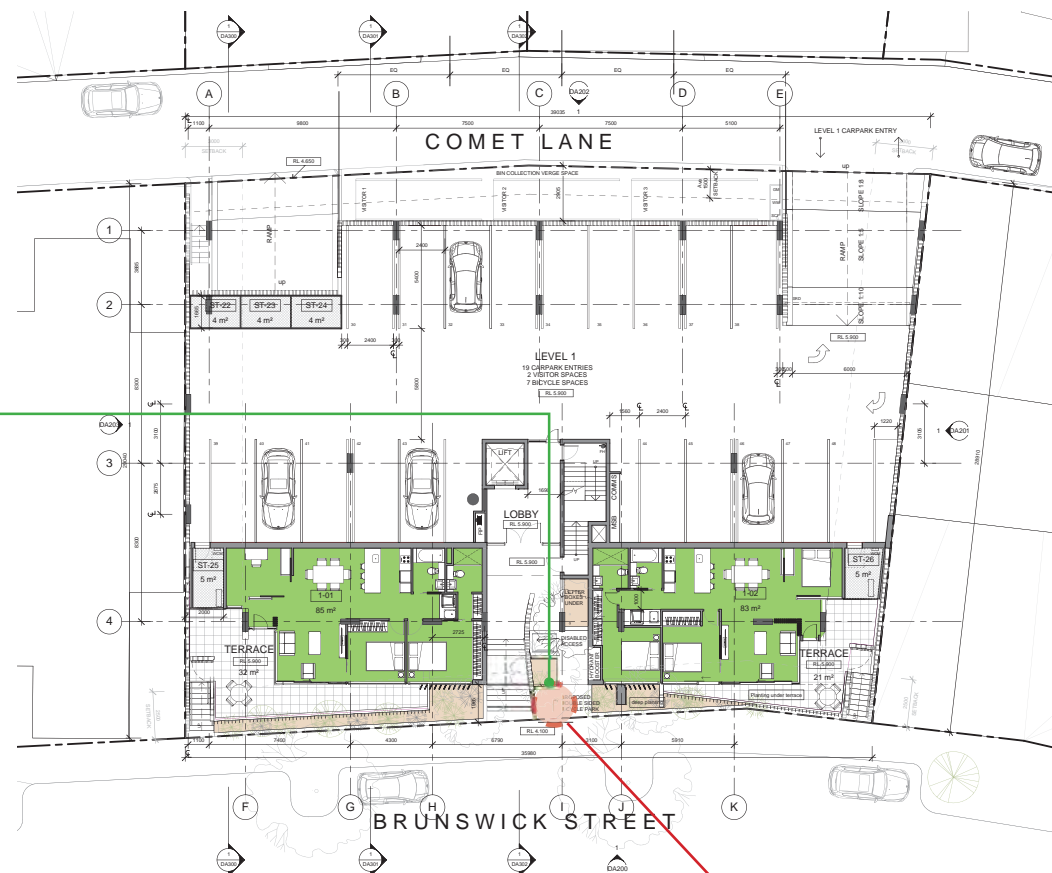
1.0 Traffic Management

- 1.01 Council comments:
The bicycle parking shown in the basement and Level 1 car parks does not comply with AS2890.3 on two counts;
- Design response:
Bicycle parking location and numbers have been revised to suit council requirements and codes. Refer to diagram 1.01
- 1.02 Council comments:
The access ramps exceed the maximum grade specified in AS2890.1 for the first 6 metres of the ramp;
- Design response:
Refer traffic engineers technical notes
- 1.03 Council comments:
The sightlines that will be available to vehicles exiting the car parks onto Comet Lane do not appear to comply with the requirements of AS2890.1.;
- Design response:
Obstructions to sightline removed. Refer to diagram 1.03
- 1.04 Council comments:
The bin storage area on Comet Lane will restrict sight lines when exiting the Level 1 car park and the landscaping adjacent to the Basement car park ramp may restrict sight lines.
- Design response:
Location of bin storage has been revised. No restriction to sightline. Refer to figure diagram 1.04 & 2.04 of for new location.
- 1.05 Council comments:
There is a clause in the parking requirements for Site 2 in the BFC's which allows for 50% of the visitor car parking to be provided on-street. I was wondering if you were aware of this and whether you would consider this as an option to help address some of the above?
- Design response
x1 additional cars added to porposal.
Total visitor spaces required = 2.7
Total Visitor spaces provided = 3.0

1.0 traffic management

non compliant
bicycle parking
removed from
basement and
level 01

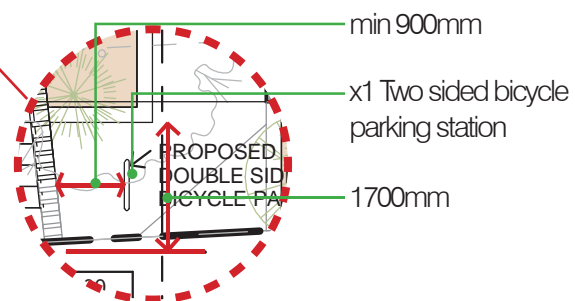
public bicycle
park positioned
to Brunswick St
entrance



1.01 bicycle parking

1.01 Council comments:
The bicycle parking shown in the basement and Level 1 car parks does not comply with AS2890.3 on two counts;

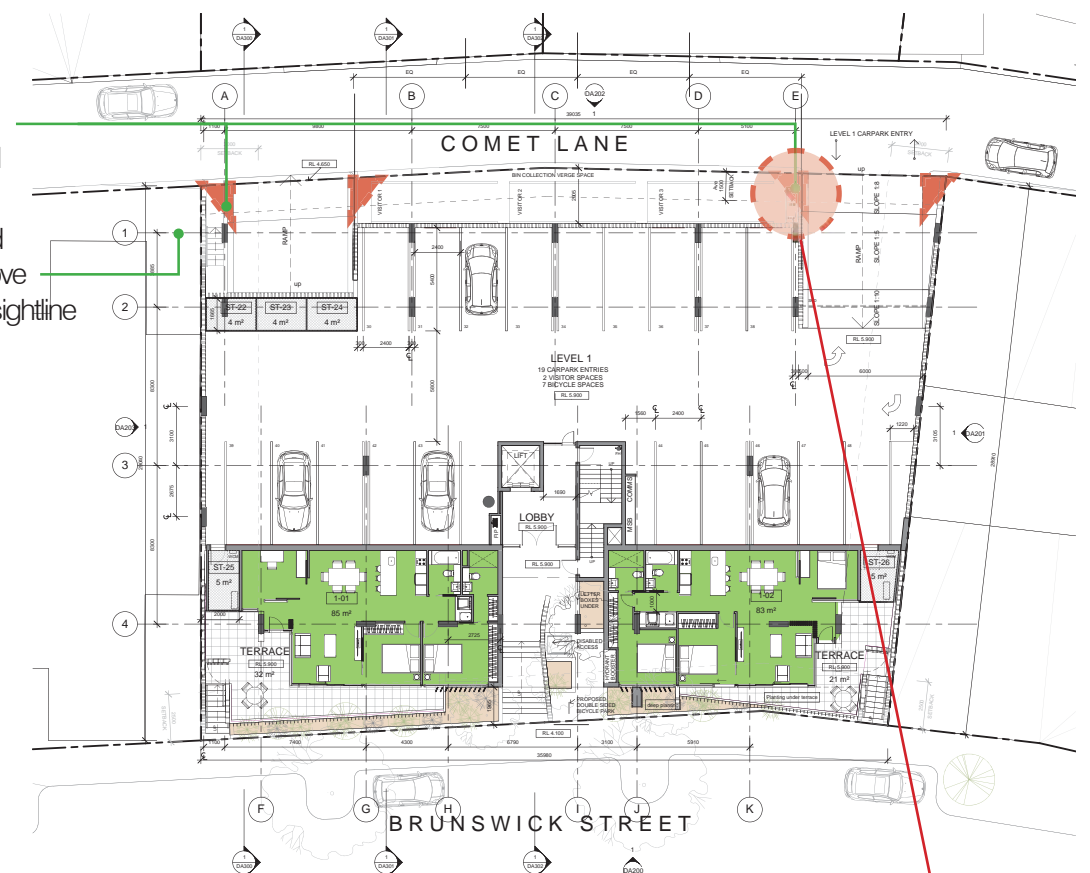
Design response:
Bicycle parking location and numbers have been revised to suit council requirements and codes.



Bicycle Parking Schedule		
Type Comments	Bikes provided	Level
DOUBLE SIDED BICYCLE RACK	2	Street level

Visual obstructions
removed/relocated

Gridline 1 relocated
to the west to remove
obstructions from sightline

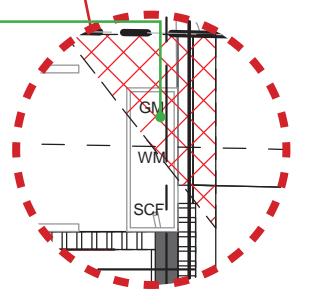


1.03 sightlines

1.03 Council comments:
The sightlines that will be available to vehicles exiting the car parks onto Comet Lane do not appear to comply with the requirements of AS2890.1.;

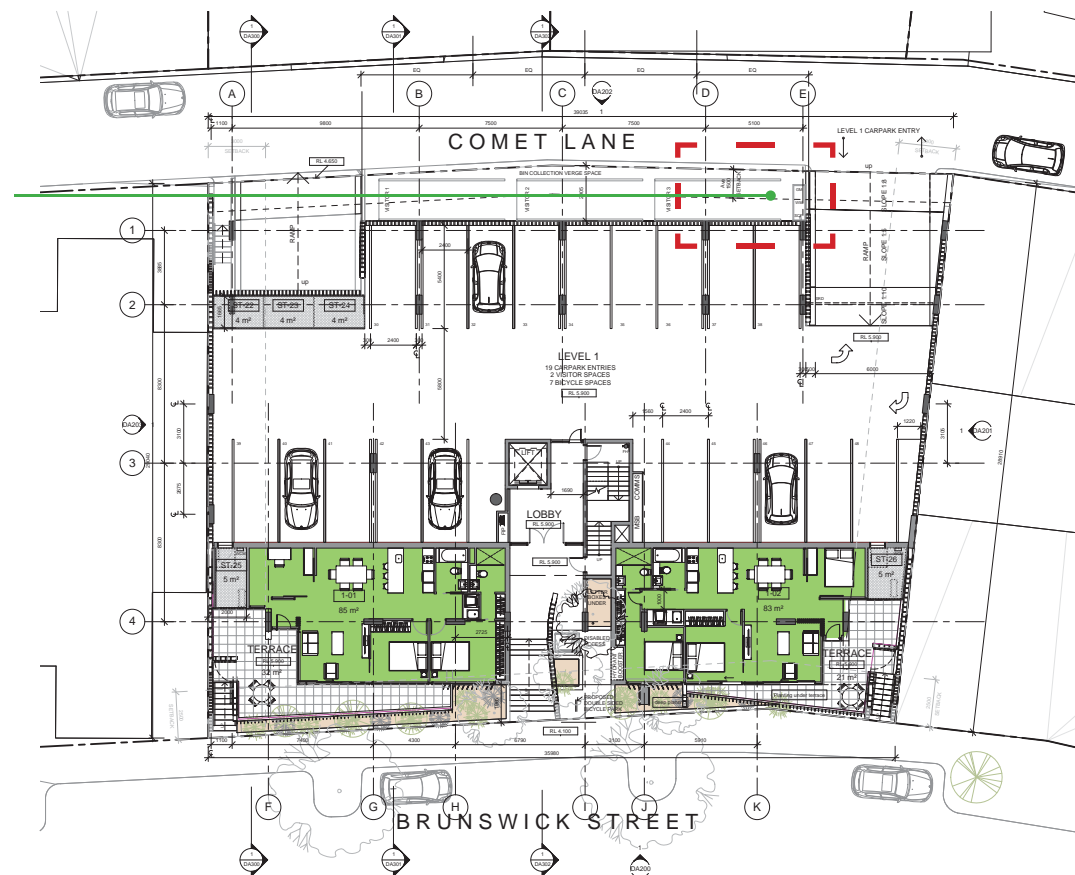
Design response:
Visual obstructions to sightline removed.

water, sewer & gas meters to
be housed in a low height
enclosure below ramp level.



1.0 traffic management

bin storage relocated from Comet Lane to basement level to reduce visual impact to sightline.

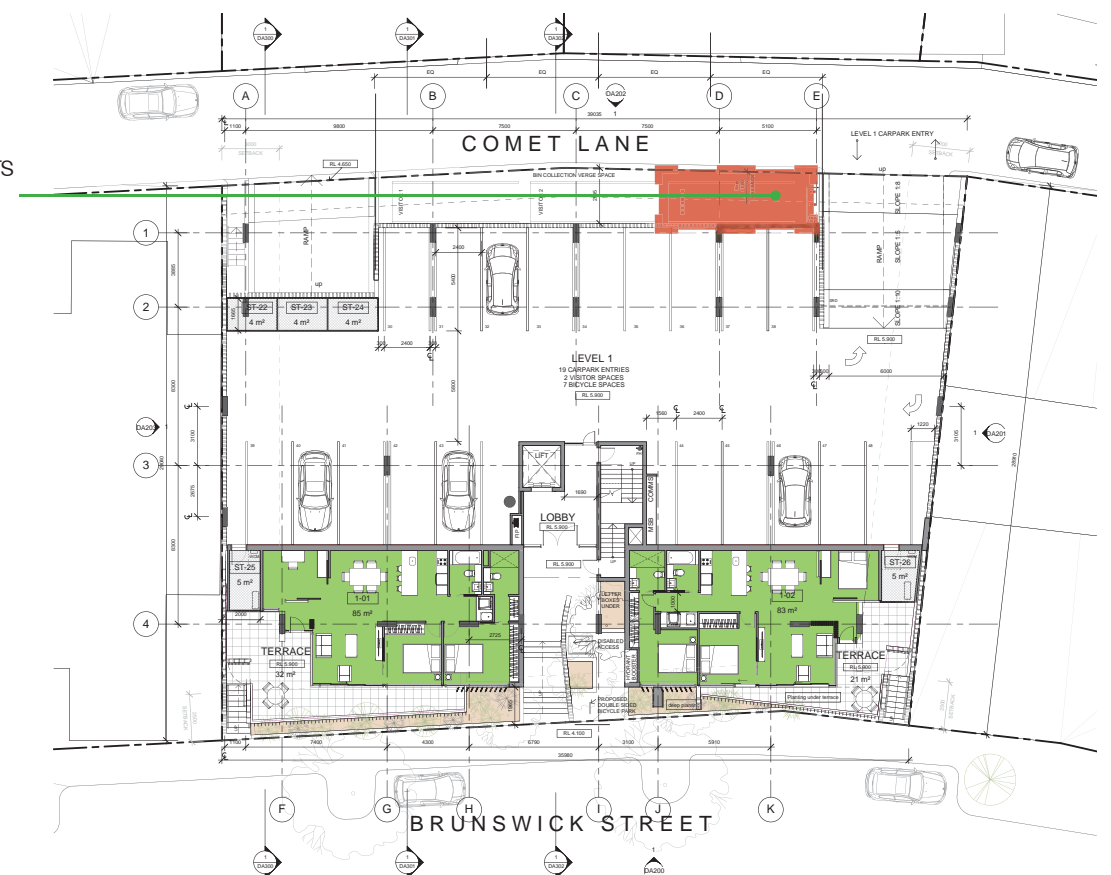


1.04 bin storage

1.04 Council comments:
The bin storage area on Comet Lane will restrict sight lines when exiting the Level 1 car park and the landscaping adjacent to the Basement car park ramp may restrict sight lines.

Design response:
Location of bin storage has been revised. No restriction to sightline.
Refer to figure diagram 2.04 of for new location.

additional visitors car park



1.05 visitors parking

1.05 Council comments
There is a clause in the parking requirements for Site 2 in the BFC's which allows for 50% of the visitor car parking to be provided on-street.
I was wondering if you were aware of this and whether you would consider this as an option to help address some of the above?

Design response:
x1 additional cars added to proposal.
Total visitor spaces required = 2.7
Total Visitor spaces provided = 3.0

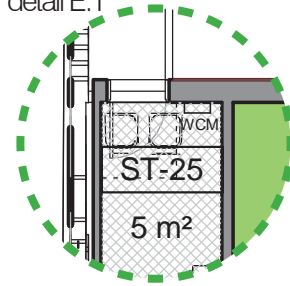
Parking Schedule		
Level	Description	Total Count
BASEMENT	5400 x 2400	29
Street Level	Parallel 2100x6600	3 (Visitors)
LEVEL 01	5400 x 2400	19

51

2.0 waste management option A

- 2.01 Council comments:
The bin store must equal 1m²/residential unit or a minimum of 38m².
- Design response:
Existing bin storage space from comet lane has been relocated to provide 37m Bin storage in the Basement level. Units 101, & 102 have been provided with dedicated bin storage area individual access to the car park refer diagram 2.01.
- 2.02 Council comments:
Basement collection using our low profile rear loader can only occur if the truck can complete the operation in a forward gear.
- Design response:
240lt bins to be collected from Comet lane. No truck access to basement required.
- 2.03 Council comments:
660lt bins are only available for use within a minimum 2.4m high basement via this low profile vehicle.
- Design response:
As above
- 2.04 Council comments:
One set of bins (1 recycle and 1 general 240lt) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.
- Design response:
28 bins provided in total. x24 in Bin Storage room x 4 bins located in individual storage space on level 01 refer diagram 2.04.
- 2.05 Council comments:
Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation
- Design response:
23.5m Verge space provided at Comet Lane refer to diagram 2.05.
- 2.06 Council comments:
Waste chutes are not supported, unless the City can be convinced that a permanent maintenance and cleaning program will prevent odours, rodents and cockroaches.
- Design response:
Refer 3.0 Waste Management Plan Option B.

detail E.1



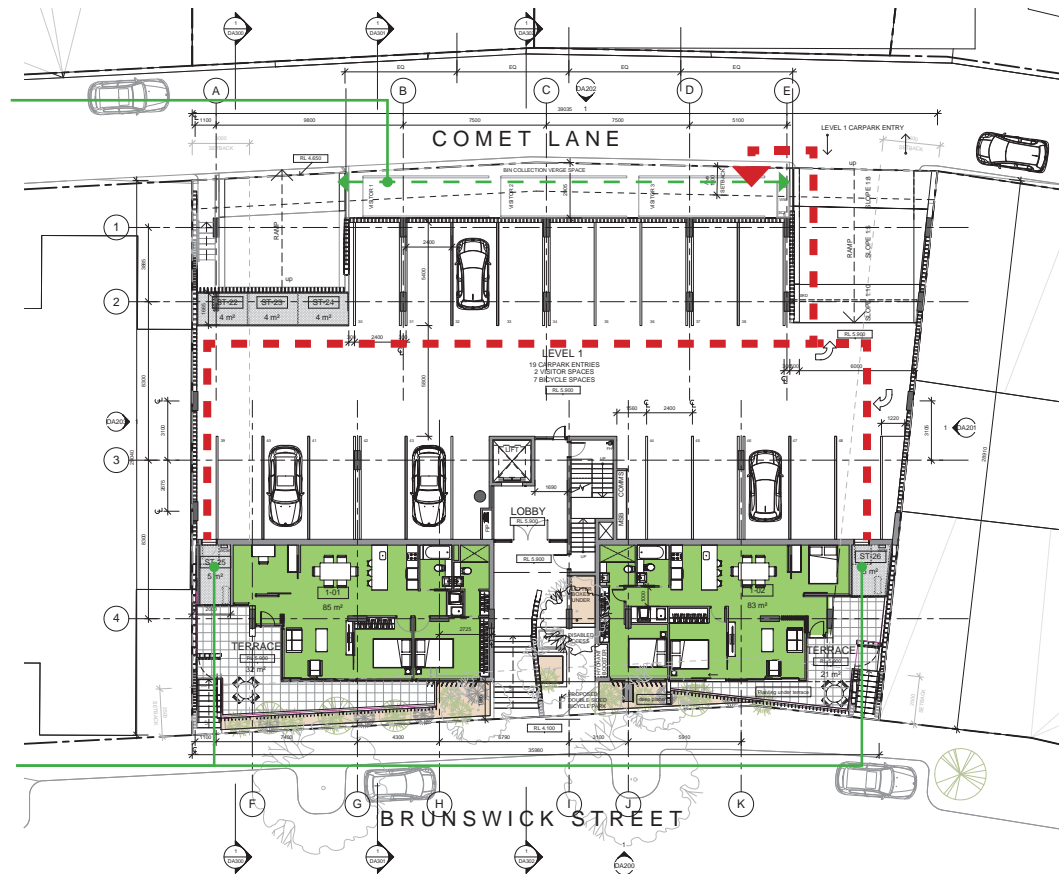
Units 1.01 & 1.02
provided with
1sqm dedicated
bin storage area

23.5m dedicated
verge space

bin storage
relocated from
Comet Lane to
basement level.

Level 1 Units
occupants
transport bins
to Comet Lane
verge space for
bin collection

Units 1.01 & 1.02
provided with
1sqm dedicated
bin storage area



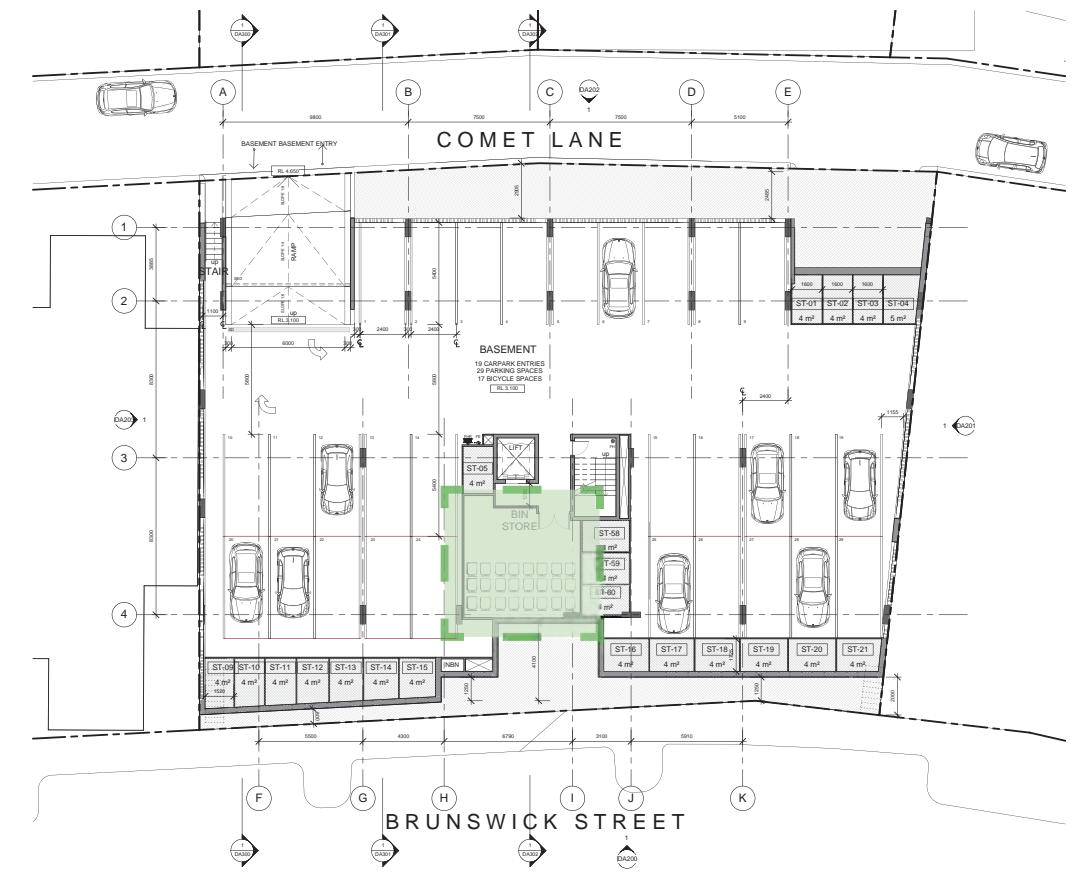
2.01 bin storage

2.01 Council comments:
The bin store must equal 1m²/residential unit or a minimum of 38m².

Design response:

Existing bin storage space from comet lane has been relocated to provide 37m Bin storage in the Basement level. Units 101, & 102 have been provided with dedicated bin storage area individual access to the car park.

2.0 waste management option A



2.04 bin numbers

2.04 Council comments:
One set of bins (1 recycle and 1 general 240lt) per 3 residential units.
38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.

Design response:

28 bins provided in total. x24 in Bin Storage room 240lt x 4 140lt bins located in individual storage space on level 01.

2.0 waste management option A

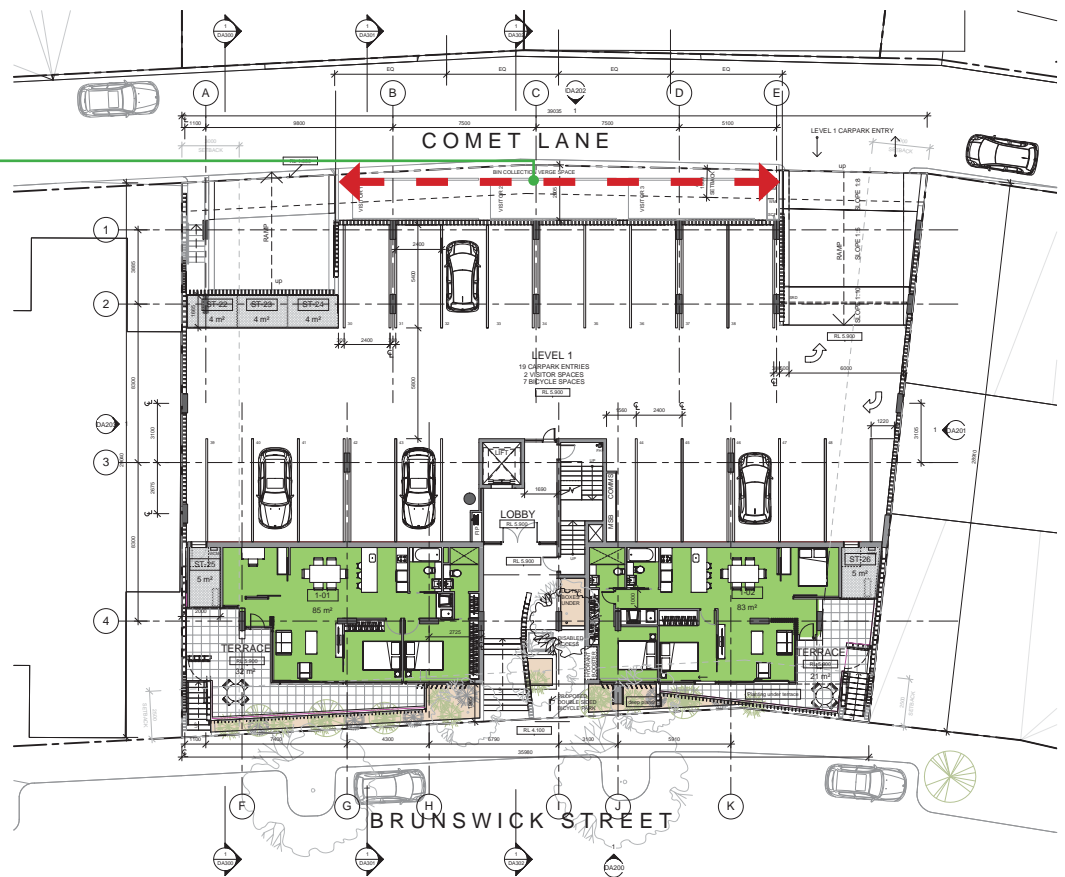
bin storage
relocated from
Comet Lane to
basement level.

Bins transported
to verge space
individually

Level 1 Units
occupants
transport bins
to Comet Lane
verge space for
bin collection.
Refer figure E



dedicated verge
space on comet
lane on street
level.



2.05 verge space

2.05 Council Comments
*Each bin set presented on the verge requires 1.8m. 23.4m of verge is
required on Comet Lane for presentation.*

Design Response:
23.5m Verge space provided at Comet Lane.

3.0 waste management option B

- 3.01

Council comments:
The bin store must equal 1m2/residential unit or a minimum of 38m2.

Design response:
Compactor as a separate unit effectively has a 1:5 compression rate with a minimum rate of 1:2, therefore decreasing the amount of area required for bins.
- 3.02

Not required
- 3.03

Not required
- 3.04

Council comments:
One set of bins (1 recycle and 1 general 240 lt) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.

Design response:
Compression rate minimizes the amount of verge space required. Decreasing the tipping load and landfill space from 50% to 20%.
- 3.05

Council comments:
Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation

Design response:
Required verge space potentially decreases by 30% 16.2m Verge space required
- 3.06

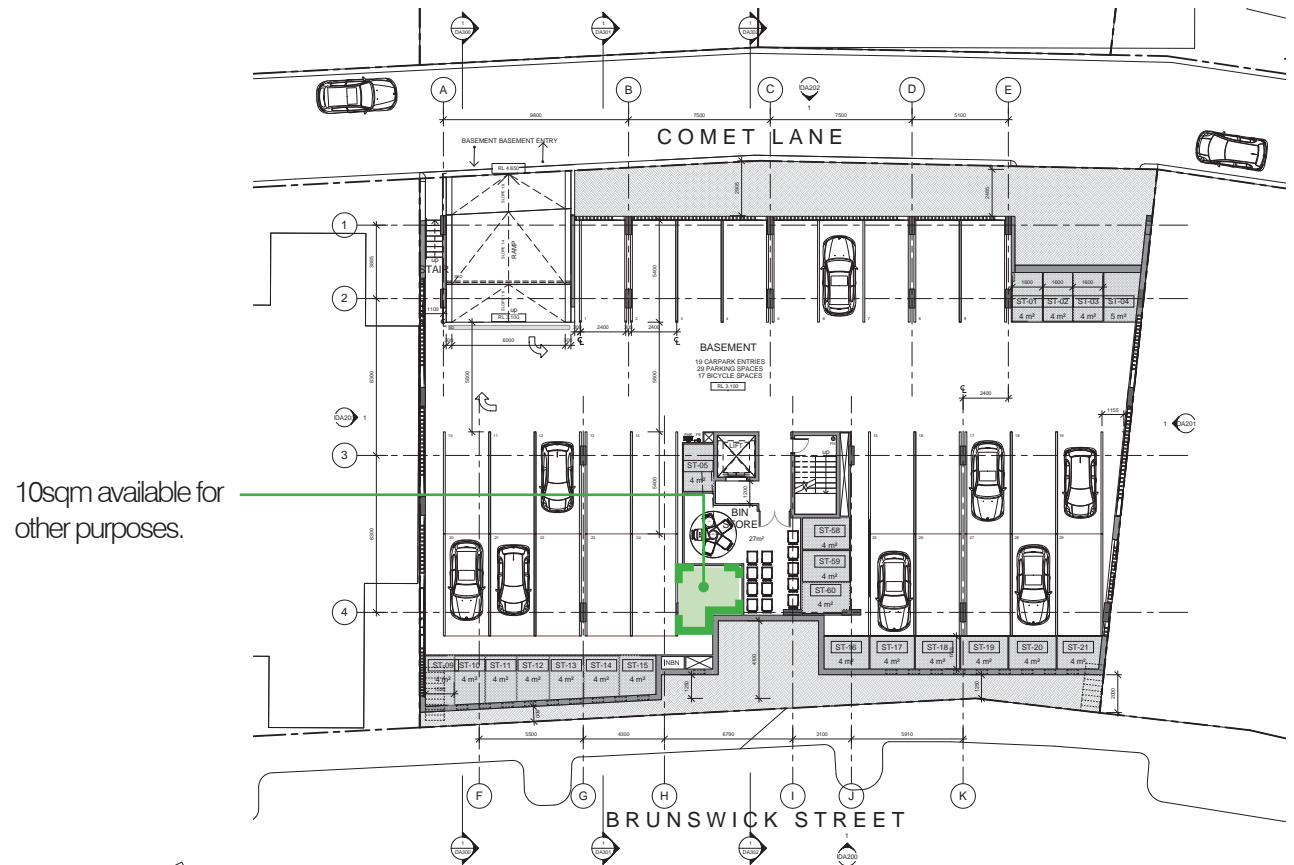
Council comments:
Waste chutes are not supported, unless the City can be convinced that a permanent maintenance and cleaning program will prevent odours, rodents and cockroaches.

Design response:
Bin storage room to be fully washable for regular cleaning. Waste chute will is to me mechanically ventilated to allowing for odour control with a permanent maintainance system.

The system is to be maintained by cleaners every 3 months and compactor to be serviced.

Bin room is to be fully enclosed to prevent rodents.

3.0 waste management option B



10sqm available for other purposes.

3.01 bin storage

3.01 Council comments:
The bin store must equal 1m2/residential unit or a minimum of 38m2.

Design response:
Compactor as a separate unit effectively has a 1:5 compression rate with a minimum rate of 1:2, therefore decreasing the amount of area required for bins.

16 bins in total (12 bins required for level 3-05 required + 4 bins for units 101 & 102)

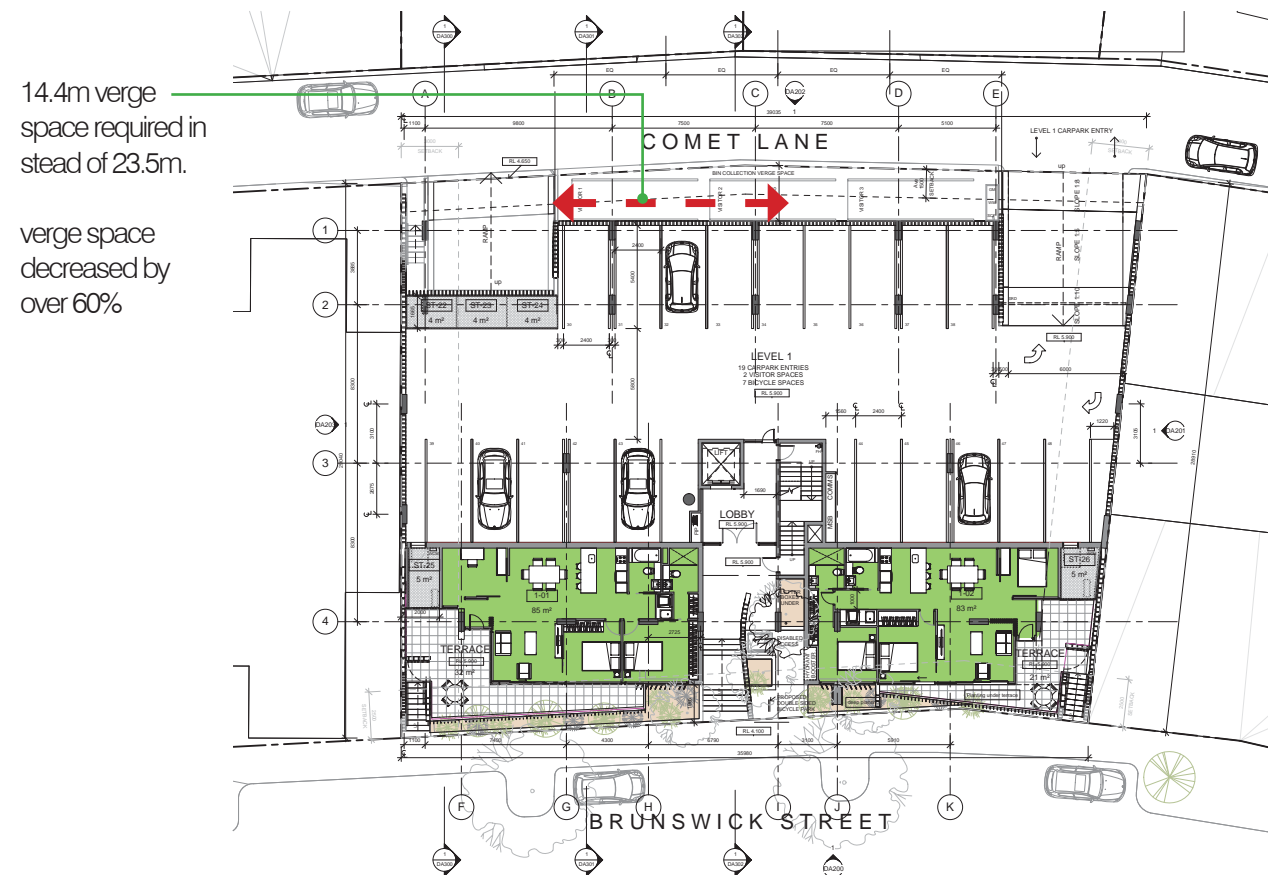


3.04 bin numbers

3.04 Council comments:
One set of bins (1 recycle and 1 general 240 lt) per 3 residential units. 38 units rounded up to 39 equals 13 sets or 26 bins as a minimum.

Design response:
Compression rate minimizes the amount of verge space required. Decreasing the tipping load and landfill space by 50% to 80%.

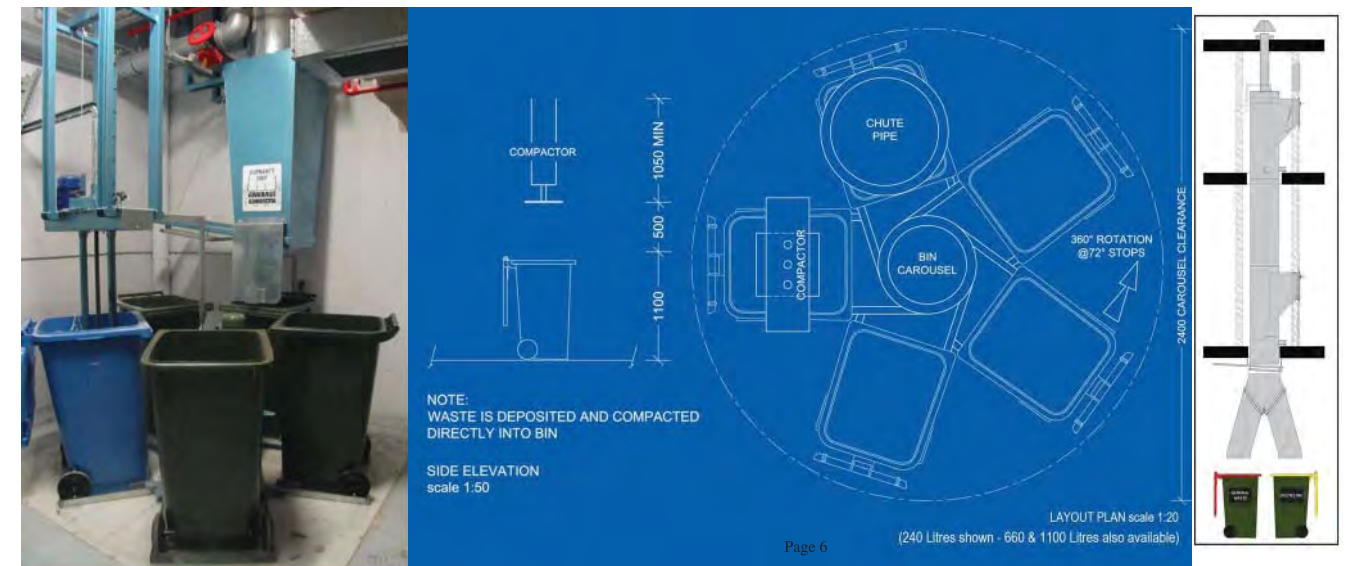
3.0 waste management option B



3.05 verge space

3.05 Council comments:
Each bin set presented on the verge requires 1.8m. 23.4m of verge is required on Comet Lane for presentation

Design response:
Required verge space potentially decreases by 60% 14.2m Verge space required



3.06 Advantages

Simple and easy to use system for garbage disposal and recycling.

Encourages participation in recycling while allowing care taker to closely monitor recyclable contamination. Allowing for further education for repeat offenders.

Easier transfer of garbage and recycling to the Bin storage area, eliminating leaking bag syndrome, manual transportation of rubbish bags which intern improves amenity of occupants.

Eliminates the possibility of residents cluttering bins and bin storage area with large items that need to be removed by a caretaker or cleaning staff.

System can operate with 240lt bins.

Compactor significantly decreases the requirement for available space & amount of landfill. Significantly decreasing the proposals ongoing impact on the environment.



WMK

 Open Corporation
DIVERSIFICATION GROUP

urbis

Technical Note: No 1b

Date: 14/11/2014

Project No: t14.266



Project: Lots 1017 and 1018 Brunswick Street, North Coogee

Subject: Car Park Management Plan

INTRODUCTION

The subject of this technical note is the proposed residential development located at Lots 1017 and 1018 Brunswick Street, North Coogee. The proposed development provides 29 car spaces in the basement level car park and 19 car spaces in the level 1 car park. Each parking level is accessed via a separate ramp connecting to Comet Lane, which runs along the rear of the development.

Based on advice provided by the Project Architect, City of Cockburn technical staff have identified two issues related to the car park ramp grades and sightlines:

-  *The access ramps exceed the maximum grade specified in AS2890.1 for the first 6m of the ramp; and,*
-  *Sightlines available to cars exiting the car park may be restricted by the bin storage area and landscaping.*

Transcore has been commissioned by Open Corporation to develop a Car Park Management Plan and to investigate and address the concerns raised by City of Cockburn regarding the ramp gradient and sightlines.

Accordingly, this management plan investigates traffic management measures to ensure safe and efficient use of the ramps and recommends usage of mirrors to improve sightlines for vehicles exiting the car park. .

RAMP GRADIENT AND SIGHT LINES FOR OUTBOUND VEHICLES

Section 3.3 of the Australian Standard AS2890.1:2004 requires that a flat area (maximum gradient 1:20), be provided at the top of a ramp for the first 6m into the car park from the property boundary. The flat area is provided to improve outbound vehicle sightlines on approach to a footpath or road edge.

The proposed basement and level 1 car park ramps provide a 1:8 grade ramp on approach to the footpath. Investigation of the proposed car park layout and ramps indicates that the site is physically constrained; providing a 6m length of 1:20 grade ramp would significantly impact the car park layout and number of bays which can be provided in the car park levels.

As both car park ramp driveways connect to a rear laneway with no footpaths, and as a result of low traffic and pedestrian volumes, the risk of any conflict is minimal. However improvement traffic management measures were investigated in order to facilitate outbound vehicle sightlines for both ramps on approach to the laneway and to improve safety.

Accordingly, it is recommended that convex mirrors be installed at either side of the car park exit points to improve visibility between pedestrians and outbound vehicles for both sides of Comet Lane. The approximate location and angle of the mirrors is shown in Figures 1 and 2 of **Appendix A**, and indicative sightlines are included to show the resulting angle of visibility.

The mirrors should be mounted at a suitable height and angled down to improve driver sightlines. Consideration should also be given to installing “Caution Watch for Vehicles” signs on the wall on either side of the driveways (refer to Figure 3 of **Appendix A**), to warn pedestrians to cross the driveways with care. These signs should be installed in a suitable location and angled to be clearly visible to pedestrians.

It is also recommended to install “SLOW pedestrian crossing” signs in both car park ramps at a suitable location to be clearly visible to outbound vehicles, as shown in Figure 3. This will alert outbound motorist to be aware of potential pedestrian movements and clearly defines the right of way to pedestrians.

CONCLUSIONS

The total number of outbound vehicle movements per day will be low for a residential development of this scale. As the car park ramps connect to a rear laneway with no footpath, the number of vehicular and pedestrian movements crossing the ramp driveways will be minimal. Therefore the frequency of pedestrian and vehicular interaction at the car park ramps is also expected to be minimal.

Nevertheless, installation of the recommended safety mirrors and supplementary signage will ensure clear lines of sight between outbound vehicles and

pedestrians and will alert drivers to beware of potential pedestrian movements on the laneway.

APPENDIX A – RECOMMENDED TRAFFIC MANAGEMENT TREATMENTS

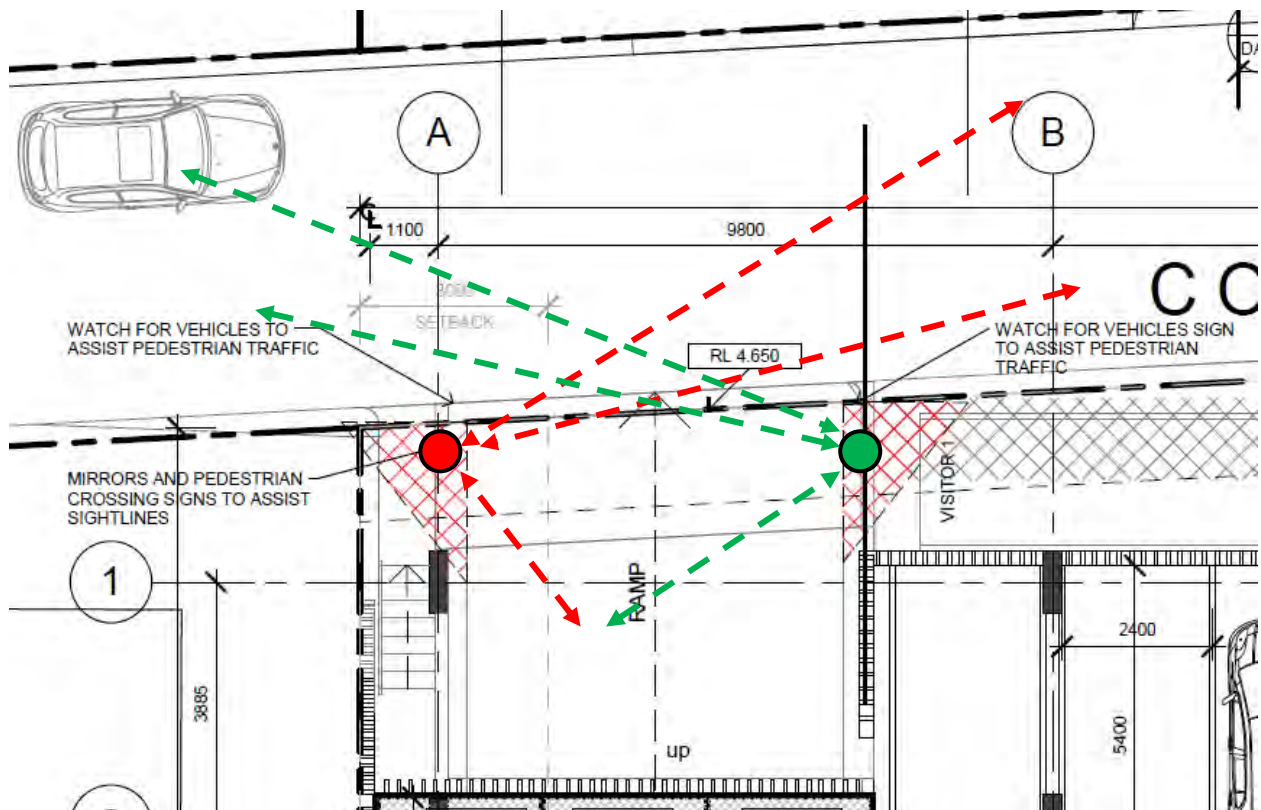


Figure 1. Recommended mirror treatments and proposed signage – basement ramp

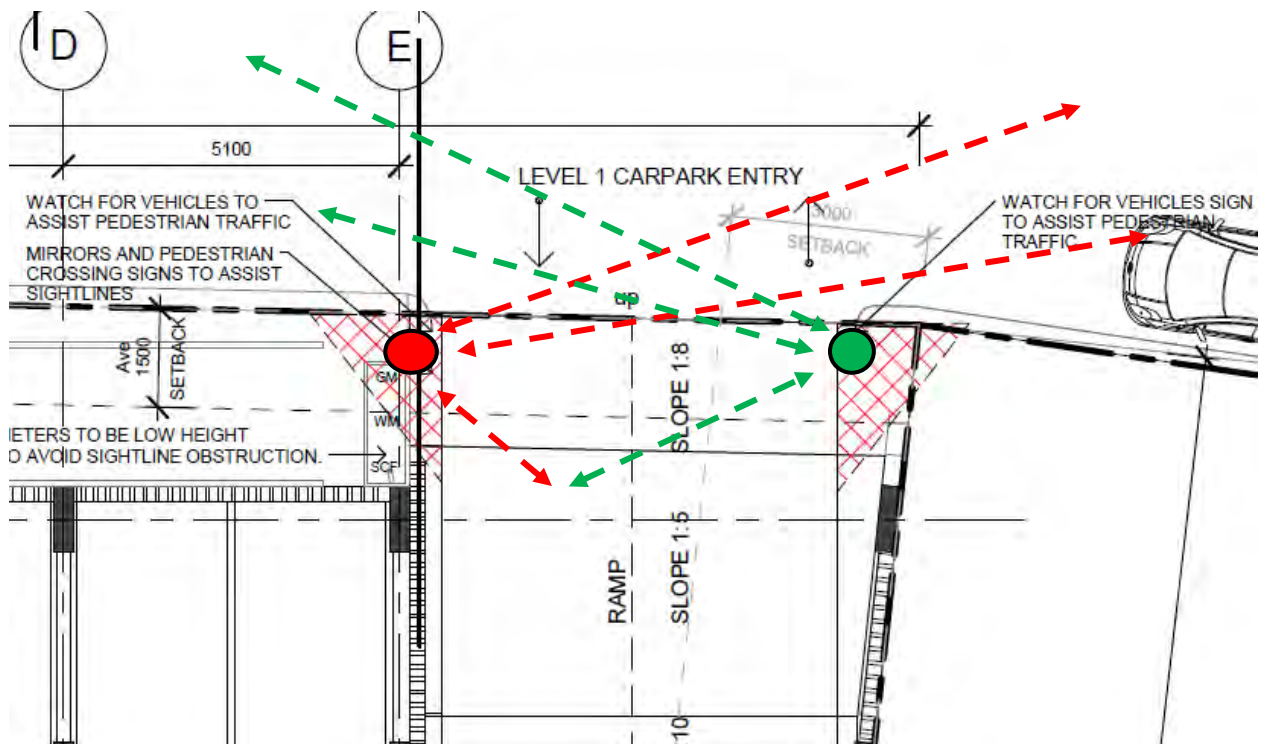
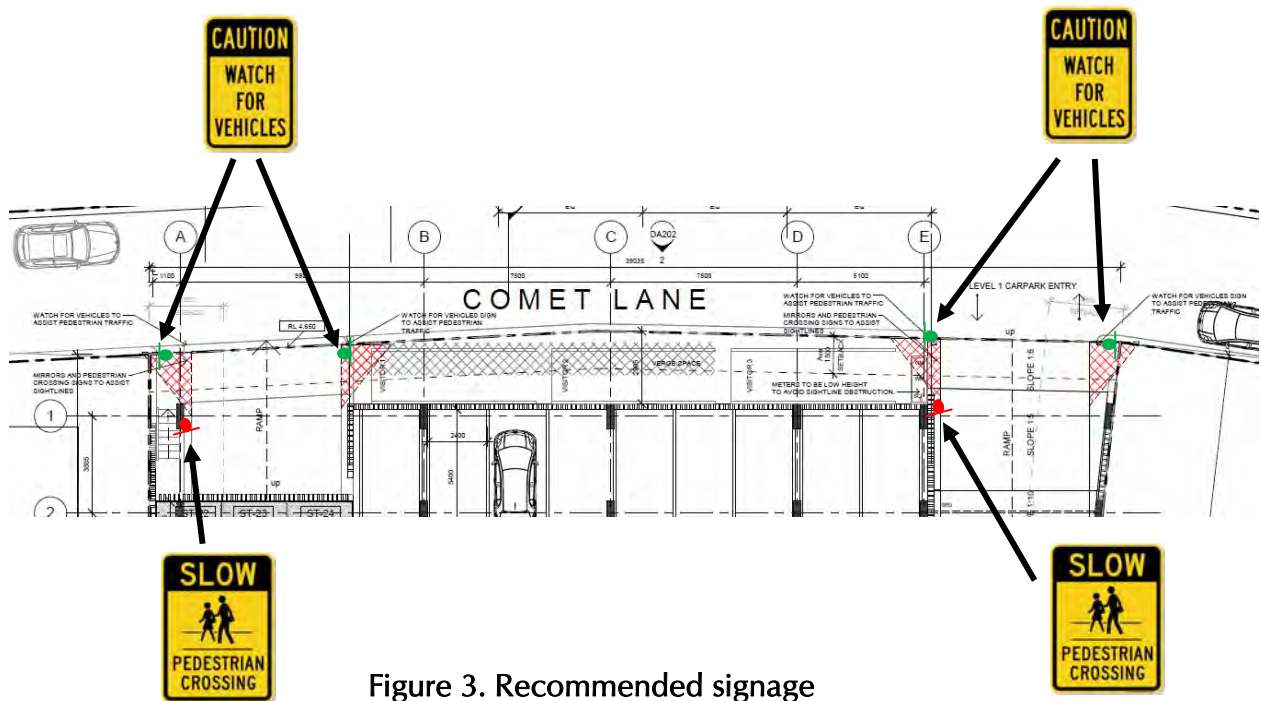
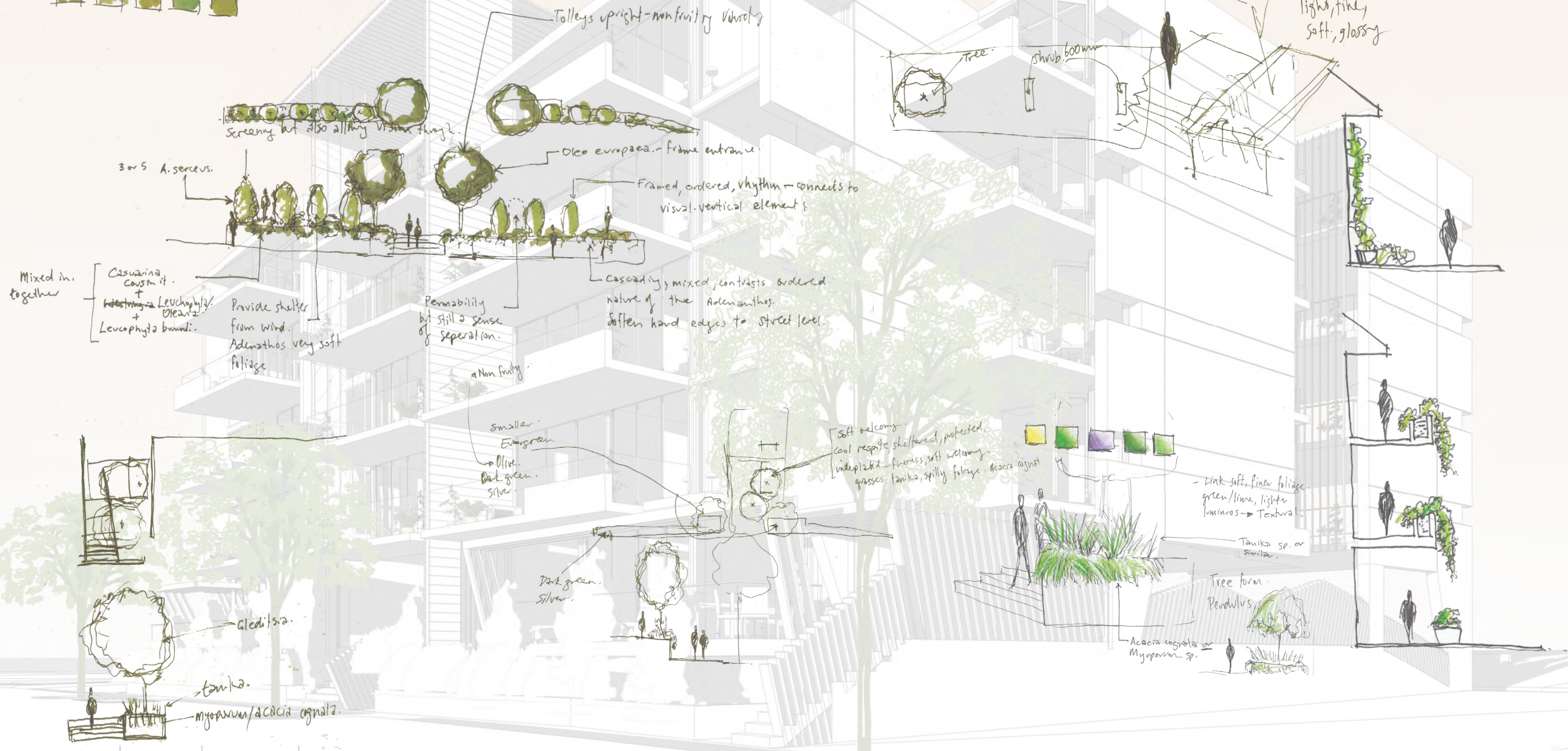
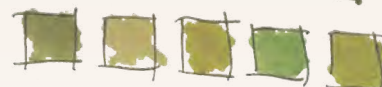


Figure 2. Recommended mirror treatments and proposed signage – level 1 ramp



Brunswick St Entrance.



LANDSCAPE CONCEPT DESIGN

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

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pg 3_LANDSCAPE CONCEPT THEMING

pg 4_LEVEL ONE PLAN

pg 5_LEVEL ONE ELEVATION

pg 6_PLANT PALETTE

pg 7_ROOF GARDEN CONCEPT

pg 8_ROOFTOP GARDEN PLANTING PALETTE

pg 9_VERTICAL GARDENS PLANT PALETTE & LOCATION

CONTENTS

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015



Landscape Concept Theming

The landscape concept is highly cognisant of the local context and the challenging coastal conditions in which the site is located.

The planting design seeks to ground the building within its context, simultaneously softening and fragmenting the edge between the built form and the street. Responding to the architectural form the planting design provides a soft and textural element that offers year round visual interest.



SERVICE_PRACTICAL_SURVEILLANCE



The Comet Lane interface ensures access to the meter box, with planting maintained to under 600mm adjacent to the carparks to maintain sight lines and passive surveillance.

RESPITE_SHELTERED_LIGHT_WELCOMING



The foyer area within the building recess becomes an area of respite from wind and sun, with soft and light planting providing a welcoming entrance.

SOFTENED_TEXTURAL_RHYTHM_SCREENING



The building is 'grounded' to the street with the use of planting as a means of fragmenting and softening the interface between building and street. The contrasting forms between the cascading and upright planting provides visual interest.

LANDSCAPE CONCEPT THEMING

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015





Native planting to car bay
— nib to a max. of 600mm to
ensure clear sight lines.

- 2 x Xanthorrhoea grass tree species to lobby area of building.

- Planting to allocated planters adjacent to Brunswick St. Planting to existing enhance existing street planting conditions and provide a soft interface between the building and street.

LEVEL 1 PLAN

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:200@A3

Level 1, 55 St Georges Tce,
Perth, WA 6000 Australia

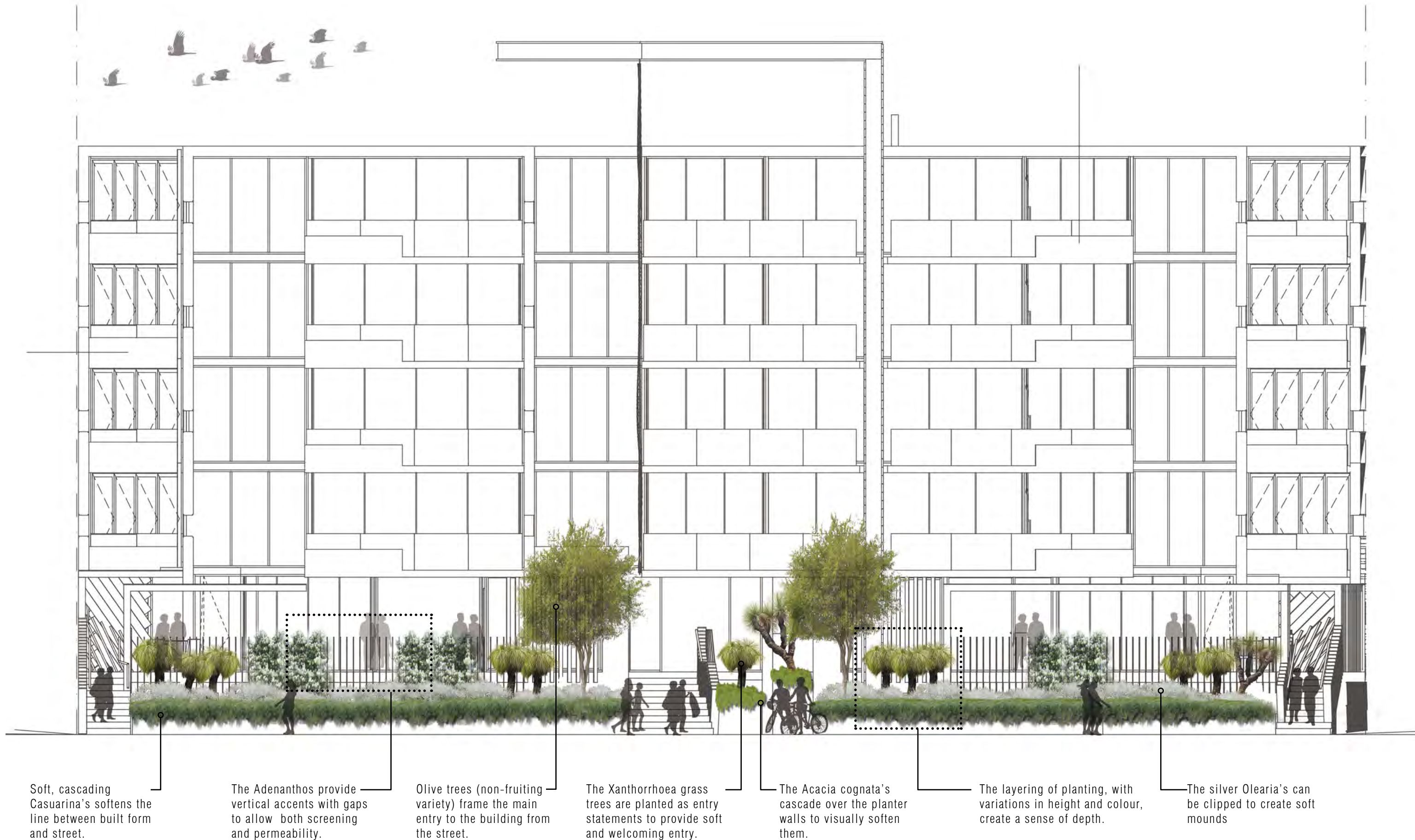
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LEVEL 1 ELEVATION

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:150 @A3

Level 1, 55 St Georges Tce,
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COMET LANE

LARGE SHRUB



Adenanthos sericeus 'Silver Streak'
Woolly Bush
3m x 1.5m

SMALL SHRUB



Olearia axillaris 'Little Smokie'
Coastal Daisy Bush
1m x 0.8m

PLANT NOTES:

All plants listed respond well to pruning, and can be easily managed.
Plant sizes quoted (height x width) are estimated size of mature plants.

All plant species selected will tolerate the exposed and salty coastal conditions of the site, and blend seamlessly with the surrounding streetscape.

FOYER ENTRY

TREE



Xanthorrhoea preissii*
Grasstree
2m x 1m (typical)

SMALL SHRUB



Acacia cognata 'Limelight'
Narrow Leaf Bower Wattle
1m x 1m

GRASS



Lomandra longifolia 'Tanika'
Basket Bush
0.6m x 0.6m

*NOTE: Xanthorrhoea preissii also used in Brunswick St

BRUNSWICK ST

TREE



Olea europaea
Olive (non fruiting variety)
6m x 3m

LARGE SHRUB



Adenanthos sericeus 'Silver Streak'
Woolly Bush
3m x 1.5m

SMALL SHRUB



Olearia axillaris 'Little Smokie'
Coastal Daisy Bush
0.8m x 0.8m

GROUND COVER



Casuarina glauca 'Cousin It'
Swamp Oak
0.3m x 1m

PLANT PALETTE

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE NTS

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Artificial turf is used as a low maintenance option adjacent to the pool and deck area.

The Pandanus are able to withstand the open and exposed environment whilst providing shade around the seating areas.

The herb gardens are planted with hardy, drought tolerant herbs.

Agave's are used feature specimens in planter beds, with underplantings of contrasting and colourful plants.

ROOFTOP GARDEN CONCEPT

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE 1:200 @ A3

Level 1, 55 St Georges Tce,
Perth, WA 6000 Australia

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TREE



Pandanus utilis
Swan Neck Agave
2.5m x 2.5m

PLANTERS AND POTS WITH PLANTING



The use of additional pots and planters as required as a means of providing additional wind protection and screening.



FEATURE PLANTING



Agave attenuata
Swan Neck Agave
0.6m x 0.6m

GROUNDCOVERS



Carpobrotus edulis
Ice Plant
0.2m x 0.5m



Echeveria elegans
Chickens and Hens
0.1m x 0.5m



Aeonium purpurea
Schwartzkop
0.2m x 0.5m



Senecio serpens
Chalksticks
0.2m x 0.5m



Tradescantia pallida
Purple Queen
0.2m x 0.5m



Lomandra "Seascape"
0.2m x 0.5m

HERBS FOR HERB GARDEN



Thymus vulgaris
Thyme
0.2m x 0.2m



Cymbopogon citratus
Lemongrass
0.6m x 0.6m



Oreganum vulgare
Oreganum
0.2m x 0.2m



Rosmarinus officinalis
Rosemary
0.6m x 0.6m



Salvia officinalis
Sage
0.3m x 0.3m

ROOFTOP GARDEN PLANTING PALETTE

LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015

DATE 06.10.2014

DWG NO A

REV

SCALE NTS

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CLIMBERS FOR GREEN FACADE



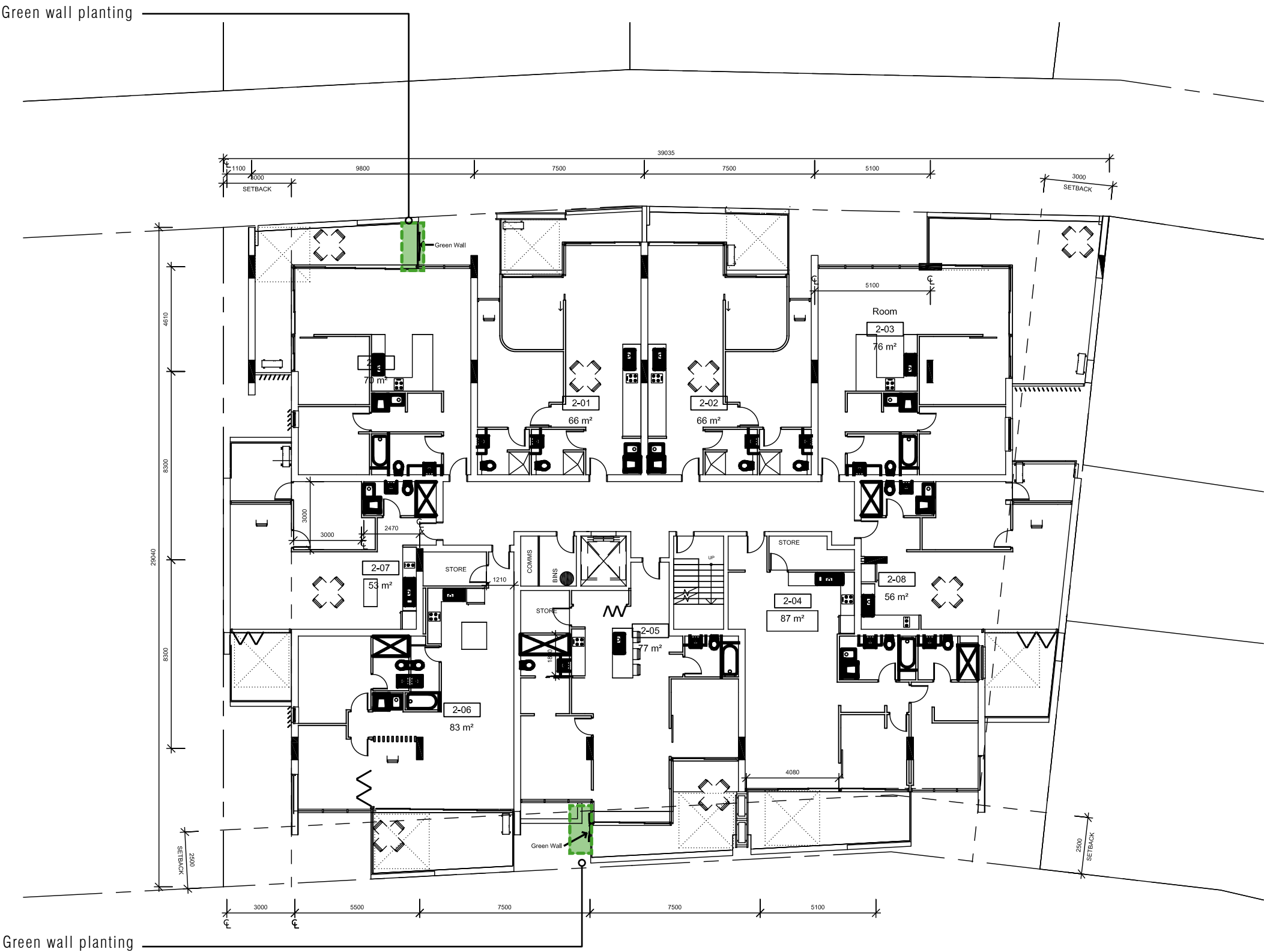
Hardenbergia comptonia
Native Wisteria
3.0m x 3.0m
An endemic coastal climbing plant of the Perth region. Hardenbergia is an excellent climber, with most of the vegetative mass of the plant is concentrated toward the top half.



Trachelospermum jasminoides
Star Jasmine
2.0m x 2.0m
Can be used as both a ground cover and climber and is suitable in sunny and coastal aspects. Star Jasmine also grows densely from the base to give a dense vegetative covering over walls



Examples of green facades with climbing plants growing on stainless steel wires.



VERTICAL GARDENS PLANT PALETTE & LOCATION

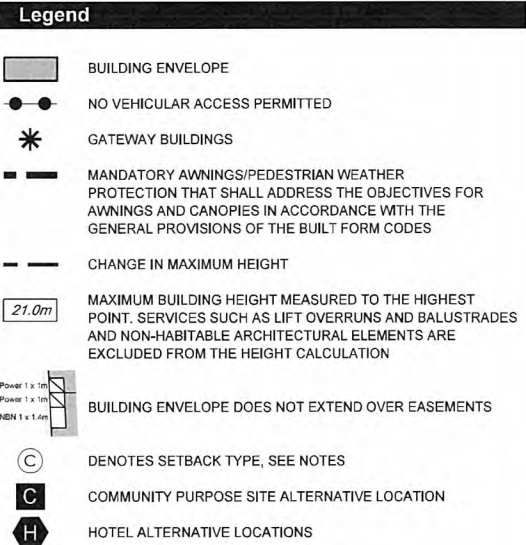
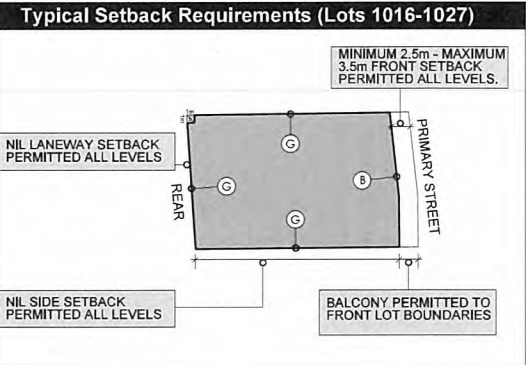
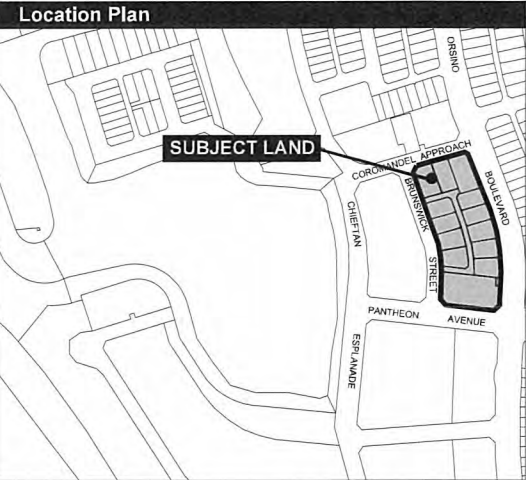
LOTS 1017 & 1018 BRUNSWICK ST, PORT COOGEE ND 1015



Detailed Area Plan – Site Specific Building Requirements

This Detailed Area Plan (DAP) replaces the Site Specific Building Requirements of the Port Coogee Marina Village Built Form Codes in relation to Site 2 only. All other provisions of the Port Coogee Marina Village Built Form Codes apply to the land the subject of this DAP (Lots 1015 to 1027).

Land Uses			
Ground Floor:	Pantheon Avenue:	Commercial/Retail. A non residential use is mandatory at ground level	
	Orsino Boulevard:	Commercial/Retail/Residential	
	Coromandel Approach/Brunswick Street:	Commercial/Residential	
Level 1:	All streets	Commercial/Residential	
Level 2 and above:	All streets	Residential	
Setbacks		Type	<i>(Typical street setback illustration reference to Built Form Codes)</i>
Basement:	All boundaries:	-	Nil permitted
Ground Floor/Level 1/	Pantheon Avenue:	C	Nil required.
Level 2:	Orsino Boulevard/Coromandel Approach/Brunswick Street:	B	2.5m minimum / 3.5m maximum. Note: nil setback permitted to Lot 1015 Orsino Boulevard frontage as indicated on DAP.
	Lot side boundaries	G	Nil permitted
	Internal Laneway/Access Road	G	Nil permitted
Level 3 and above:	Pantheon Avenue	C	Nil minimum / 3.0m maximum
	Orsino Boulevard/Coromandel Approach/Brunswick Street	B	2.5m minimum / 3.5m maximum
	Lot side boundaries	G	Nil permitted
	Internal Laneway/Access Road	G	Nil permitted
Projections: Balconies and architectural elements may extend to the lot boundary. On commercial and retail frontages, awnings and canopies at ground level may extend beyond the boundary by a maximum of 3.0m or to within 0.5m from the back of adjacent kerb.			
Height			
Overall:	Lots 1015, 1020-1022: 21.0m maximum height. Lots 1016-1019 & 1023-1027: 17.3m maximum height. Development fronting Orsino Boulevard is required to be a minimum of three storeys.		
Minimum floor level height to Pantheon Avenue:	Ground to first floor:	4.0m minimum	
	Top of Podium Parapet (Level 1):	8.0m-10.0m [measured from finished pavement level]	
	Top of Podium Parapet (Level 2):	11.5m-13.5m [measured from finished pavement level]	
Wind	All designs are subject to wind assessment in accordance with the Wind General Provision as detailed within the Built Form Codes.		
Design Elements	Each dwelling shall be provided with a minimum of one balcony.		
Gateway Building (Lot 1015)	Built form to corners noted as 'gateway buildings' shall be designed with consideration to framing the street via setback, height, mass and detail elements, in accordance with the Icon and Gateway Building General Provisions as detailed within the Built Form Codes.		
Hotel Site (Lot 1015)	This site has been allocated as a possible location for the required hotel under the Port Coogee Local Structure Plan.		
Community Purpose Site (Lot 1015)	This site has been allocated as a possible location for the community purpose use required by the Port Coogee Local Structure Plan.		
Vehicular Access	All lots shall obtain vehicle access via the laneway. No vehicular access is permitted to lots via Orsino Boulevard, Pantheon Avenue, Coromandel Approach or Brunswick Street.		
Parking	A total of 18 on-street parking bays are available for inclusion in the calculation of visitor parking provided for multiple dwellings on Lots 1015-1027. The 18 on-street parking bays are allocated specifically as provided below: - Lots 1016 to 1027: 1 on-street parking bay per lot for visitor parking for multiple dwellings. - Lot 1015: 6 on-street parking bays for visitor parking for multiple dwellings.		
Fencing	Orsino Boulevard/Coromandel Approach/Brunswick Street:	Type 1/Type 2 (Refer to Built Form Codes for typical fencing configuration)	
	Pantheon Avenue:	No fencing permitted other than Type 3 to service areas only.	
	Laneways/Internal Access Roads:	Type 3 to service areas only/Type 4	



Endorsement Table

This Detailed Area Plan has been adopted by Council and signed by the Principal Planner:

Principal Planner

Date

29.11.2011

Site Specific Building Requirements Detailed Area Plan - Marina Village (Lots 1015-1027)

PORT COOGEE
AN AUSTRALAND PROJECT



Form 2 - Responsible Authority Report (Regulation 17)

Property Location:	No. 53-74/9 (Lot 802) McCabe Street, North Fremantle
Application Details:	Variations to Five Storey Multiple Dwelling Development (26 Multiple Dwellings) and Basement Vehicle Parking Level
DAP Name:	Metro South-West Joint Development Assessment Panel
Applicant:	TPG Town Planning, Urban Design and Heritage
Owner:	Taskers Living Pty Ltd
LG Reference:	DAPV50003/14
Responsible Authority:	City of Fremantle
Authorising Officer:	Manager Development Approvals
Department of Planning File No:	DP/13/00843
Report Date:	22 December 2014
Application Receipt Date:	18 November 2014
Application Process Days:	50 days
Attachment(s):	1: Location Plan 2: Development Plans dated 12 November 2014 (A0.01, A1.01, 1.02, 1.03, 1.04, 1.05 & 1.06, A2.01, 2.02 & 2.03) 3: Previous Responsible Authority Report (DAP80006/13) (DP/13/00843)

Officer Recommendation:

That the Metro South-West Joint Development Assessment Panel resolves to:

Approve DAP Application reference DP/13/00843 and accompanying plans dated 12 November 2014, A0.01, A1.01, 1.02, 1.03, 1.04, 1.05 & 1.06, A2.01, 2.02 & 2.03 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4.

Amended Conditions:

- 1. This approval relates only to the development as indicated on the approved plans, dated 12 November 2014. It does not relate to any other development on this lot and must substantially commence within four years from the date of this decision letter. If the subject development is not substantially commenced within a 4 year period, the approval shall lapse and be of no further effect.**
- 2. The design and construction of the development is to meet the 4 star green star standard as per Local Planning Policy 2.13 or alternatively to an equivalent standard as agreed upon by the Chief Executive Officer, City of Fremantle. Any costs associated with generating, reviewing or modifying the alternative equivalent standard is to be incurred by the owner of the development site. Within 12 months of an issue of a**

certificate of Building Compliance for the development, the owner shall submit either of the following to the City to the satisfaction of the City of Fremantle;

- a) a copy of documentation from the Green Building Council of Australia certifying that the development achieves a Green Star Rating of at least 4 Stars, or
 - b) a copy of agreed equivalent documentation for instance where there is no green star rating tool available certifying that the development achieves a Green Star Rating of at least 4 Stars.
3. All storm water discharge shall be contained and disposed of on site or otherwise approved by the City of Fremantle.

Additional conditions

11. Prior to the issue of a Demolition Permit a Demolition and Construction Management Plan shall be submitted to the City to the satisfaction of the City of Fremantle addressing the following matters:
- a) Use of City car parking bays for construction related activities;
 - b) Protection of infrastructure within the road reserve;
 - c) Protection of street trees;
 - d) Security fencing around construction sites;
 - e) Gantries;
 - f) Access to site by construction vehicles;
 - g) Contact Details;
 - h) Site offices;
 - i) Noise - Construction Work and Deliveries;
 - j) Sand drift and dust management;
 - k) Waste management;
 - l) Dewatering;
 - m) Traffic management; and
 - n) Works affecting pedestrian areas.

The approved Demolition and Construction Management Plan shall be adhered to throughout the demolition of the existing building on site and construction of the new development.

12. Prior to occupation, twelve (12) bicycle parking bays, in accordance with design element 5.3.3 of the Residential Design Codes, shall be provided and thereafter maintained to the satisfaction of the City of Fremantle.

Advice note(s)

- i. The applicant is advised that determination as to whether conditions of approval have been satisfied is delegated to the Chief Executive Officer – City of Fremantle.*
- ii. The applicant is advised that the installation of air-conditioning plant and equipment is exempt from the need to obtain planning approval when it is positioned so as to not extend more than 1.0m above the highest point of an existing roof. Further planning approval of the City will be required if this height is exceeded.*

Background:

Property Address:	No. 53-74/9 (Lot 802) McCabe Street, North Fremantle
Zoning	MRS: Urban
	LPS: Development Area
Use Class:	Multiple Dwellings – N/A
Strategy Policy:	Local Planning Area 3 – North Fremantle
Development Scheme:	City of Fremantle – Local Planning Scheme No. 4 – McCabe Street Structure Plan
Lot Size:	Lot 802: 5608m ²
Existing Land Use:	N/A - Vacant
Value of Development:	Total cost of development: \$8,000,000 Variation to previous proposal: \$500,000 in additional costs

The application includes modifications to an application previously determined by the joint Development Assessment Panel (JDAP) pursuant to DAP80006/13 (DP/13/00843). A detailed discussion of the site background and history is contained in the preliminary section of **Attachment 3**.

Details: outline of development application

The application seeks planning approval for 'Modification to approved five storey Multiple Dwelling Development (26 Multiple Dwellings) and basement vehicle parking level' (see **Attachment 2**). The modified proposal differs from that previously approved by the JDAP pursuant to DP/13/00843 in the following ways;

- An increase in the number of Multiple Dwellings from 20 dwellings to 26 dwellings;
- An increase in the external wall height of the proposal from 46.50m AHD to 49.50 AHD (+3.0m);
- An increase in the maximum roof height of the proposal from 47.10m AHD to 49.50m AHD (+2.4m);
- An increase in the volume of vehicle parking provided from 43 bays to 52 bays (inclusive of 4 external visitors bays);
- An increase in the plot ratio of the proposed building from 2,677m² to 2,730m² (+53m²).

Legislation & policy:

The application has been assessed against the following legislative documents:

- City of Fremantle Local Planning Scheme No. 4 (LPS4) – application for development on the site is to be determined in accordance with provisions of Part 10 of the Scheme.

Legislation

The legislative framework and policy base providing for the assessment and determination of the subject application is as follows:

- City of Fremantle Local Planning Scheme No. 4 (LPS4), including the following provisions;
 - Table 2 - Zoning;
 - Clause 6.2.3.2 – Structure plan requirements;
 - Clause 10.2 - Matters to be considered by Council;
 - Clause 11.8 – Design Advisory Committee; and,
 - Schedule 1 – Dictionary of Defined Words and Expressions.
- State Planning Policy 3.1 Residential Design Codes (R-Codes);
- Local Planning Policies including;
 - Local Planning Policy 1.3 – Public notifications of planning proposals;
 - Local Planning Policy 1.9 – Design Advisory Committee & Principles Of Design;
 - Local Planning Policy 2.13 - Sustainable Buildings Design Requirements; and,
 - Local Planning Policy 3.11 – McCabe Street Area – Height of New Buildings; and,

Consultation:

Design Advisory Committee (DAC) (*Internal referral*)

The original proposal was reviewed by the DAC at its meetings of 15 November and 4 December 2013. The proposal was supported by the DAC subject to a condition relating to further details relating to finishes and materials.

The proposal includes internal modifications which are likely to be of interest to the City's DAC. The previously recommended condition of the DAC regarding external colours, finishes and textures is still included. It is noted that the City's DAC considered matters of design and internal amenity. A number of the dwellings proposed include rooms which have no direct access to natural light. The City's DAC has typically raised concerns regarding such elements however it is considered that this matter alone is not a primary statutory planning matter that would require a redesign or a recommendation for refusal. On this basis it is balance recommended this aspect of the design is supported.

Should however these matters be of significant concern to Council, the following alternative recommendation is provided in the 'Conclusion' section of this report;

- That a condition of approval be applied requiring DAC consideration prior to the issue of a Building Permit.

Manners by which the DAC may wish to deal with the matter of internalised bedrooms includes configuration of internal spaces, inclusion of light wells/skylights where possible, amalgamation of some two bedroom apartment into single bedroom apartments.

Public Consultation

The application was required to be advertised in accordance with Clause 9.4 of the LPS4. In accordance with Local Planning Policy 1.3 – Public Notification of Planning Proposal, the application was advertised as a ‘Significant Application’. LPP1.3 specifies that significant applications should be advertised for 28 days. Due to the processing time constraints, the proposal can only be advertised for 26 days only (closing 16 December 2014). Advertising of the application included notice by the following means;

- A sign being placed on-site throughout the duration of the advertising period;
- Notice of the application being included on the City’s website, social media pages and in the local newspaper circulations;
- A community information session being held on 11 December 2014;;
- Written notice being provided to adjoining landowners within 100m of the subject site advising them of the proposal and inviting them to the information session specified above;
- Written notice being provided to elected members and ‘Precinct Groups’ of the City.

At the conclusion of the advertising period, the City had received 14 submissions including 14 objections. The objections raised in the submissions are summarised as follows;

- The proposal results in no benefit to the wider community and does not achieve a diversity in housing;
- The height of the proposal is too great and will impact on amenity and streetscape;
- There is a lack of parking available and the additional traffic volumes will impact on the safety of the local road network;
- The density of the development and the surrounding site is too great;
- View corridors will be impacted by the additional height and bulk of the proposal
- The proposal is inconsistent with the adopted structure plan;
- There is a lack of open space resulting from the proposal;
- The additional height should include less bulk and greater graduation of scale;
- Responses from the Town of Mosman Park should be given greater weight;
- Sufficient social, economic, transport, environmental and security assessments have not been undertaken;
- The proposal will result in a loss of privacy and solar access;
- If approved, the proposal will set an undesirable precedent for the introduction of greater height in the area.

Ordinary Council Meeting

The application was presented to the Ordinary Council Meeting (OCM) of the City on 17 December 2014. The OCM resolved as follows;

‘Cr J Strachan MOVED to defer the item to the next appropriate Ordinary Meeting of Council to consider Application reference DP/13/00843 and accompanying plans dated 15 October 2013, SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4, so

that the proposal can be presented to the 12 January 2014 Design Advisory Committee meeting of the City of Fremantle for comment.'

The conclusion section of this report includes an alternative recommendation and conditions which reflect the above resolution should the JDAP seek comments from the City's DAC regarding the proposal.

Planning assessment:

The subject land is zoned 'Development Area' and is not given a density coding. As a result, the assessment of the proposed development is largely provided for in the Structure Plan.

Clause 6.2.3.2 of LPS4 requires that;

'The subdivision and development of land within a Development Area is to generally be in accordance with any structure plan that applies to the land.'

Consideration of the development is therefore made, generally, against the content of the Structure Plan.

External Wall Height

The adopted Structure Plan provides a maximum external wall of 43.50m AHD and a maximum ridge height of 48.00m AHD for the subject site. Local Planning Policy 3.11 – McCabe Street (Height of New Buildings) provides a maximum height for development of 14.0m. As the Structure Plan provides a more specific height requirement for the land, the Structure Plan is preferred and the higher order statutory document (the policy was used to inform the structure plan).

The table below outlines the required (as per the adopted Structure Plan), previously approved and currently proposed AHD levels of the external wall height and maximum roof ridge height.

Element	Structure Plan	Previously approved	Proposed	Variation to Structure Plan
Maximum Wall	43.50m AHD	46.50m AHD	49.50m AHD	+6.00m AHD
Maximum Roof	48.00m AHD	47.10m AHD	49.50m AHD	+1.50m AHD

The merit based decision is supported for the following reasons;

- The Structure Plan outlines that buildings in the location of the proposed development should be limited to four storeys. The building proposed is clearly a five storey proposal but the addition storey is setback from the main building wall to all elevations. This not only reduces the bulk and scale of the building but takes advantage of space that would otherwise form a pitched roof. Given the setback of the additional floor, the proposal is not considered to be of greater bulk, scale or impact compared to a four storey proposal with a pitched roof or loft level;

- The portions of wall height that exist above the Structure Plan requirement are setback from the edge of the main building face, reducing the overall bulk of the proposal;
- The additional height, in particular the additional external wall height is not considered to unreasonably restrict access to direct light or views of significance to surrounding properties having regard to the separation distance between the development and the nearest residential property;
- The proposed modifications are considered minor in nature having regard to previous approvals and represent only a marginal departure from the recommendations of the Structure Plan;
- The additional height is unlikely to contribute to a loss of privacy or solar access given the nearest residential property is located approximately 30m away from the building.

Bicycle parking

The modified proposal does not include any direct provision for bicycle parking bays. The deemed-to-comply criteria of the R-Codes specifies that 11.2 (12) bays could be provided and the proposal comply. A condition of approval requiring provision of these bays is therefore recommended.

Visual Privacy

The Structure Plan outlines that the desired density of the subject site ought to be equivalent to R80; despite the fact that no density coding is directly applicable under the scheme maps of LPS4. On the basis of the R80 code, the proposal is considered to meet all of the deemed-to-comply requirements of the R-Codes in the following ways;

- All major openings and raised outdoor living areas to boundaries of adjoining residential properties (including the internal separation between the proposed development and other residential developments on the subject site) meet the cone-of-vision setback requirement of the R-Codes; or
- The elevations where a lesser setback is sought address non-residential premises at No. 19-21 McCabe Street) and in those instance, the visual privacy provisions do not apply.

Sustainable Building Design

The proposed development is required to achieve a rating of not less than 4 Star Green Star (or equivalent) using the relevant Green Building Council of Australia rating tool pursuant to Local Planning Policy 2.13 – Sustainable Building Design (LPP2.13).

The applicant has proposed the use of an alternative energy efficiency rating system in lieu of the 4 Star Green Star requirements. Use of this rating tool is consistent with previous resolutions of the City and Joint Development Assessment panels relating to recent applications.

A condition of approval requiring achievement of an equivalent rating under the alternative tool is recommended.

Construction Management

The officer's recommendation includes a condition relating to the submission of a construction management plan. The condition is intended to ensure construction of the development does not result in a loss of amenity to adjoining sites and also coordinates development in a complex location, such as at the subject site.

Conclusion:

The application seeks planning approval for modifications to an approved five storey multiple dwelling development (26 Multiple dwellings) at the subject site. The proposal is summarised as follows;

- The subject site is zoned 'Development Area (DA18) pursuant to LPS4 and is subject to the adopted Structure Plan;
- In accordance with clause 6.2.3.2 of LPS4, the proposal is required to be generally in accordance with the Structure Plan;
- The proposed building envelope is very similar to that already approved by JDAP;
- The proposal is generally consistent with the Structure Plan in terms of building position, scale, density, height and vehicle traffic; and,
- The proposal is required to meet the sustainable building requirements of LPP2.13 and a condition of approval is recommended to this affect;

The application is considered to meet the relevant planning framework and is therefore recommended for conditional approval.

Given the scope of DAC comments, or potential concerns, are unknown it may be considered unreasonable to determine the application without any DAC comments. It is noted the City's OCM recommended that the application be deferred for this reason. The following alternative recommendation is provided to allow the application to be deferred so DAC comments can be sought;

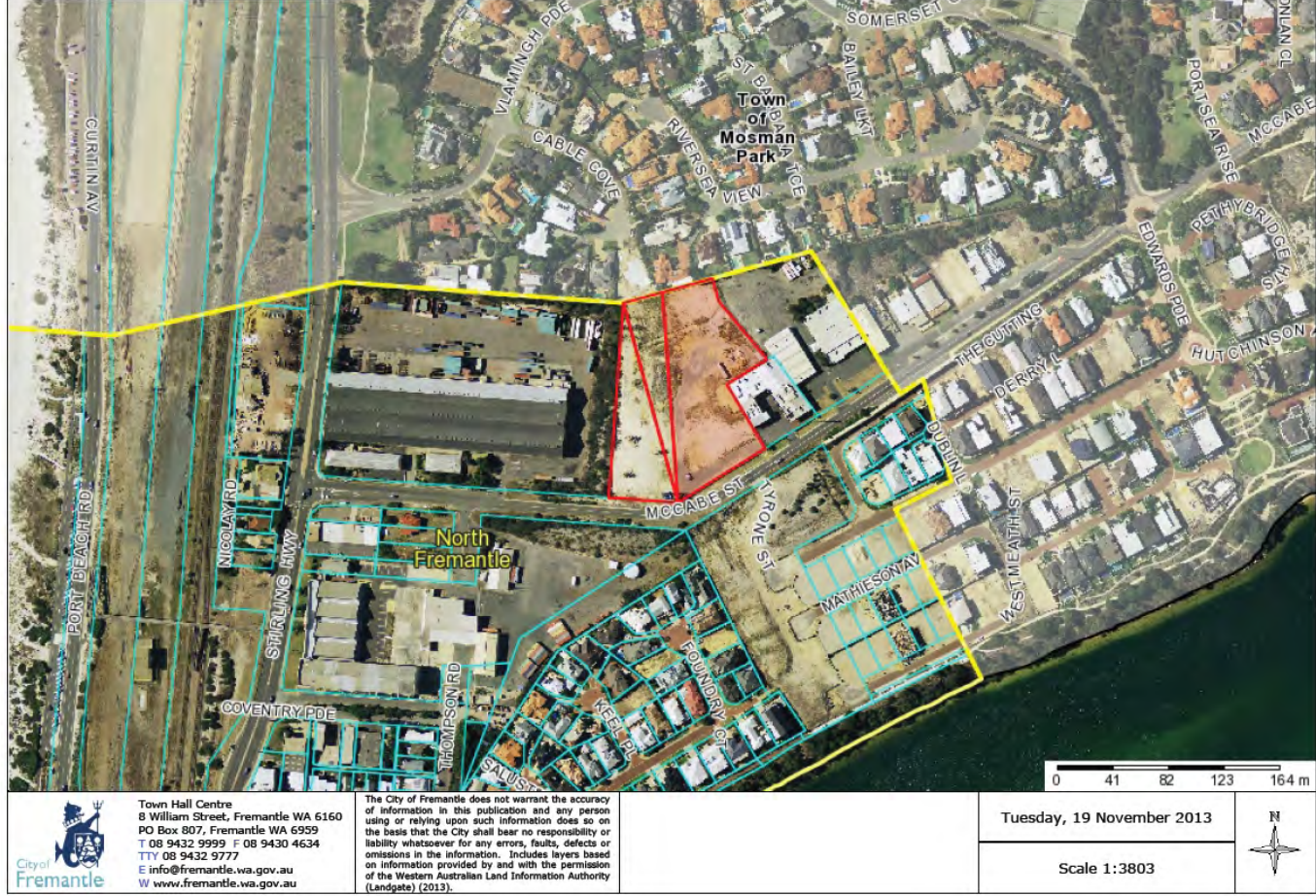
'That the Metro South-West Joint Development Assessment Panel resolves to:

DEFER DAP Application reference DP/13/00843 and accompanying plans dated 15 October 2013, SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4, so that the proposal can be presented to the 12 January 2014 Design Advisory Committee meeting of the City of Fremantle for comment.'

Alternatively, an additional condition could be included to ensure DAC consideration prior to the issue of a Building Permit. Such a condition (#13) could be as follows;

'Prior to the issue of a Building Permit, the plans hereby approved are to be presented to the City's Design Advisory Committee for comment specifically relating to the internal bedrooms. Any recommendation made by the Design Advisory Committee shall be given regard and the plans modified thereto, to the satisfaction of the Chief Executive Officer – City of Fremantle.'

ATTACHMENT 1: Location Plan



ATTACHMENT 2: Development Plans dated 12 November 2014

Project : TASKERS
Lots 800, 801, 802 & 803, McCabe Street, North Fremantle.
Multi Residential Development - "Tekona" - West Building



Architectural Drawings :

A0.01 1:300 Site Plan

A1.01 1:100 Basement Level Floor Plan

A1.02 1:100 Level 1 Floor Plan

A1.03 1:100 Level 2 Floor Plan

A1.04 1:100 Level 3 Floor Plan

A1.05 1:100 Level 4 Floor Plan

A1.06 1:100 Level 5 Floor Plan

A2.01 1:100 Elevations West & South

A2.02 1:100 Elevations East & North

A2.03 1:100 Sections

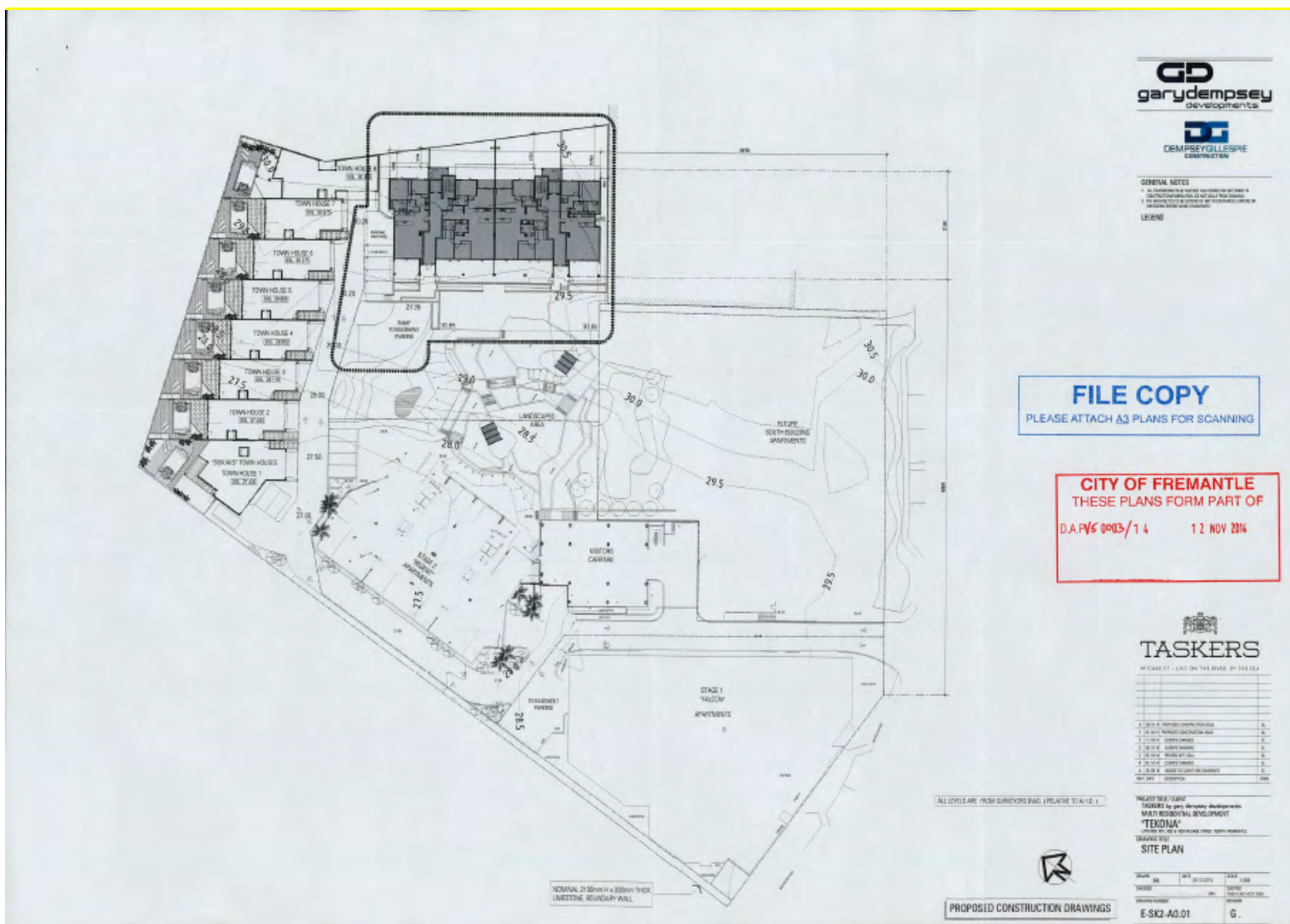
PROPOSED CONSTRUCTION DRAWINGS

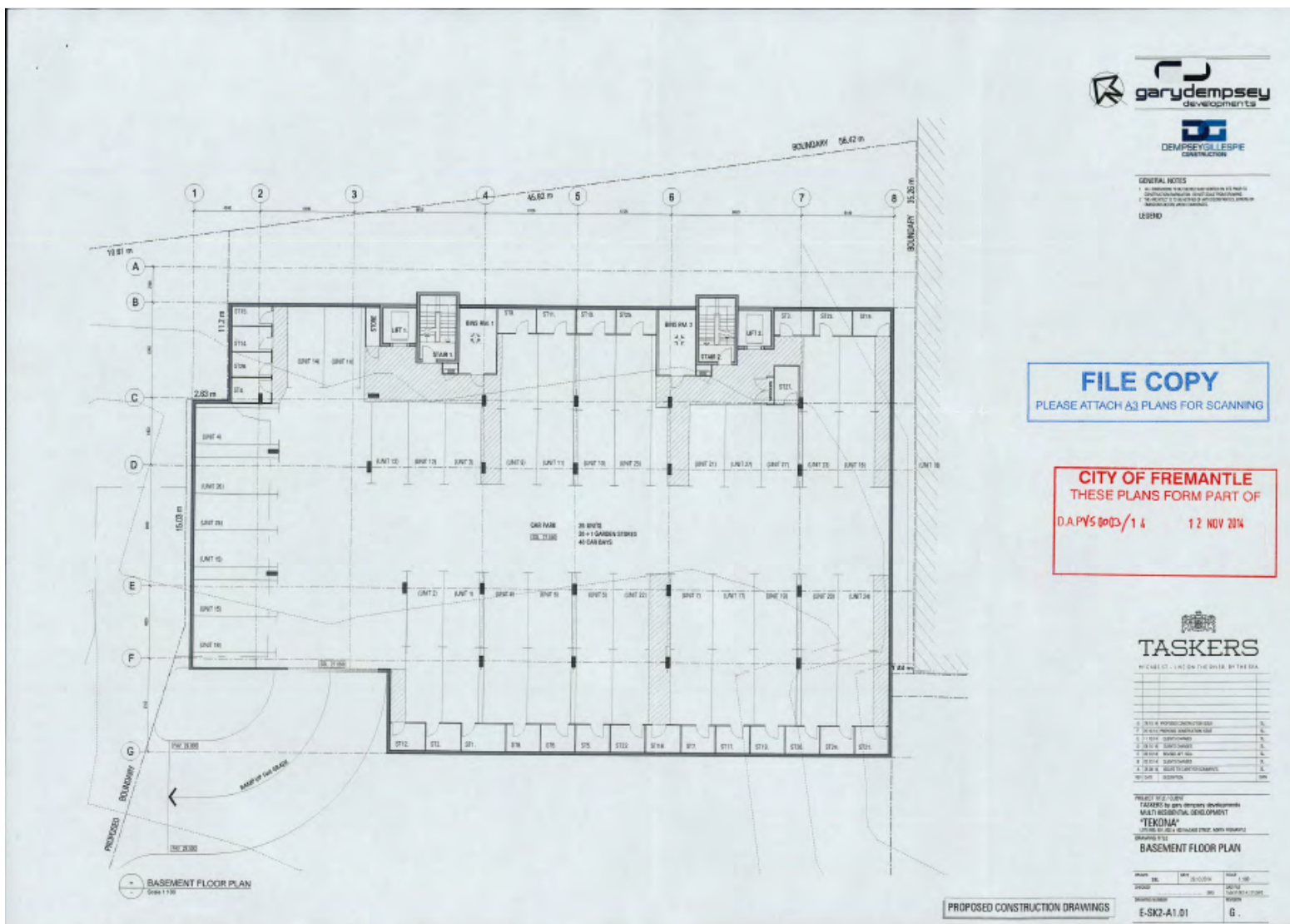
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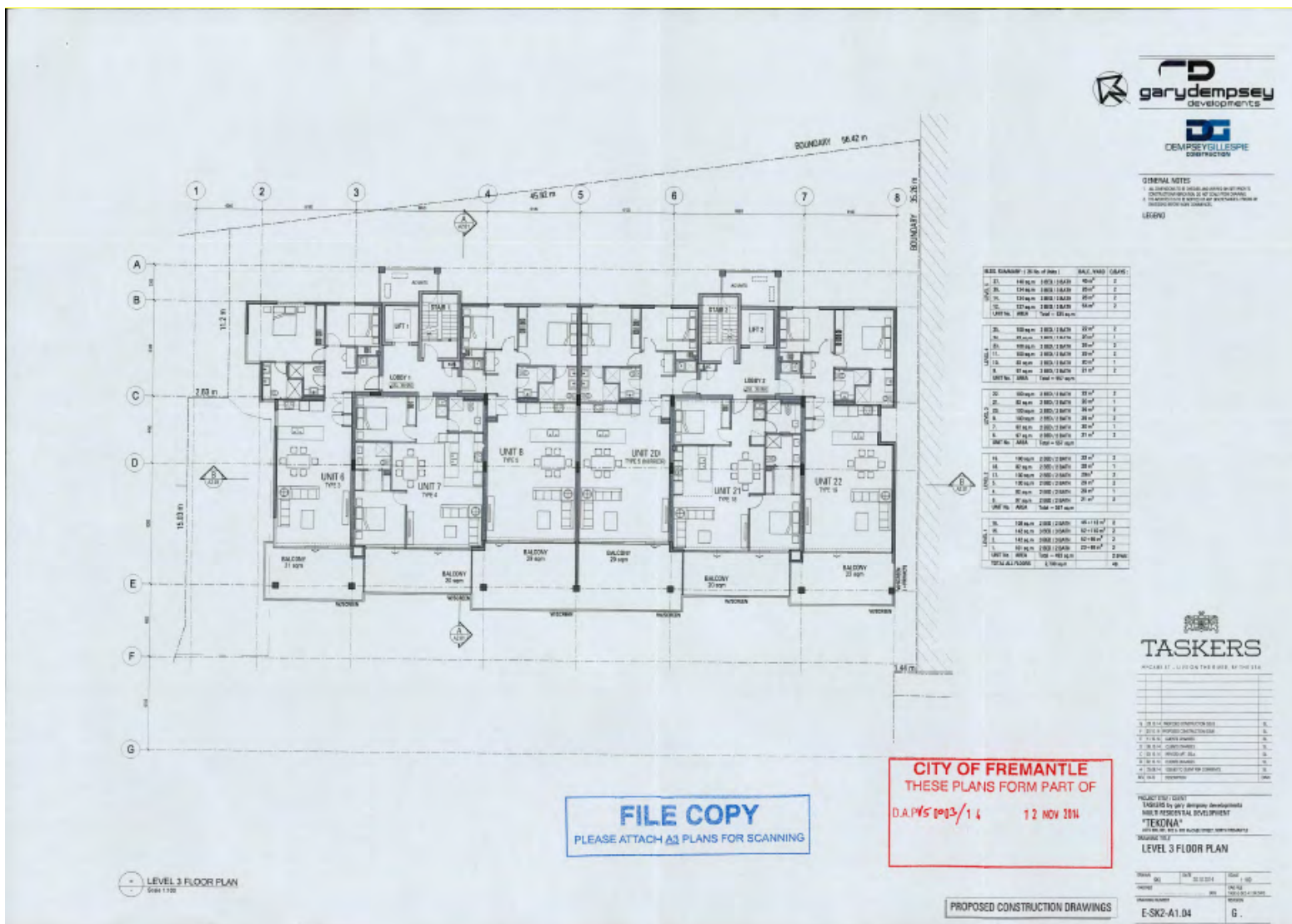
PLEASE ATTACH A3 PLANS FOR SCANNING

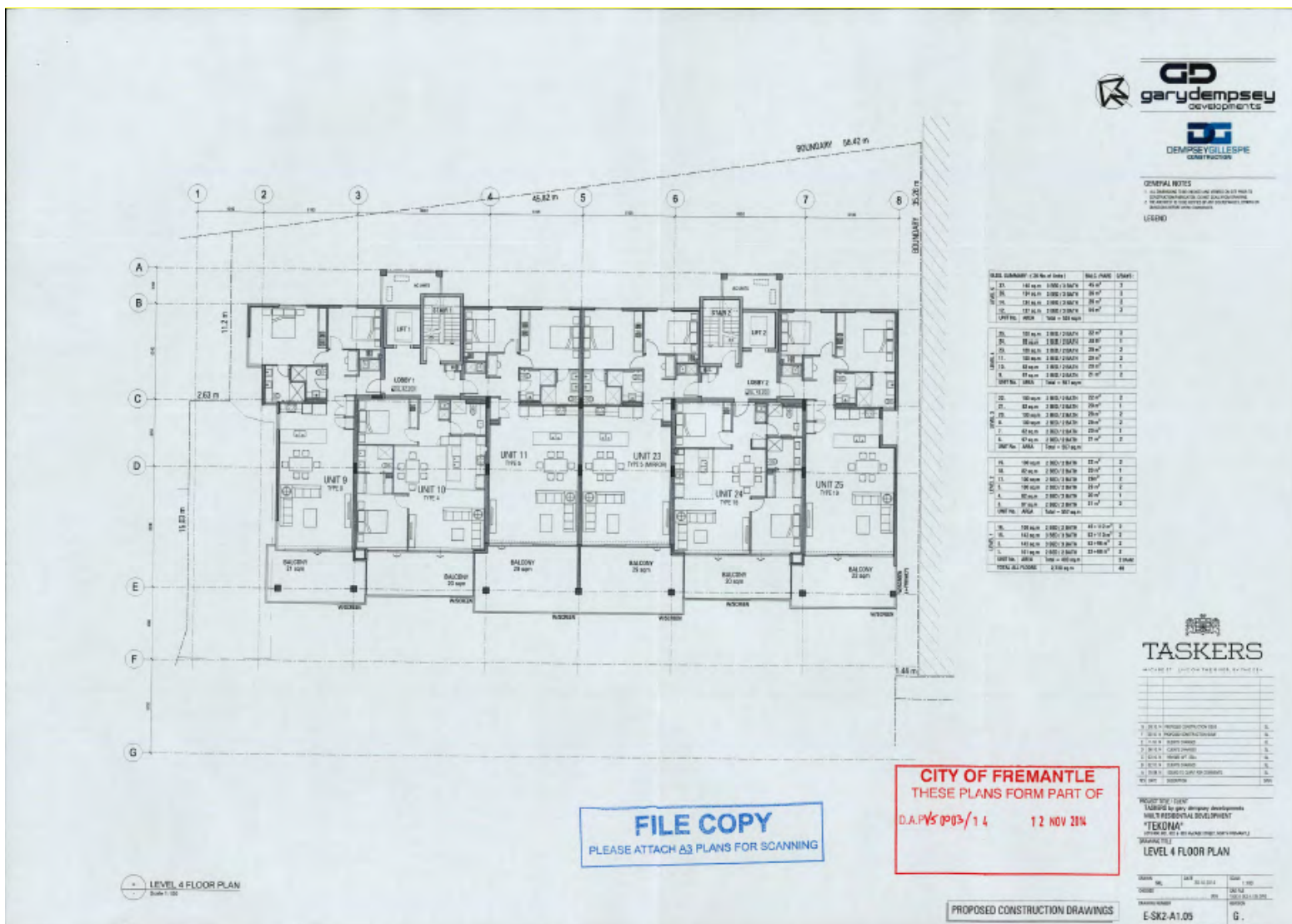
CITY OF FREMANTLE
THESE PLANS FORM PART OF

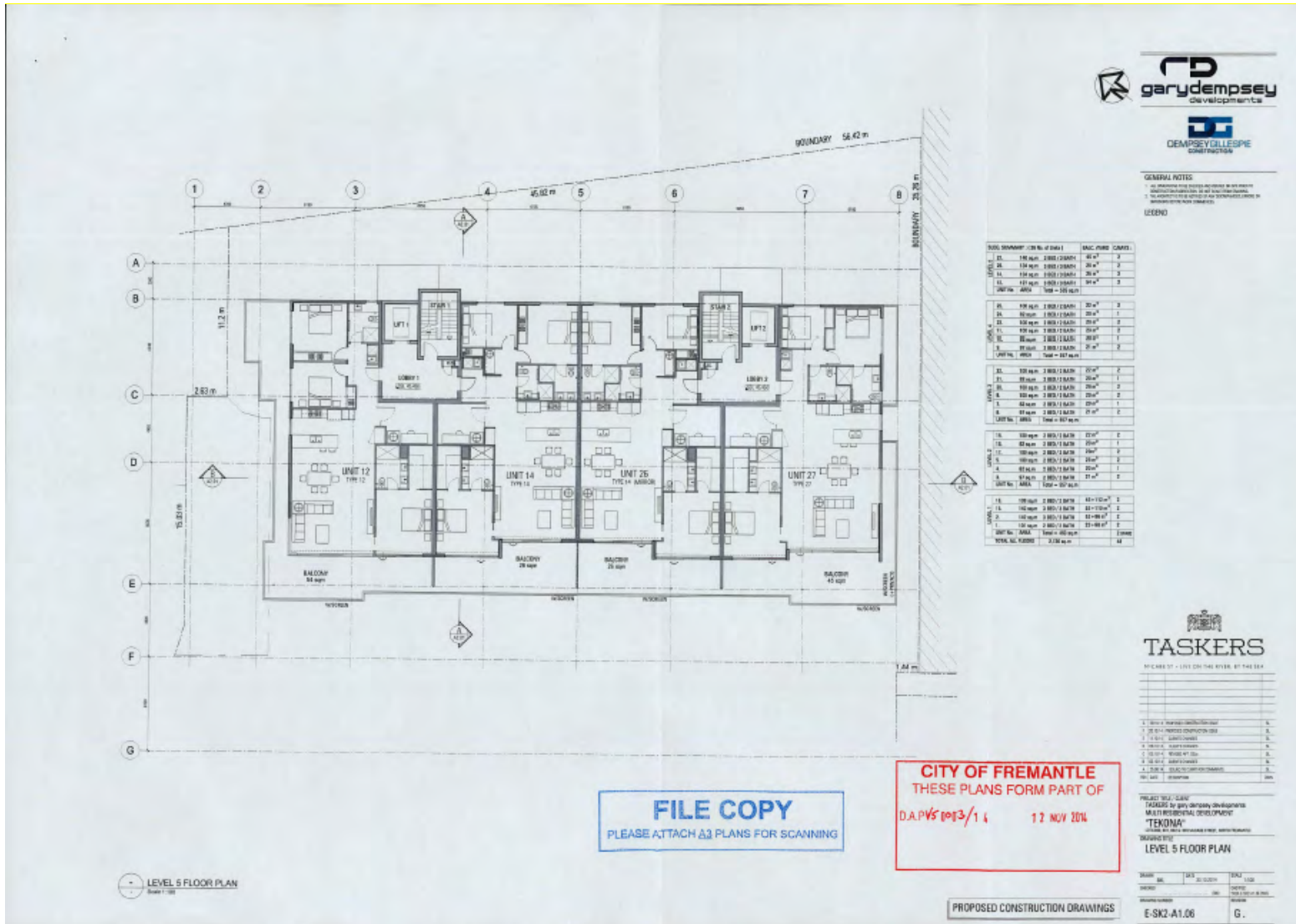
D.A.P/S 0003/1 & 12 NOV 2014

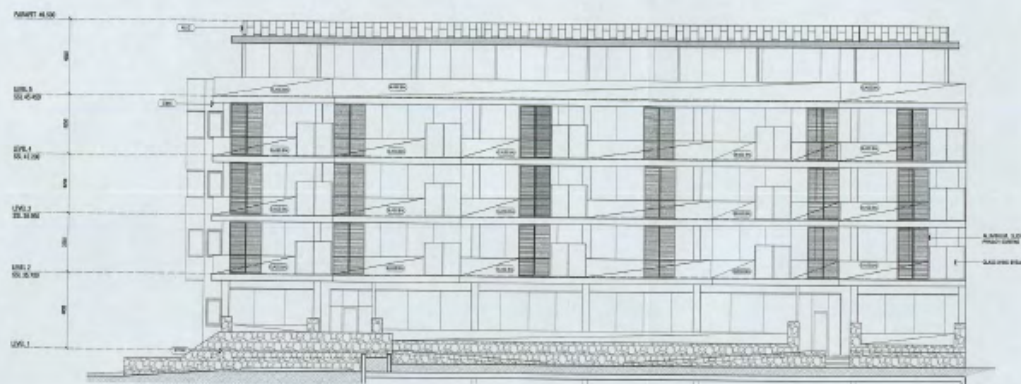












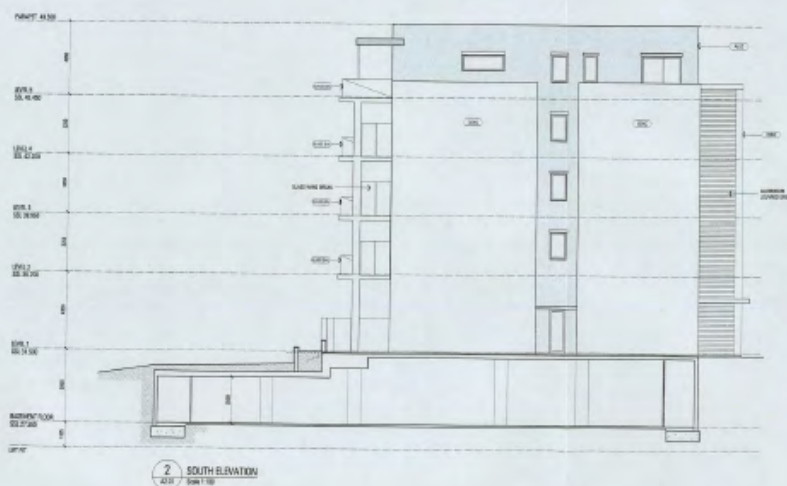
GENERAL NOTES

1. ALL SURVEYING TOTAL STATION WORK TO BE BY THE STATE SURVEYING DEPARTMENT. NO OTHER SURVEYING WORK TO BE DONE BY THE CONTRACTOR.
2. THE SURVEYING DEPARTMENT WILL BE RESPONSIBLE FOR THE SURVEYING WORK AND THE CONTRACTOR WILL BE RESPONSIBLE FOR THE CONSTRUCTION WORK.

LEGEND

FILE COPY

PLEASE ATTACH A3 PLANS FOR SCANNING



2 SOUTH ELEVATION
42.31 (Scale 1"=10')

CITY OF FREMANTLE
THESE PLANS FORM PART OF
D.A.PV51003/14 12 NOV 2014

D.A.PV50003/14 12 NOV 2014



TASKERS

en-Case 57 - 2011-02-01 09:00:00.000

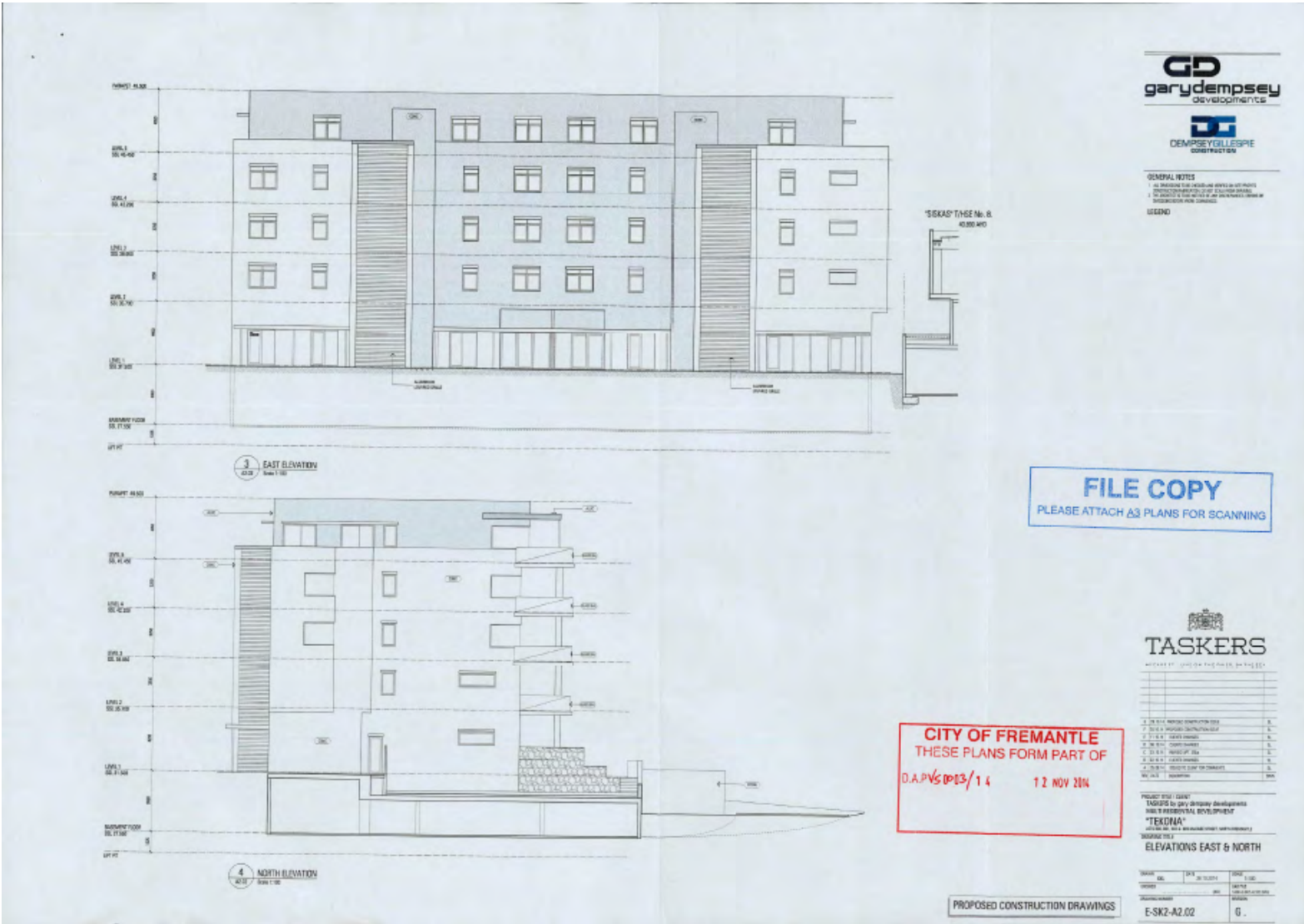
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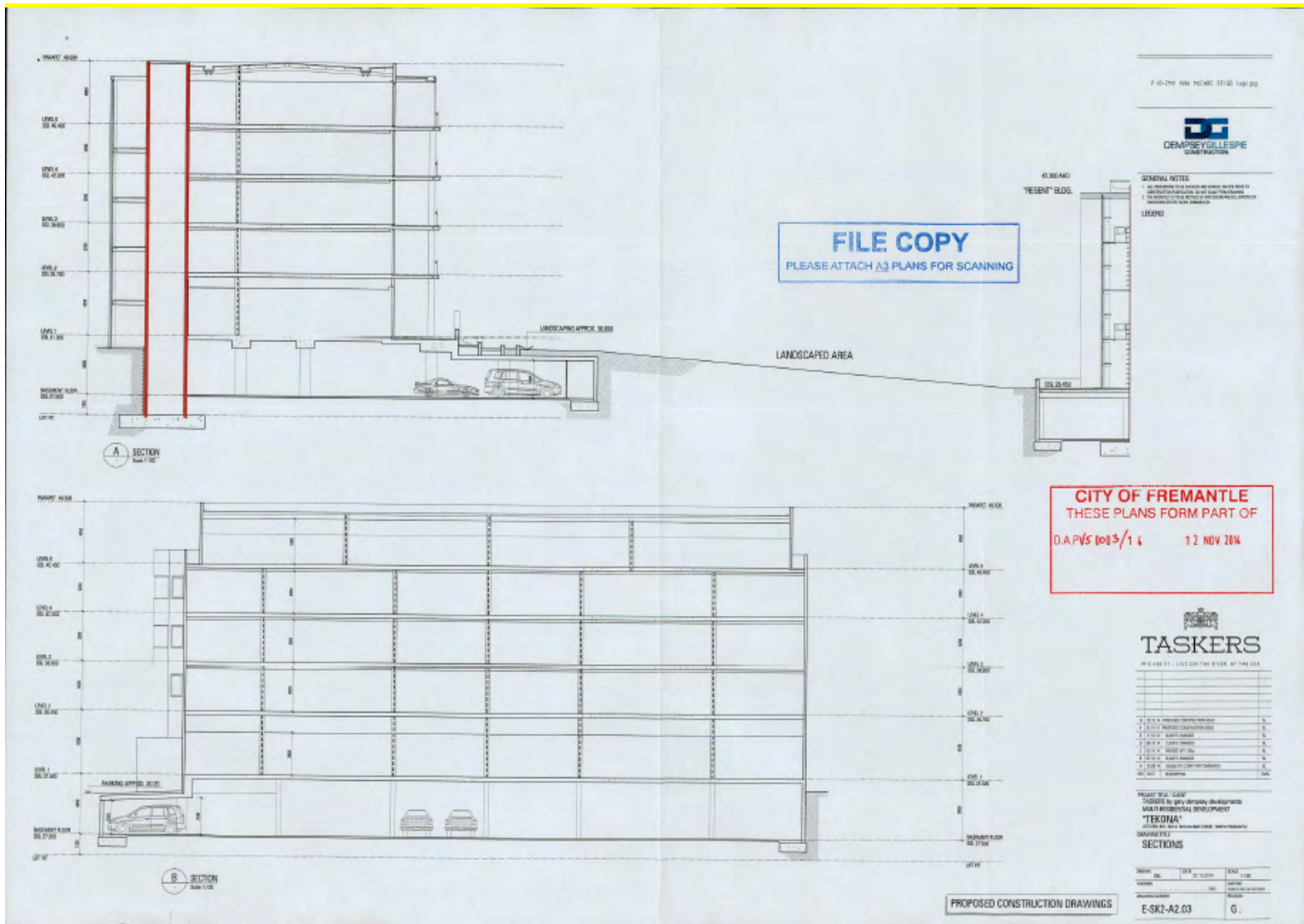
PROJECT TITLE/OWNER
TAKERS by Gary Dempsey Developments
MULTI RESIDENTIAL DEVELOPMENT
"TEKONA"
10000 80 AVE + 800000 STREET NORTH HAWAII
DRAWING TITLE
ELEVATIONS WEST & SOUTH

Model	SRL	REV	09-10-2014	Scale	1:100
Project				Scale	NONE
Project Name				Scale	NONE
F-SK2-A2.01				G	

PROPOSED CONSTRUCTION DRAWINGS

Designation	Version
E-SK2-A2.01	G





**ATTACHMENT 3: Previous Responsible Authority Report (DAP80006/13)
(DP/13/00843)**

**Form 1 - Responsible Authority Report
(Regulation 12)**

Application Details:	Five Storey Multiple Dwelling Development (20 Multiple Dwellings) and Basement Vehicle Parking Level
Property Location:	No. 9-11 (Lots 18, 315 and 326) McCabe Street, North Fremantle
DAP Name:	Metro South-West JDAP
Applicant:	TPG Town Planning, Urban Design and Heritage
Owner:	Taskers Living Pty Ltd
LG Reference:	DAP80006/13
Responsible Authority:	City of Fremantle
Authorising Officer:	Manager Statutory Planning
Department of Planning File No:	DP/13/00843
Report Date:	20 December 2013
Application Receipt Date:	14 October 2013
Application Process Days:	90 days
Attachment(s):	1: Location Plan 2: Development Plans dated 15 October 2013 (SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01)

Recommendation:

That the Metro South-West Joint Development Assessment Panel resolves to:

Approve DAP Application reference DP/13/00843 and accompanying plans dated 15 October 2013, SK2.00, SK2.01, SK2.02, SK2.03, SK2.04, SK2.05 & SK3.01 in accordance with Clause 10.2 of the City of Fremantle Local Planning Scheme No. 4, subject to the following conditions:

- 1. This approval relates only to the development as indicated on the approved plans, dated 15 October 2013. It does not relate to any other development on this lot and must substantially commence within four years from the date of this decision letter.**
- 2. The design and construction of the development is to meet the 4 star green star standard as per Local Planning Policy 2.13 or alternatively to an equivalent standard as agreed upon by the Chief Executive Officer, City of Fremantle when a green star rating tool is not available. Any costs associated with generating, reviewing or modifying the alternative equivalent standard is to be incurred by the owner of the development site. Within 12 months of an issue of a certificate of**

Building Compliance for the development, the owner shall submit either of the following to the City to the satisfaction of the Chief Executive Officer – City of Fremantle;

- i. a copy of documentation from the Green Building Council of Australia certifying that the development achieves a Green Star Rating of at least 4 Stars, or**
 - ii. a copy of agreed equivalent documentation for instance where there is no green star rating tool available certifying that the development achieves a Green Star Rating of at least 4 Stars.**
- 3. All storm water discharge shall be contained and disposed of on-site.**
- 4. Prior to occupation, all air-conditioning plant, satellite dishes, antennae and any other plant and equipment to the roof of the building shall be located or screened so as not to be highly visible from beyond the boundaries of the development site to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 5. All landscaped areas are to be maintained on an ongoing basis for the life of the development on the site to the satisfaction of the Chief Executive Officer, City of Fremantle**
- 6. Prior to the occupation of the development, vehicle crossovers shall be constructed in either paving block, concrete, or bitumen and thereafter maintained to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 7. Prior to occupation of the development, the car parking and loading area(s), and vehicle access and circulation areas shown on the approved site plan, including the provision of disabled car parking, shall be constructed, drained, and line marked and provided in accordance with Clause 5.7.1(a) of the City of Fremantle Local Planning Scheme No.4, to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 8. All car parking, and vehicle access and circulation areas shall be maintained and available for car parking/loading, and vehicle access and circulation on an ongoing basis to the satisfaction of the Chief Executive Officer, City of Fremantle.**
- 9. Prior to the submission of a Building Permit application, the owner is to submit further details on the storage and management of the waste generated by the development to the satisfaction of and approval by the Chief Executive Officer, City of Fremantle.**
- 10. Prior to the issue of a building permit the Applicant is to provide details of facade design and a schedule of colours, finishes and textures to the satisfaction of the Chief Executive Office, City of Fremantle, on advice of the Design Advisory Committee.**

Background:

Property Address:	No. 9-11 (Lots 18, 315 and 326) McCabe Street, North Fremantle
Zoning	MRS: Urban
	LPS: Development Area
Use Class:	N/A
Strategy Policy:	Local Planning Area 3 – North Fremantle
Development Scheme:	City of Fremantle – Local Planning Scheme No. 4 – McCabe Street Structure Plan
Lot Size:	Lot 18: 8,059m ² , Lot 315: 3,743m ² , Lot 326: 2318m ²
Existing Land Use:	N/A - Vacant
Value of Development:	\$8,000,000

The subject site consists of three lots; Lot 18, 315 and 326 McCabe Street, North Fremantle ('the site') (see **Attachment 1 – Location Plan**). The subject site is mostly vacant of improvements however a multiple dwelling development subject to prior approval of the City is under construction in the south-west corner of the subject site.

The subject site is located within the North Fremantle Local Planning Area pursuant to Local Planning Scheme No. 4 ('LPS4') and exists in the portion of McCabe Street bound by Stirling Highway to the west, and Edwards Parade, Mosman Park, to the east.

The subject site also subject to the following classifications;

- Zoned 'Development Area (DA18)' pursuant to LPS4 and subject to the McCabe Street Structure Plan;
- Contained within the North Fremantle Heritage Area but is not individually listed on the City's Heritage List. The subject site also falls within the 'Rocky Bay' Aboriginal Heritage Area; and,
- The subject site is noted as being adjacent to high voltage transmission lines located on McCabe Street.

A summary of the relevant planning history relating to the site is as follows;

- On 3 March 2009, the SAT set aside a decision made by the City on 25 June 2008 to refuse the Structure Plan for the subject site and approved the Structure Plan. The Structure Plan outlines a maximum wall height of 43.50m AHD and a maximum ridge height of 48.00m AHD. The Structure Plan contemplates a ridged roof design, as opposed to a flat or skillion style roof with higher wall or gable ends;
- On 9 June 2010, the City granted planning approval for 'Residential Grouped and Multiple Dwelling Development' (DA0087/10). This application consisted of the construction of ninety-five (95) multiple dwellings across four buildings and eight (8) grouped dwellings in an additional building at the land. A further variation to this approval increasing the number of multiple dwellings from 95 to 96 and making

minor modifications to common areas and on-site parking was approved by the City on 13 April 2011 (VA0009/11). Condition 14 of the original approval (DA0087/10) required that the land be amalgamated into one lot;

- On 9 June 2012, planning approval DA0087/10 and VA0009/11, being the original and subsequently modified approval for the redevelopment of the land expired;
- On 25 October 2012, the City approved an application for 'Six Storey (Two Basement), 51 Apartment Multiple Dwelling Development' at the subject site (DA0417/12). This proposal broadly reflected that approved as part of DA0087/10 and VA0009/11;
- At its meeting of 15 May 2013, the City granted planning approval for modifications to DA0417/12 that included modifications to the external height of the proposal (DA0176/13); and,
- The application forms part of the staged development of the subject site. The first stage of the proposal, being the 6 storey multiple dwelling development approved as DA0417/12 (and subsequently DA0176/13), is currently under construction. Two further applications DA0448/13 (8 Grouped Dwellings) and DA0449/13 (21 multiple dwellings), represent the second and third stages of the proposal. The current proposal DAP80006/13 (20 multiple dwellings) represents the fourth stage of the development. At this time, the fifth and final stage, shown indicatively on the development plans in the south-east corner of the subject site has not been submitted to the City.

Details: outline of development application

The application seeks planning approval for a 'Five Storey Multiple Dwelling Development (20 Multiple Dwellings) and Basement Vehicle Parking Level' ('the proposed development') (see **Attachment 2 – Development Plans dated 15 October 2013**) at the site including;

- A five (5) storey multiple dwelling building containing 20 residential dwellings and an additional basement level containing vehicle parking;
- The building having a maximum external wall height of 46.50m AHD and a maximum roof height of 47.1m AHD;
- 39 vehicle parking bays being provided in the basement level of the building including 14 tandem bays. 4 visitor bays are also provided to the northern side of the building;
- The ground floor containing lobby and service areas as well as 5 multiple dwelling;
- The first, second and third floors all containing service areas and an additional 5 dwellings per floor;
- The fourth (most upper floor) containing additional living and bedroom areas for three of the dwelling accessed via the third floor; and,
- The total plot ratio of dwelling proposed as determined by the R-Codes being 2,677.2m².

Legislation & policy:

The application has been assessed against the following legislative documents:

- City of Fremantle Local Planning Scheme No. 4 (LPS4) – application for development on the site is to be determined in accordance with provisions of Part 10 of the Scheme.

Legislation

The following provisions of LPS4 are considered relevant in the assessment of the planning application;

- Clause 6.2.3 – Subdivision and Development in Development Areas;
- Clause 9.4 – Advertising of Applications;
- Clause 2.3 – Relationship of Local Planning Policies to Scheme; and,
- Clause 10.2 – Matters to be considered by the Council.
- Clause 11.8 – Design Advisory Committee

State Government Policies

There are no State Government policies relevant to the assessment of the application.

Local Policies

The proposed development is subject to the following Local Planning Policies, adopted under LPS4;

- Local Planning Policy 3.11 – McCabe Street Area – Height of New Buildings; and,
- Local Planning Policy 2.13 – Sustainable Building Design Requirements.
- Local Planning Policy 1.9 - *Design Advisory Committee and Principles of Design*;

Consultation:

Public Consultation

The application was required to be advertised in accordance with Clause 9.4 of the LPS4. In accordance with Local Planning Policy 1.3 – Public Notification of Planning Proposal, the application was advertised as a ‘Significant Application’. At the conclusion of the advertising period, being 15 November 2013, the City had received one submission including 1 objection. The issues raised are summarised as follows;

- *Vehicle traffic*; Concern is raised in regards to the vehicle traffic the proposal will generate once the site is fully developed;
- *Cycle route*; No major cycle route along McCabe Street will become hazardous, especially at the intersection of Stirling Highway.

Consideration of the matters raised is discussed further in the Planning Assessment section of this report.

Planning Services Committee (Internal referral)

The application was presented to the City's Planning Services Committee ('PSC') meeting of 4 December 2013. The PSC resolved to adopt the same recommendation as recommend in this report.

Design Advisory Committee (DAC) (Internal referral)

The proposal was presented to the City's Design Advisory Committee at its meeting of 15 November 2013. The following recommendations were made;

"The Committee was not able to provide detailed advice as the required information was not provided.

It is acknowledged that the development previously had planning approval however it is the role of this committee to provide advice relating to design quality. The statutory planning framework has changed since the original planning approval was issued and now requires that this development be referred to the DAC for comment.

The Committee would be happy to give further consideration to the proposal once additional information has been provided. This will need to include:

1. *Streetscape character analysis;*
2. *Design matters raised in Local Planning Policy 1.9 - Design Advisory Committee and Principles of Design;*
3. *Response to the CABA Alarm Bells;*
4. *Site sections and sectional elevations;*
5. *Materials and finishes;*
6. *Detailing;*
7. *Durability;*
8. *Comprehensibility (how an individual navigates the site)."*

As explained earlier in this report, the application was presented to the City's PSC meeting of 4 December 2013. At this meeting, the Committee resolved to recommend approval of the application subject to a condition requiring further design details to the satisfaction of the Chief Executive Office, City of Fremantle.

The application has not been re-presented to the City's DAC. Consistent with the advice of the PSC, a condition of approval requiring further design details is recommended.

Planning assessment:

The subject land is zoned 'Development Area' and is not given a density coding. As a result, the assessment of the proposed development is largely provided for in the Structure Plan.

Clause 6.2.3.2 of LPS4 requires that;

*'The subdivision and development of land within a Development Area is to **generally be in accordance with any structure plan** that applies to the land.'* [Emphasis added].

Consideration of the development is therefore made, generally, against the content of the Structure Plan.

Building location & form

The design context for the proposed development has been broadly set by decisions at earlier stages of the planning process, notably the approval of a structure plan for the site by the State Administrative Tribunal in March 2009. The structure plan included details of the location and orientation of new buildings within the site and their overall massing including maximum heights. Subsequent development applications have proposed building forms in general conformity with the content of the approved structure plan as is required under clause 6.2.3.2 of LPS4. These previous applications were not considered by the DAC as the committee was only established after the original planning approvals were granted..

The applicant was previously advised of the requirements for presentations to the DAC that include the 8 matters raised above. The architect for the proposal was not present at the DAC meeting to give an explanation of the architectural design rationale for the proposal. Consideration of matters by the DAC is important to the assessment of the application in terms of its design and function and is required to be given due regard pursuant to clause 11.8.5 of LPS4.

The proposed development is considered to be generally consistent with the requirements of the Structure Plan, however as per the recommendation of the DAC, it is considered that there is insufficient information provided in respect to certain design elements of the proposal.

As a result a condition of approval requiring further design details being submitted and endorsed by the City is recommended.

External Wall Height

The adopted Structure Plan provides a maximum external wall of 43.50m AHD and a maximum ridge height of 48.00m AHD for the subject site. Local Planning Policy 3.11 – McCabe Street (Height of New Buildings) provides a maximum height for development of 14.0m. As the Structure Plan provides a more specific height requirement for the land, the Structure Plan is preferred and the higher order statutory document (the policy was used to inform the structure plan).

The table below outlines the required (as per the adopted Structure Plan), previously approved and currently proposed AHD levels of the external wall height and maximum roof ridge height.

Element	Structure Plan Requirement	Current Proposal	Variation to Structure Plan
Maximum Wall	43.50m AHD	46.50m AHD	3.00m AHD

Maximum Roof	48.00m AHD	47.10m AHD	Nil
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The discretionary decision is supported for the following reasons;

- The Structure Plan outlines that buildings in the location of the proposed development should be limited to four storeys. The building proposed is clearly a five storey proposal but the addition storey is setback from the main building wall to all elevations. This not only reduces the bulk and scale of the building but takes advantage of space that would otherwise form a pitched roof. Given the setback of the additional floor, the proposal is not considered to be of greater bulk, scale or impact compared to a four storey proposal with a pitched roof or loft level;
- The portions of wall height that exist above the Structure Plan requirement are setback from the edge of the main building face, reducing the overall bulk of the proposal;
- The additional height, in particular the additional external wall height is not considered to unreasonably restrict access to direct light or views of significance to surrounding properties having regard to the separation distance between the development and the nearest residential property; and,
- The proposed modifications are considered minor in nature having regard to previous approvals and represent only a marginal departure from the recommendations of the Structure Plan.

Visual Privacy

The Structure Plan outlines that the desired density of the subject site ought to be equivalent to R80; despite the fact that no density coding is directly applicable under the scheme maps of LPS4. On the basis of the R80 code, the proposal is considered to meet all of the deemed-to-comply requirements of the R-Codes in the following ways;

- All major openings and raised outdoor living areas to boundaries of adjoining residential properties (including the internal separation between the proposed development and other residential developments on the subject site) meet the cone-of-vision setback requirement of the R-Codes; or
- The elevations where a lesser setback is sought address non-residential premises at No. 19-21 McCabe Street) and in those instance, the visual privacy provisions do not apply.

Sustainable Building Design

The proposed development is required to achieve a rating of not less than 4 Star Green Star (or equivalent) using the relevant Green Building Council of Australia rating tool pursuant to Local Planning Policy 2.13 – Sustainable Building Design ('LPP2.13').

The applicant has proposed the use of an alternative energy efficiency rating system in lieu of the 4 Star Green Star requirements. Use of this rating tool is

consistent with previous resolutions of the City and Joint Development Assessment panels relating to recent applications.

A condition of approval requiring achievement of an equivalent rating under the alternative tool is recommended.

Vehicle traffic

As part of DA0087/10 previously approved by the City, the applicant provided to the City a Transport Statement prepared by Shawmac, dated July 2009. The statement stated that the transport capabilities of the site and surrounding connecting roads were sufficient. This report was based on a similar yield of dwellings to what is currently proposed and remains relevant.

Given the relevancy of this report and the general compliance with the adopted Structure Plan, the submission received relating to vehicle traffic are not considered to be of planning concern.

Conclusion:

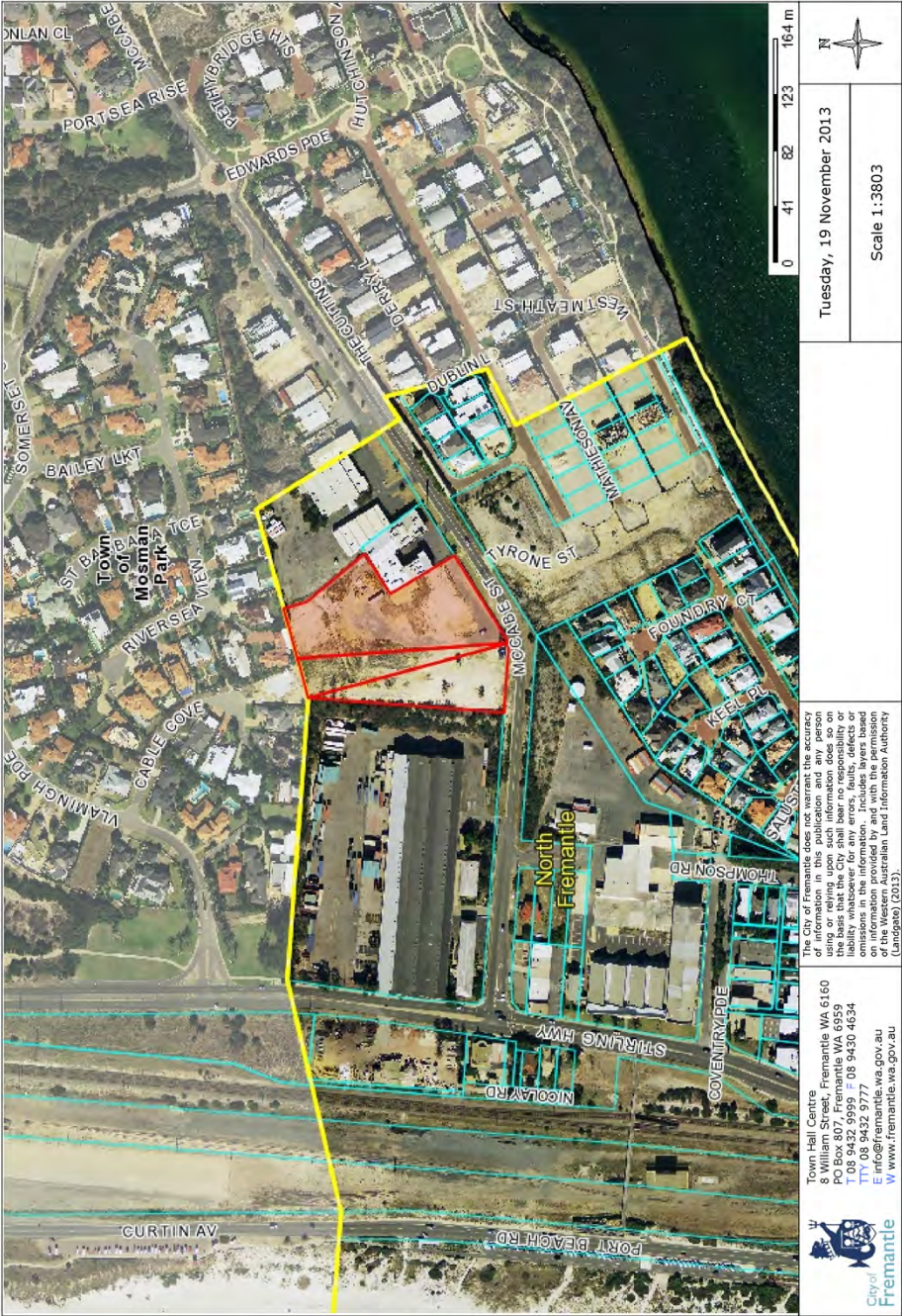
The application seeks planning approval for the development of a five storey multiple dwelling development (20 residential dwellings) at the subject site. The proposal is summarised as follows;

- The subject site is zoned 'Development Area (DA18) pursuant to LPS4 and is subject to the adopted Structure Plan;
- In accordance with clause 6.2.3.2 of LPS4, the proposal is required to be generally in accordance with the Structure Plan;
- The proposal is generally consistent with the Structure Plan in terms of building position, scale, density, height and vehicle traffic; and,
- The proposal is required to meet the sustainable building requirements of LPP2.13 and a condition of approval is recommended to this affect;

The application was presented to the DAC which determined that there was insufficient information required to comment on the proposal. The application was presented to the PSC who resolved to recommend approval of the application, subject to a condition requiring further design details.

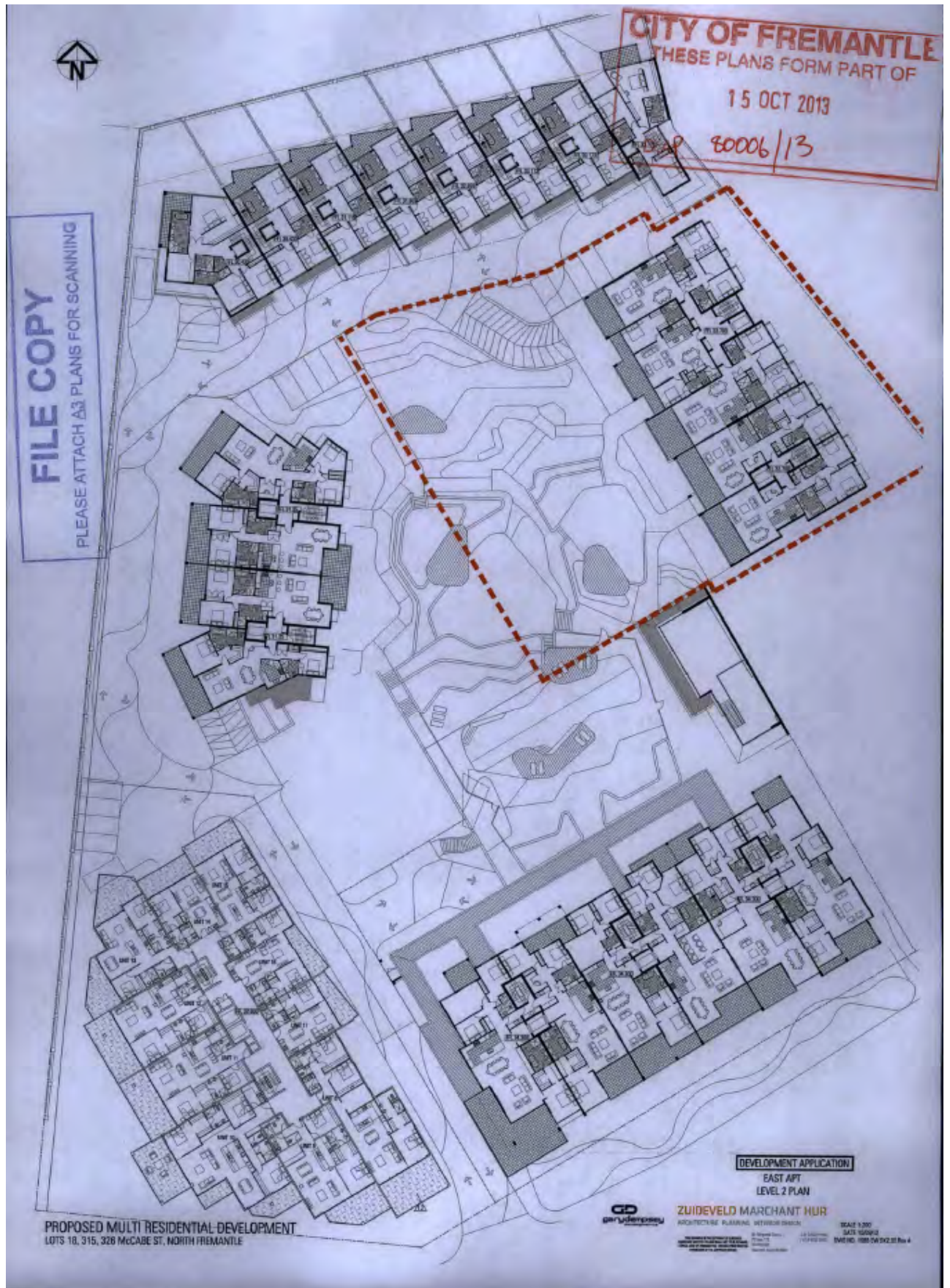
The application is considered to meet the relevant planning framework and is therefore recommended for conditional approval.

Attachment 1: Location Plan



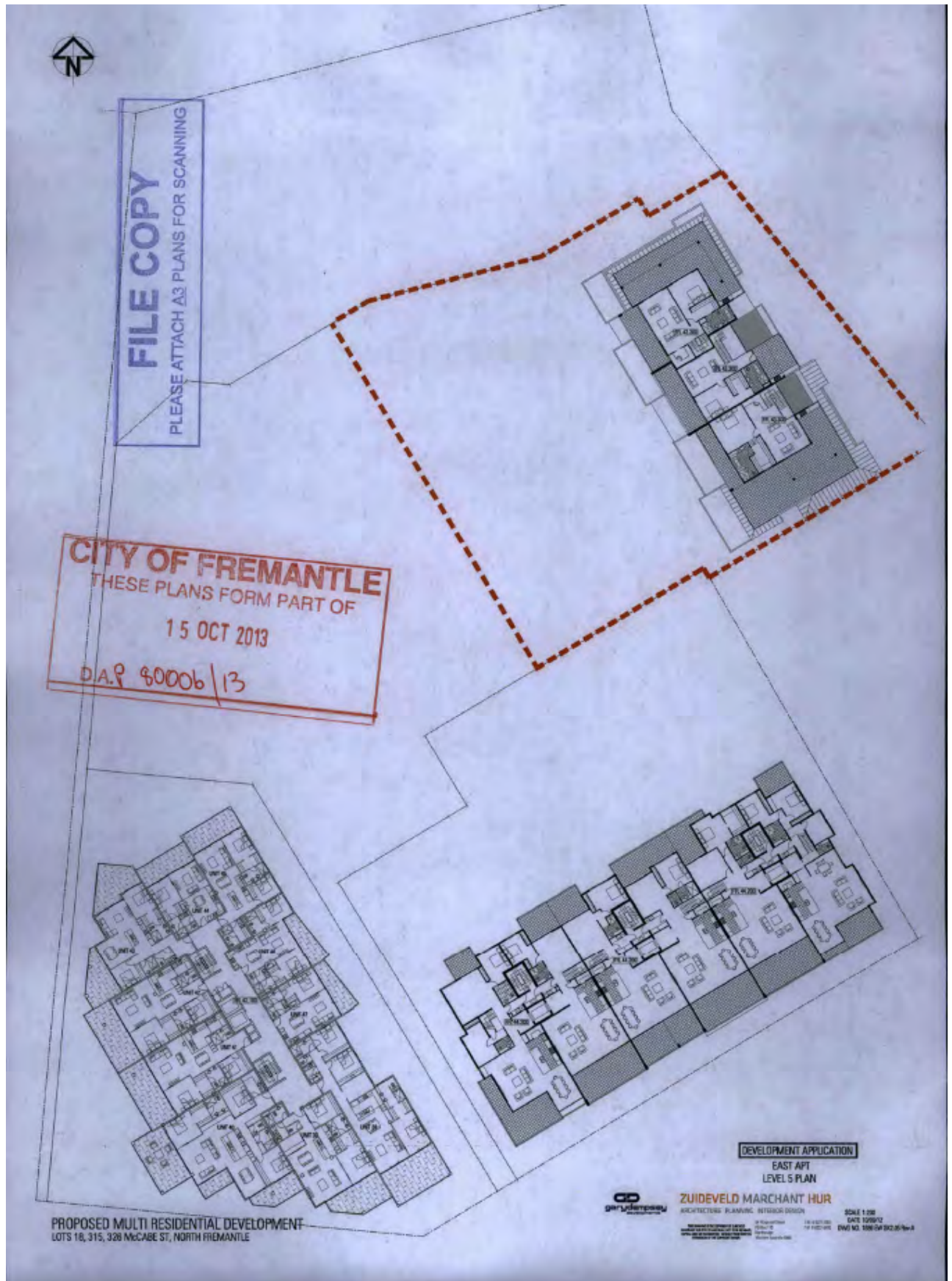
Attachment 2: Development Plans dated 15 October 2013



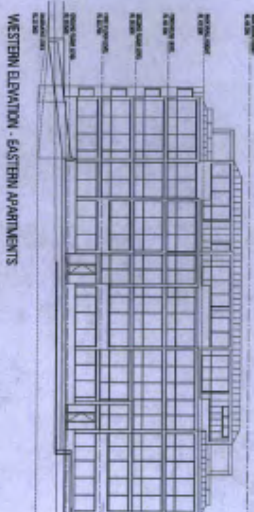
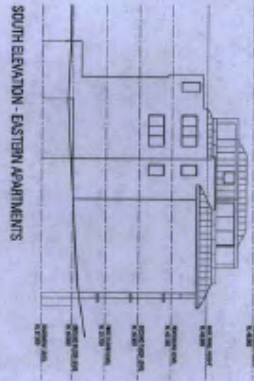
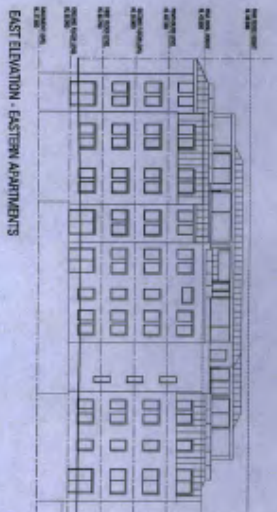
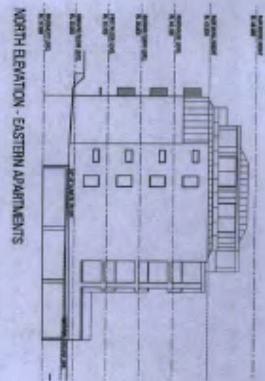








FILE COPY
PLEASE ATTACH A3 PLANS FOR SCANNING



CITY OF FREMANTLE
THESE PLANS FORM PART OF
15 OCT 2013
D.A. 80006/13

PROPOSED MULTI RESIDENTIAL DEVELOPMENT
LOTS 18, 315, 326 McCABE ST, NORTH FREMANTLE

DEVELOPMENT APPLICATION
EAST APT ELEVATIONS
ZUIDEVELD MARCHANT HUR
ARCHITECTURE PLANNING INTERIOR DESIGN
100 Fitzgerald Street, Suite 101, Fremantle, WA 6155
Tel: 08 9437 1000 Fax: 08 9437 1001
Email: info@zuidenveld.com.au Website: www.zuidenveld.com.au
SCALE 1:200 DATE 10/08/11 DWG NO. 1888 EN SCALE Rev A